



April 18, 2024

Ref: 73305.01

Mr. Jeffrey Willis, Executive Director  
Coastal Resources Management Council  
Oliver H. Stedman Government Center  
4808 Tower Hill Road  
Wakefield, RI 02879-1900

Re: **Fishing Structure Relocation**  
RIDOT – East Bay Bike Path Bridge Replacements – Reconstruction  
Barrington River Bridge (RIDOT Bridge No. 083751) and  
Palmer River Bridge (RIDOT Bridge No. 083851), Barrington and Warren, RI  
PTSID No. 0881A  
CRMC Application No. 2023-04-094

Dear Mr. Willis:

The Rhode Island Department of Transportation (RIDOT) submitted a Category B Assent Application on April 19, 2023 for proposed reconstruction of the East Bay Bike Path Bridges across the Barrington and Palmer Rivers in Barrington and Warren (the Project). More recently, design modifications were submitted under cover letter dated February 7, 2024 pertaining to fishing accommodations and riprap scour protection at each bridge pier. This current submission provides new plan sets that address public comments regarding the location of the fishing pier at the Barrington River in the February 7, 2024 submission. RIDOT has now shifted the fishing pier from the east side of the Barrington River to the west side of the River. Plans and elevations related to the fishing structure have been outlined with revision clouds on the revised Project plans for ease of identification. Please find enclosed for your staff's coordination, three copies of the following materials: this transmittal letter, revised Project plan sets for Volumes 1 and 2.

Thank you, and please feel free to contact either Ms. Alisa Diaz Richardson of RIDOT at (401) 479-1327 or [Alisa.Richardson@dot.ri.gov](mailto:Alisa.Richardson@dot.ri.gov), or me at (401) 457-2053 or [aprezioso@vhb.com](mailto:aprezioso@vhb.com), if you have any questions or require additional information.

Sincerely,  
VHB

Andrew Prezioso, PE  
Project Manager

cc: Alisa Diaz Richardson, MS, PE, PMP, RIDOT  
Hamid Akinfolarin, Project Manager I, RIDOT  
Scott S. Hobson, PWS, VHB  
Andres Aveledo, Project Manager, Aetna Bridge



Engineers | Scientists | Planners | Designers

1 Cedar Street, Suite 400, Providence, Rhode Island 02903

P 401 272 8100 F 401 277 8400 [www.vhb.com](http://www.vhb.com)

INDEX

SHEET NO.	DESCRIPTION
1	COVER SHEET
2	SYMBOLS AND STANDARD LEGEND
3	STANDARD NOTES - 1
4	STANDARD NOTES - 2
5	SYMBOLS, LEGEND & NOTES
6	TYPICAL SECTIONS
7-12	TEMPORARY WORK PLATFORM PLAN AND SECTION NOS. 1-2
13	GENERAL PLAN NOS. 1-4
14	RESTORATION PLAN NOS. 1-3
15-20	MISCELLANEOUS DETAILS
21	RESTORATION PLAN NOS. 1-3
22-25	CROSS SECTIONS

STATE OF RHODE ISLAND



DEPARTMENT OF TRANSPORTATION

PLAN, PROFILE AND SECTIONS OF PROPOSED

EAST BAY BIKE PATH BRIDGE REPLACEMENTS  
BIKE PATH AND BRIDGE APPROACH PLANS

RECONSTRUCTION PLANS

ENVIRONMENTAL PERMITTING SET

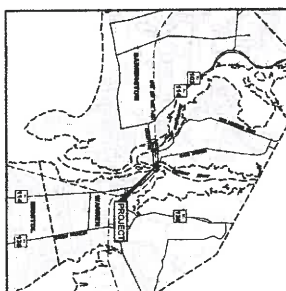
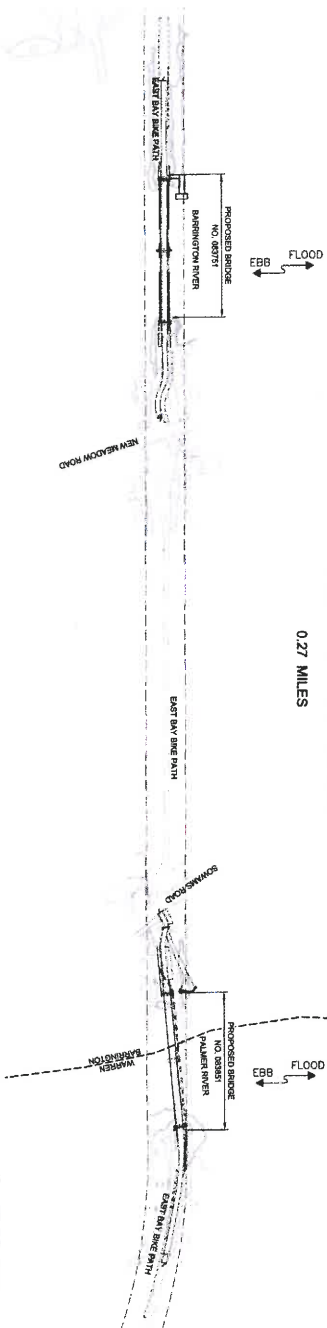
VOLUME 1

EAST BAY BIKE PATH OVER BARRINGTON RIVER AND PALMER RIVER

TOWNS OF BARRINGTON AND WARREN  
COUNTY OF BRISTOL

R.I. CONTRACT NO. 2022-DB-012 F.A. PROJECT NO. BRO-0838(002)

PAVEMENT STRUCTURE  
1" CLASS 4.75 HMA  
3" CLASS 19.0 HMA  
12" GRAVEL BORROW SUBBASE COURSE  
0.27 MILES



REV.	DATE	REVISIONS	BY	CHKD.	DATE
1	01/16/2024	PROJ-0838(002)	2022	1	28

PERMIT PLAN  
REVISIONS  
FEBRUARY 2024  
(REVISED APRIL 2024)

APPROVED	DATE
ADMINISTRATOR, PROJECT MANAGEMENT	DATE
APPROVED	DATE
CHIEF ENGINEER OF INFRASTRUCTURE	DATE
APPROVED	DATE
DIRECTOR	DATE
DEPARTMENT OF TRANSPORTATION FEDERAL HIGHWAY ADMINISTRATION	DATE
DIVISION ADMINISTRATOR	DATE

SCALES OF DRAWINGS  
Plan 1 inch = 20 feet  
Profile 1 inch = 4 feet  
Cross Section 1 inch = 4 feet  
BASE OF LEVELS  
NAVD 83



Contract Number 2022-DB-012  
Number of Sheet 1  
Total Sheets 28



AEMA  
AEMA Bridge Company

[illegible]



## GENERAL NOTES

1. ANY DAMAGE TO EXISTING PAVEMENT, BRIDGES, DRAINAGE STRUCTURES, ETC. CAUSED BY THE CONTRACTOR SHALL BE REPAIRED BY THE CONTRACTOR AT AN ADDITIONAL COST TO THE STATE.
2. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL EXISTING UTILITIES, STRUCTURES, AND MATERIALS AS WELL AS ANY ADJACENT PROPERTY.
3. IT IS THE CONTRACTOR'S RESPONSIBILITY TO PROTECT THE EXISTING CONCRETE PAVEMENT FROM DAMAGE BY THE CONSTRUCTION OF THE NEW PAVEMENT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL EXISTING UTILITIES, STRUCTURES, AND MATERIALS AS WELL AS ANY ADJACENT PROPERTY.
4. ALL UTILITIES SHALL BE PROTECTED AND MAINTAINED IN WORKING ORDER THROUGHOUT THE CONSTRUCTION PROCESS.
5. THE FREQUENCY AND APPLICATION RATES FOR THE DUST CONTROL TREATMENT SHALL BE DETERMINED BY THE CONTRACTOR TO MEET THE REQUIREMENTS OF SECTION 907.
6. ALL EXISTING AND NEW DRAINAGE STRUCTURES SHALL BE PROTECTED BY THE CONTRACTOR AND MAINTAINED IN WORKING ORDER THROUGHOUT THE CONSTRUCTION PROCESS.
7. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL EXISTING UTILITIES, STRUCTURES, AND MATERIALS AS WELL AS ANY ADJACENT PROPERTY.
8. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL EXISTING UTILITIES, STRUCTURES, AND MATERIALS AS WELL AS ANY ADJACENT PROPERTY.
9. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL EXISTING UTILITIES, STRUCTURES, AND MATERIALS AS WELL AS ANY ADJACENT PROPERTY.
10. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL EXISTING UTILITIES, STRUCTURES, AND MATERIALS AS WELL AS ANY ADJACENT PROPERTY.
11. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL EXISTING UTILITIES, STRUCTURES, AND MATERIALS AS WELL AS ANY ADJACENT PROPERTY.
12. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL EXISTING UTILITIES, STRUCTURES, AND MATERIALS AS WELL AS ANY ADJACENT PROPERTY.
13. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL EXISTING UTILITIES, STRUCTURES, AND MATERIALS AS WELL AS ANY ADJACENT PROPERTY.
14. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL EXISTING UTILITIES, STRUCTURES, AND MATERIALS AS WELL AS ANY ADJACENT PROPERTY.
15. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL EXISTING UTILITIES, STRUCTURES, AND MATERIALS AS WELL AS ANY ADJACENT PROPERTY.
16. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL EXISTING UTILITIES, STRUCTURES, AND MATERIALS AS WELL AS ANY ADJACENT PROPERTY.
17. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL EXISTING UTILITIES, STRUCTURES, AND MATERIALS AS WELL AS ANY ADJACENT PROPERTY.
18. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL EXISTING UTILITIES, STRUCTURES, AND MATERIALS AS WELL AS ANY ADJACENT PROPERTY.
19. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL EXISTING UTILITIES, STRUCTURES, AND MATERIALS AS WELL AS ANY ADJACENT PROPERTY.
20. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL EXISTING UTILITIES, STRUCTURES, AND MATERIALS AS WELL AS ANY ADJACENT PROPERTY.

## GENERAL NOTES (CONTINUED)

20. FOR ALL SUBJECTS INVOLVING NUCLEAR REACTION ISSUES, THE COORDINATOR SHALL BECOME FAMILIAR WITH, AND OBTAIN TO ALL PROJECTS, THE FOLLOWING INFORMATION: (1) THE NUCLEAR REACTOR, REACTOR CORES, WORK, AND/OR USE, MANAGEMENT PLANS DEVELOPED FOR THE PROJECT, ACTIONS OF THE PROJECT, AND THE STATUS OF THE PROJECT; (2) THE PROJECT DOCUMENTS, ALL CORRESPONDENCE WITH CONTRACTORS, AND ALL CORRESPONDENCE WITH THE PROJECT MANAGER, AND INCLUDED WITH THE COST FOR THE ASSOCIATED RISK (RISK);
21. NO UNAPPROVED CONSTRUCTED FACILITY, ANY PROJECT, NUCLEAR REACTOR, THE FINISHED SOURCE OF A TRANSMISSIBLE SLOPE, IN A CLAR ZONE, HEADWALL, PAVEMENT, ETC. ETC.
22. THE FINISHING SCENE OR STAGE OF A BREAKAWAY, ANY, ANY PROJECT, NUCLEAR REACTOR, THE FINISHED SOURCE OF A TRANSMISSIBLE SLOPE, IN A CLAR ZONE, 4-6 SON POINT, LOCAL POLICE, FIRE DEPARTMENT, ETC.

### DRAINAGE AND EROSION CONTROL NOTES

- [illegible]

### DRAINAGE AND EROSION CONTROL NOTES (CONTINUED)

- [illegible]

## UTILITY NOTES

- [illegible]



Aetna  
Aetna Bridge Company



1 Cedar Street  
Suite 400  
Providence, RI  
401.272.8100



**RHODE ISLAND**  
**DEPARTMENT OF TRANSPORTATION**

RHODE ISLAND

SCALE: NOT TO SCALE

## EAST BAY BIKE PATH BRIDGE REPLA

VOLUME  
EN

## STANDARD NOTES - 1

LANDSCAPE NOTES:

1. ALL PLANT MATERIAL MUST BE THOUGHT AS THE HUSBANDRY (A RECOGNIZED GROWING AND MAINTENANCE PRACTICE) OF THE PLANT MATERIAL. THE PLANT MATERIAL MUST BE SPECIFICATIONS, LATEST EDITION. ALL PLANT MATERIAL MUST BE HUSBANDRY GROWN.
2. NO PLANTATION GROWN PLANT MATERIAL WILL BE ACCEPTED.
3. ALL PLANT SUBSTITUTIONS AND/OR CHANGES IN PLANT LOCATION MUST BE APPROVED BY THE PROJECT ARCHITECT.
4. ALL PLANT MATERIAL IS TO BE FIELD LOCATED BY A REPRESENTATIVE FROM THE PROJECT LANDSCAPE ARCHITECTURE FIRM.
5. COORDINATE WITH THE R.I.O.D.T. CONSTRUCTION MANAGER PRIOR TO ALL PLANTING AND CLIMING NECESSARY TO COMPLETE THE WORK AS SHOWN ON THE PLANS.
6. ANY TRESS USED AS PLANTING SOIL SHALL HAVE A SHOT-LOW EXPOSED, RELATIVELY FREE OF ROOTS, MATERIAL, STONES, ROOTS, LUMPS OF SOIL, TREE M.B.G OF THE R.I.O.D.T. STANDARD SPECIFICATIONS, LATEST EDITION.
7. ALL TREES AND SHRUBS SHALL BE ATTACHED WITH ONE BARK WHOLE IN ACCORDANCE WITH THE R.I.O.D.T. STANDARD SPECIFICATIONS, LATEST EDITION.
8. ALL TREES AND/OR SHRUBS THAT ARE PLANTED AS A BED SHALL BE ATTACHED AS A BED.
9. PROTECT A MINIMUM 6"-8" BRANCHING SHOWN ON ALL TREES INSTALLED PRIOR TO DRAINAGE AND/OR PIERCING ACCESS ROADS.
10. PLANTING OF TREES AND/OR SHRUBS SHALL BE HUSBANDRY GROWN AND CHANGES OF PLANTING THE R.I.O.D.T. STANDARD SPECIFICATIONS, LATEST EDITION. NO PLANTATION GROWN PLANT MATERIAL WILL BE ACCEPTED. THE SOIL USED AS PLANTING SOIL SHALL BE ATTACHED WITH ONE BARK WHOLE.

### STRUCTURAL NOTES FOR HIGHWAY SIGNS LUMINAIRES AND TRAFFIC SIGNALS:

## GENERAL

1. ALL SUPPORT DESIGNS AND ASSOCIATED SHOP DRAWING REVIEWS SHALL BE IN CONFORMANCE WITH THE LATEST EDITION AND REVISIONS, OF THE ASDHILBED SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SPONS, LUMINAIES, AND TRAFIC SIGNALS, INCLUDING THE LATEST INTERM SPECIFICATIONS, EXCEPT AS MODIFIED HEREIN.

## CONSTRUCTION DRAWINGS AND DETAILS

1. THE FOLLOWING NOTES SHALL BE INCLUDED ON ALL PLANS AND/OR SCHEDULES IN REFERENCE TO ANCHOR BOLTS:
  - a. PREDRILLING OF ALL ANCHOR BOLTS IS REQUIRED, AND SHALL BE ACCORDING TO THE FOLLOWING:
    - i. ANCHOR BOLTS SHALL BE 1/2" DIA. OR LARGER.
    - ii. THE MINIMUM CLEARANCE BETWEEN THE BOTTOM OF THE DRILLING BITS AND THE TOP OF THE CONCRETE IS CRITICAL, AND SHALL NOT EXCEED THE ANCHOR STRENGTH ON THIS DRAWING.
2. THE USE OF READY-MIXED MASS PLASTER SHALL, GENERALLY, NOT BE ALLOWED FOR ANCHOR BOLTS. IF USED, THE ANCHOR BOLTS SHALL BE CONSIDERED TWO CURRING LAGS AND SHALL BE DIRECTLY PROTECTED BY THE ANCHOR BOLTS. ABSOLUTE DRILLING SHALL BE REQUIRED.
3. THE DAMAGING EFFECTS OF UNCONTROLLED JUNCTION ACCESS SHALL NOT BE CONSIDERED IN THE DESIGN OF STRUCTURAL SUPPORTS FOR SINKS AND EQUIPMENT. THE DESIGN OF SUCH SUPPORTS SHALL BE BASED ON THE ASSUMPTION OF AVERAGE CONDITION OF FLOORS TO WHICH SUCH SUPPORTS ARE TO BE ATTACHED. THE DESIGN OF SUCH SUPPORTS SHALL BE APPROVED BY THE DEPARTMENT PRIOR TO FABRICATION OF SUPPORTS.

**TRAFFIC SIGNAL NOTES:**

- [illegible]

## MAINTENANCE AND PROTECTION OF TRAFFIC NOTES

- [illegible]

FEED ROOM COW NO.	STATE	FEDERAL AD PRODUCT NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
1	Ri		2022	4	26

SCALE: NOT TO SCALE

DESIGNED BY:	
CHECKED BY:	
DATE:	
SHEET: 4	
OF: 26	

REVISIONS			REVISIONS		
NO.	DATE	BY	NO.	DATE	BY
1	4/07	TRB	4	12/22	SRP
2	11/07	TRB			
3	3/10	RBH			

# EAST BAY BIKE PATH BRIDGE REPLACEMENT

## (BARRINGTON RIVER AND WARREN BRIDGES)

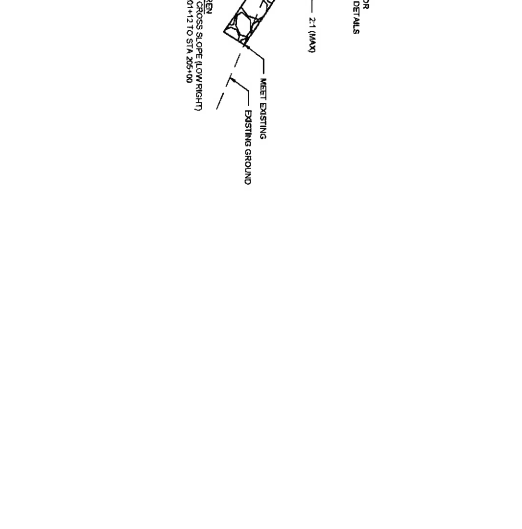
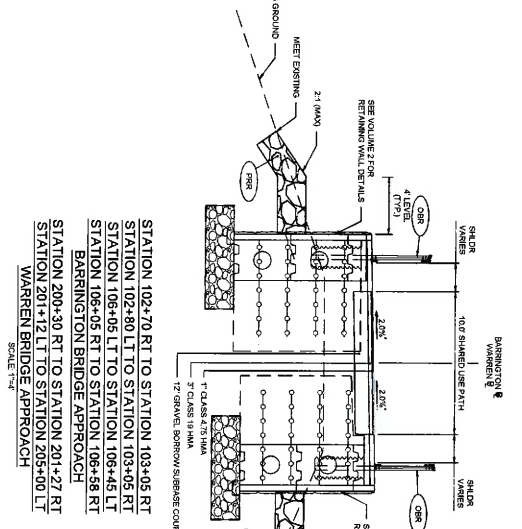
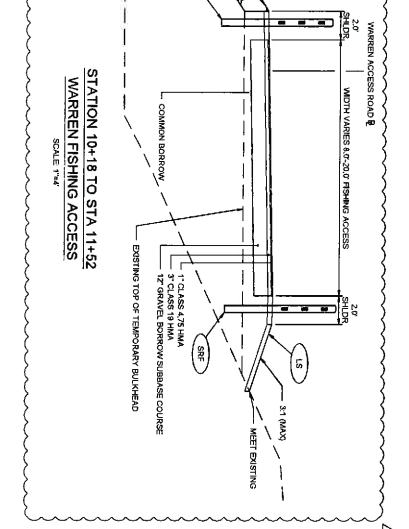
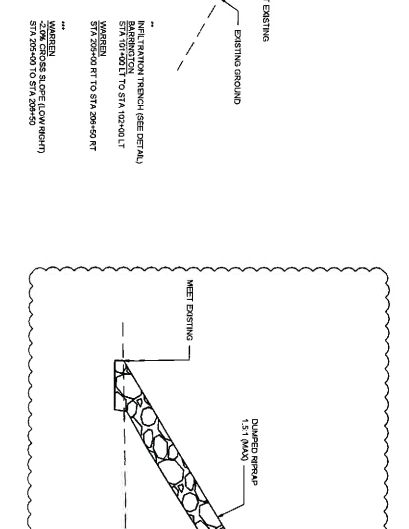
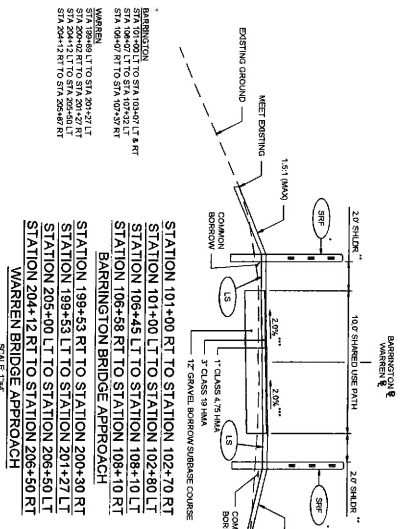
VOLUME 1

## STANDARD NOTES - 2

0188A\_V1\_004\_STDNOTES02



DATE	BY	REVISION	DATE	BY	REVISION
1	NR		2022	6	26



RHODE ISLAND  
DEPARTMENT OF TRANSPORTATION

DESIGNED BY:  
CHECKED BY:  
DATE:  
SHEET 6 OF 26

SCALE	1"=4'
0	4
1	8
2	16
3	24
4	32
5	40
6	48
7	56
8	64
9	72
10	80
11	88
12	96
13	104
14	112
15	120
16	128
17	136
18	144
19	152
20	160
21	168
22	176
23	184
24	192
25	200
26	208
27	216
28	224
29	232
30	240
31	248
32	256
33	264
34	272
35	280
36	288
37	296
38	304
39	312
40	320
41	328
42	336
43	344
44	352
45	360
46	368
47	376
48	384
49	392
50	400
51	408
52	416
53	424
54	432
55	440
56	448
57	456
58	464
59	472
60	480
61	488
62	496
63	504
64	512
65	520
66	528
67	536
68	544
69	552
70	560
71	568
72	576
73	584
74	592
75	600
76	608
77	616
78	624
79	632
80	640
81	648
82	656
83	664
84	672
85	680
86	688
87	696
88	704
89	712
90	720
91	728
92	736
93	744
94	752
95	760
96	768
97	776
98	784
99	792
100	800

EAST BAY BIKE PATH BRIDGE REPLACEMENT  
(BARRINGTON RIVER AND WARREN BRIDGES)  
VOLUME 1  
TYPICAL SECTIONS  
BRIDGE ISLAND

DESIGNED BY:  
CHECKED BY:  
DATE:  
SHEET 6 OF 26

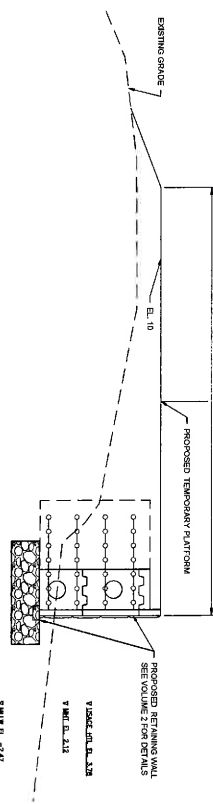
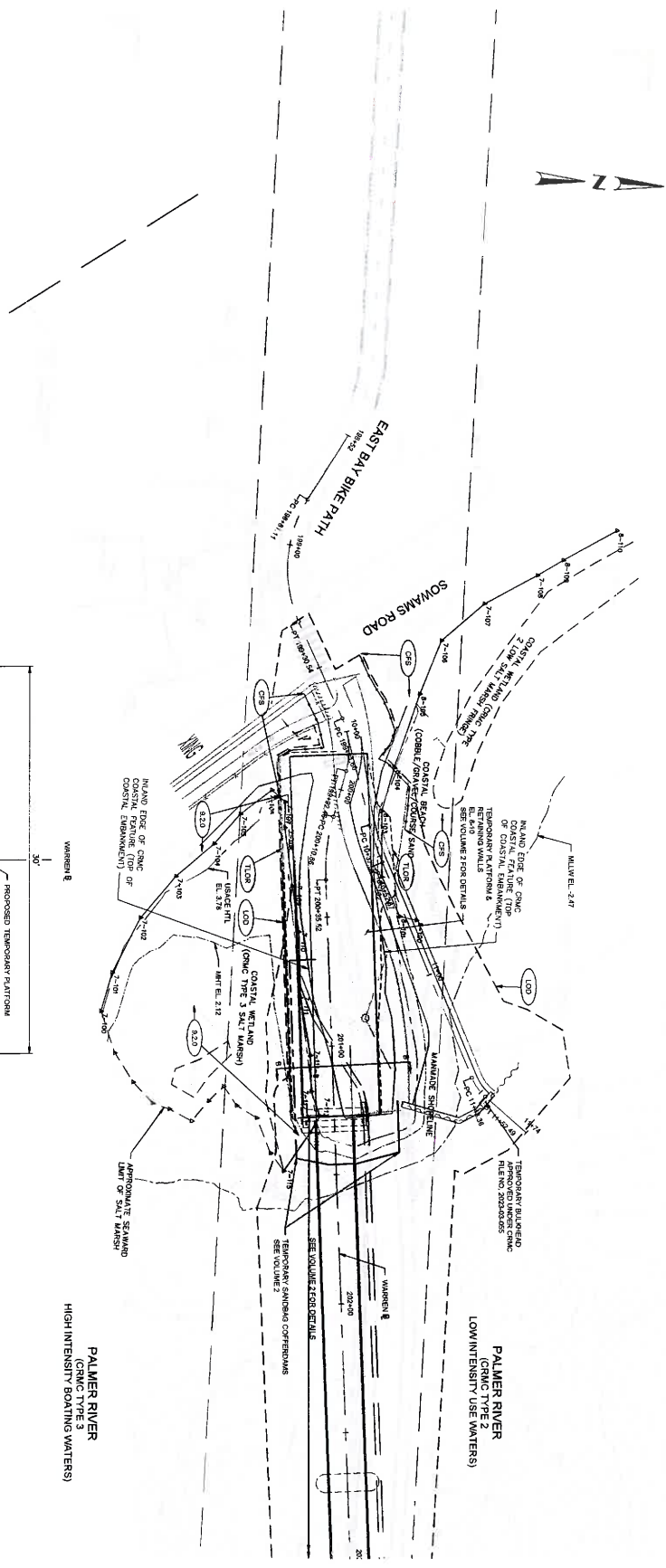
0186A-17-005-179-004







REV	DATE	BY	CHKD	APP'D	DESCRIPTION
1	01/17/22	RI			ISSUED FOR CONSTRUCTION



SECTION B-B  
TEMPORARY PLATFORM  
SCALE: 1"=4'



Aetna Bridge Company



MHB  
1000 Main Street  
Providence, RI 02903  
401.272.1700



RHODE ISLAND  
DEPARTMENT OF TRANSPORTATION

ISSUED BY:  
DATE:  
SCALE: 1"=20'

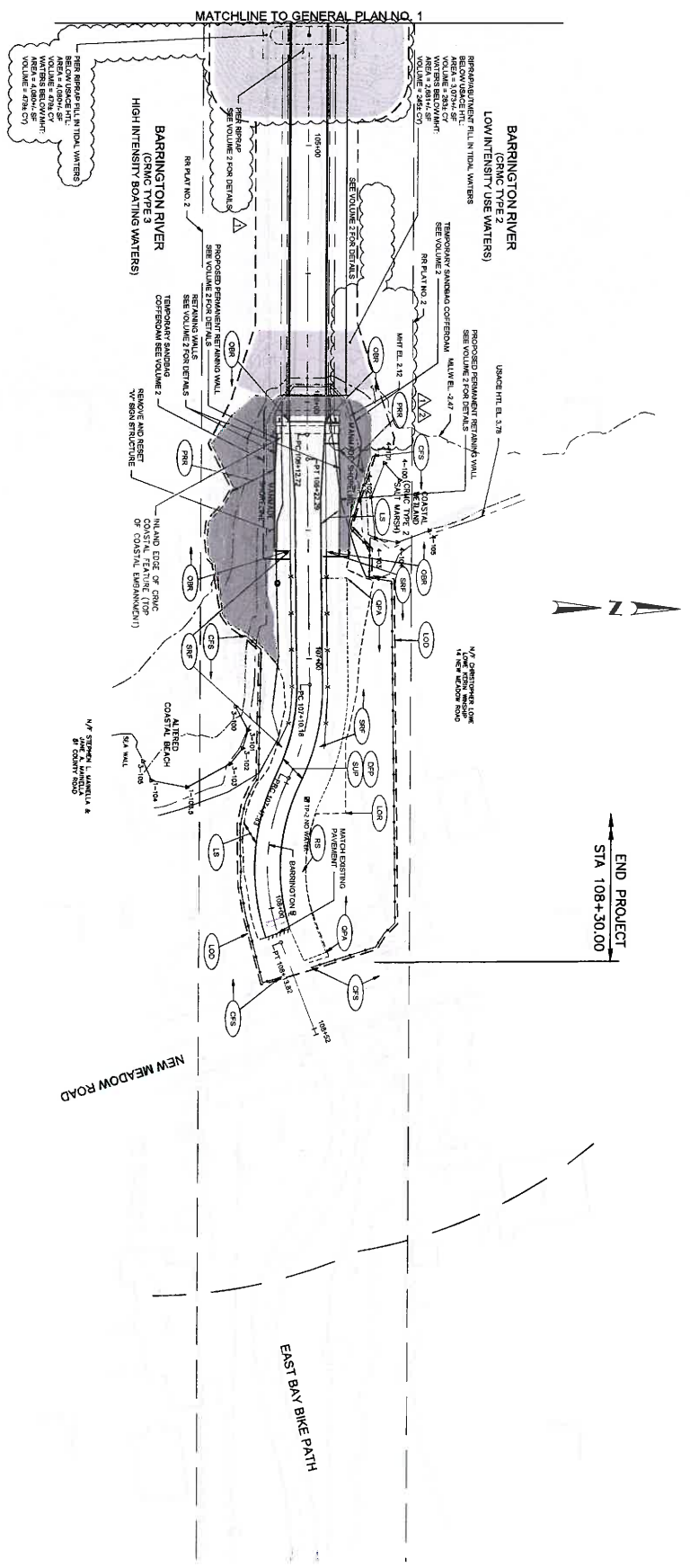
NO.	DATE	BY	CHKD	APP'D	DESCRIPTION
1	01/17/22	RI			ISSUED FOR CONSTRUCTION

EAST BAY BIKE PATH BRIDGE REPLACEMENT  
(BARRINGTON RIVER AND WARREN BRIDGES)  
VOLUME 1  
TEMPORARY WORK PLATFORM  
PLAN AND SECTION NO. 2



018BA\_V1\_009\_GENERAL00

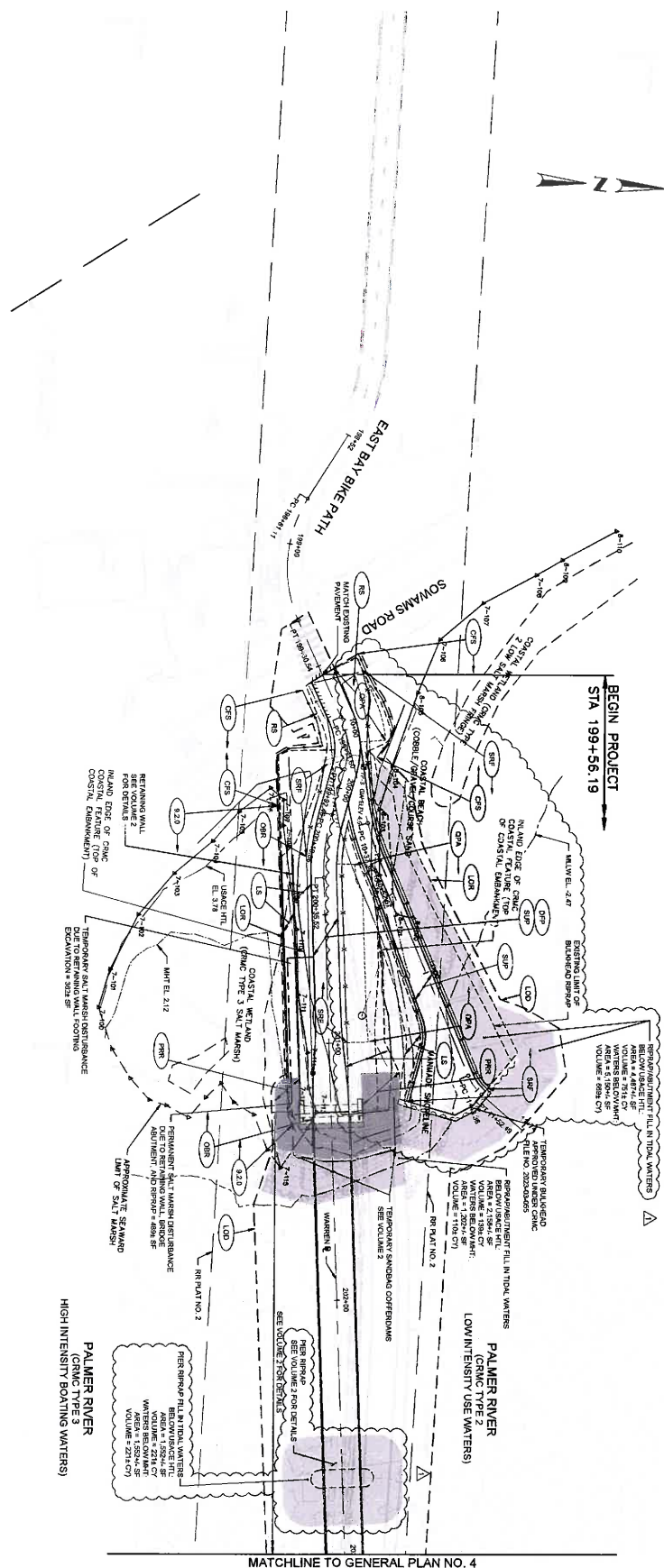
NO.	DATE	ISSUED BY	PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	8/		2022	10	28



PROJECT: EAST BAY BIKE PATH BRIDGE REPLACEMENT  
 LOCATION: BARRINGTON RIVER AND WARREN BRIDGES  
 SHEET: 10 OF 28

NO.	DATE	BY	CHK	APP
1	7/7/24	WBS		
2	7/18/24	WBS		

EAST BAY BIKE PATH BRIDGE REPLACEMENT  
 (BARRINGTON RIVER AND WARREN BRIDGES)  
 VOLUME 1  
 GENERAL PLAN NO. 2



MATCHLINE TO GENERAL PLAN NO. 4

**PALMER RIVER**  
(CRMC TYPE 3)  
HIGH INTENSITY BOATING WATERS



Aetna  
Aetna Bridge Company

 **VHB**  
1 Cedar Street  
Suite 400  
Providence, RI 02903  
401.272.6100

**RI**  
**DOT**

**RHODE ISLAND**  
**DEPARTMENT OF TRANSPORTATION**

MODIFICATIONS SINCE  
OCTOBER 24, 2023  
CRMC FULL COUNCIL  
PUBLIC HEARING.

CHECKED BY: \_\_\_\_\_  
DATE: \_\_\_\_\_  
SHEET: 11

SCALE: 1"=20'

REVISIONS				REVISIONS			
NO.	DATE	BY		NO.	DATE	BY	
1	7/17/74	WAB					

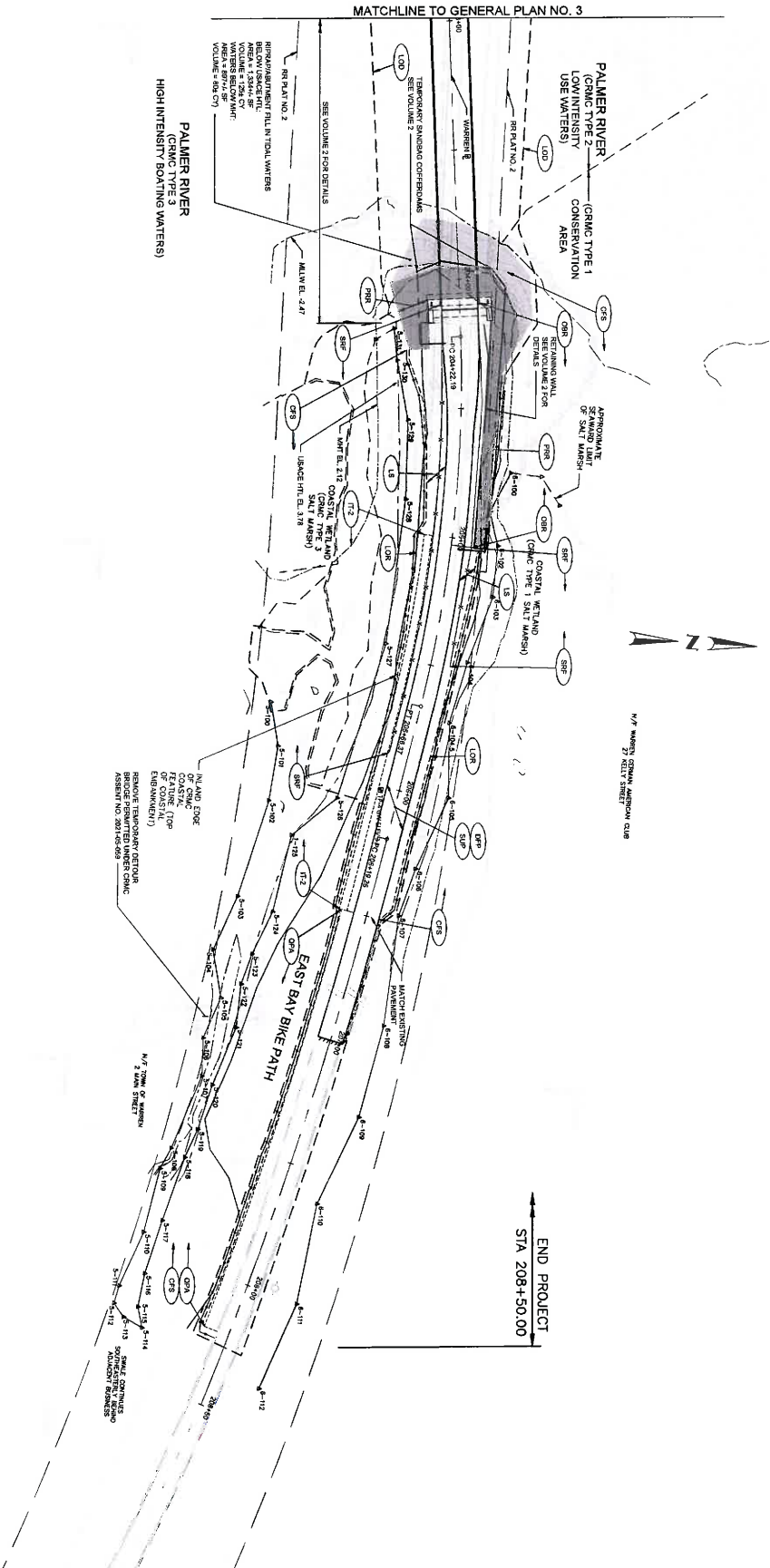
## EAST BAY BIKE PATH BRIDGE REPLACEMENT (BARRINGTON RIVER AND WARREN BRIDGES)

GENERAL PLAN NO. 3

018BA\_V1\_011\_GENERAL.001



NO.	DATE	REVISION	BY	DATE	NO.	DATE	NO.	DATE
1	RI			2022	12			26



Aetna Bridge Company



1000 State Street  
Providence, RI 02903  
401.272.7100



RHODE ISLAND  
DEPARTMENT OF TRANSPORTATION

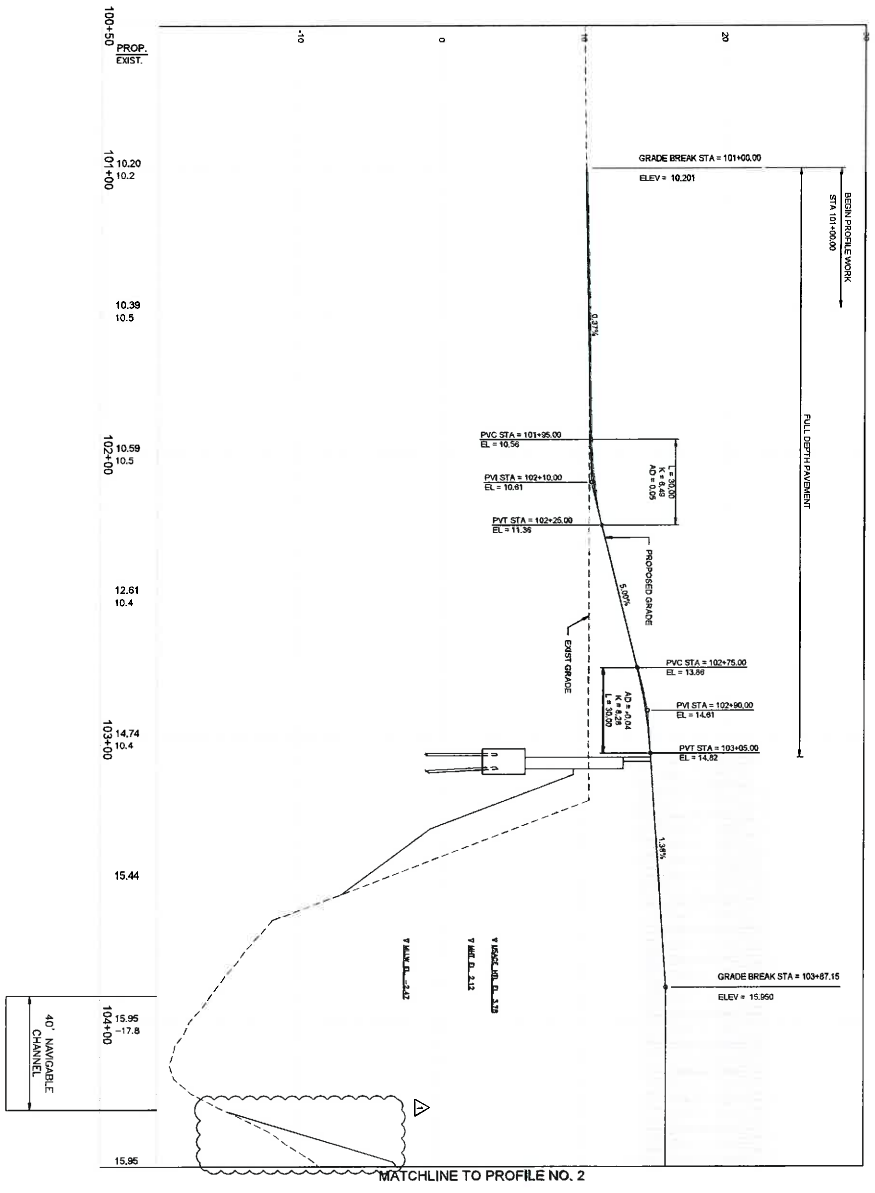
RECORD NO.  
DATE  
SHEET 12  
OF 26

NO.	DATE	BY	NO.	DATE	BY
1	2022	12	1	2022	12

EAST BAY BIKE PATH BRIDGE REPLACEMENT  
(BARRINGTON RIVER AND WARREN BRIDGES)  
VOLUME 1  
GENERAL PLAN NO. 4

PROJECT NO.	SHEET NO.	DATE	BY	CHKD.
2022-13	28			

R-1



Aetna Bridge Company



1000 Street  
Providence, RI 02903  
401.772.7100



RHODE ISLAND  
DEPARTMENT OF TRANSPORTATION



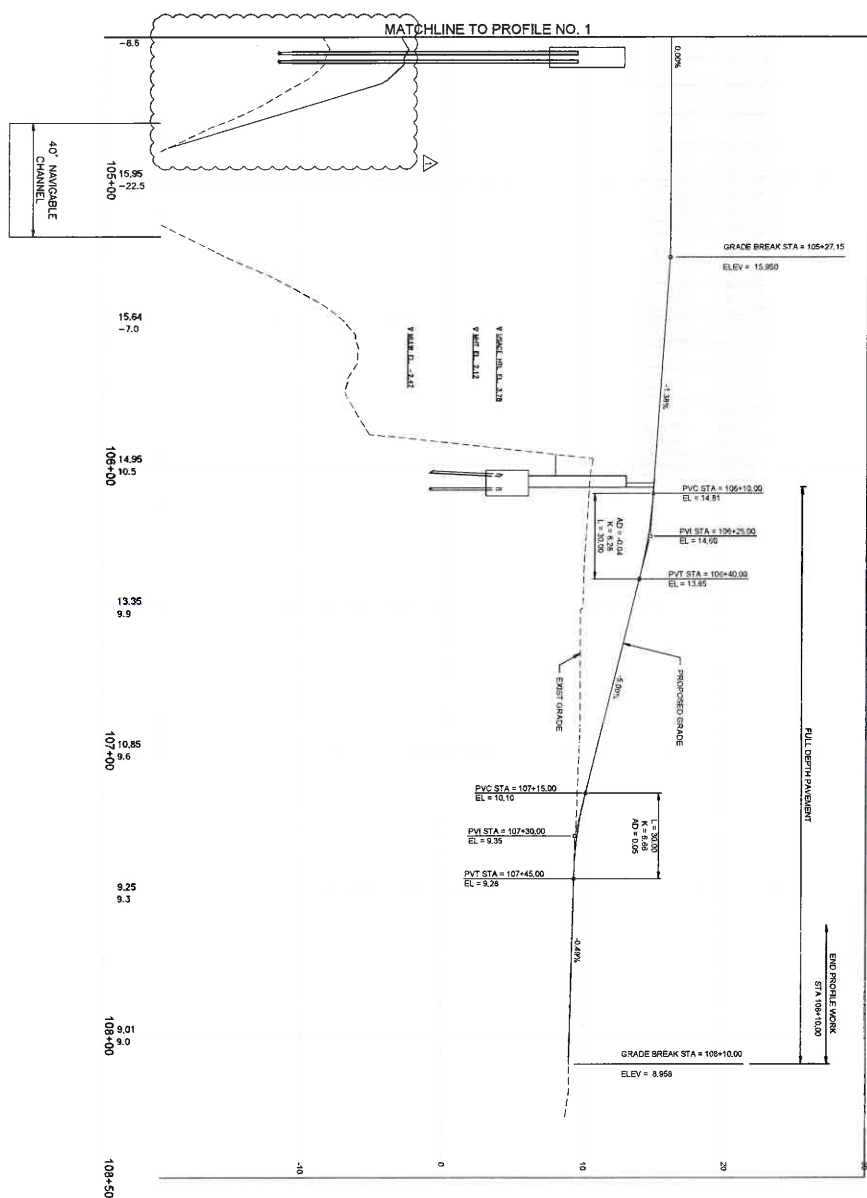
PROJECT NO. 2022-13  
SHEET 28

SCALE	1" = 20' HOR.	1" = 4' VERT.
NO. OF STATIONS	20	20
NO. OF STATIONS	20	20
NO. OF STATIONS	20	20

EAST BAY BIKE PATH BRIDGE REPLACEMENT  
(BARRINGTON RIVER AND WARREN BRIDGES)  
VOLUME 1  
PROFILE NO. 1

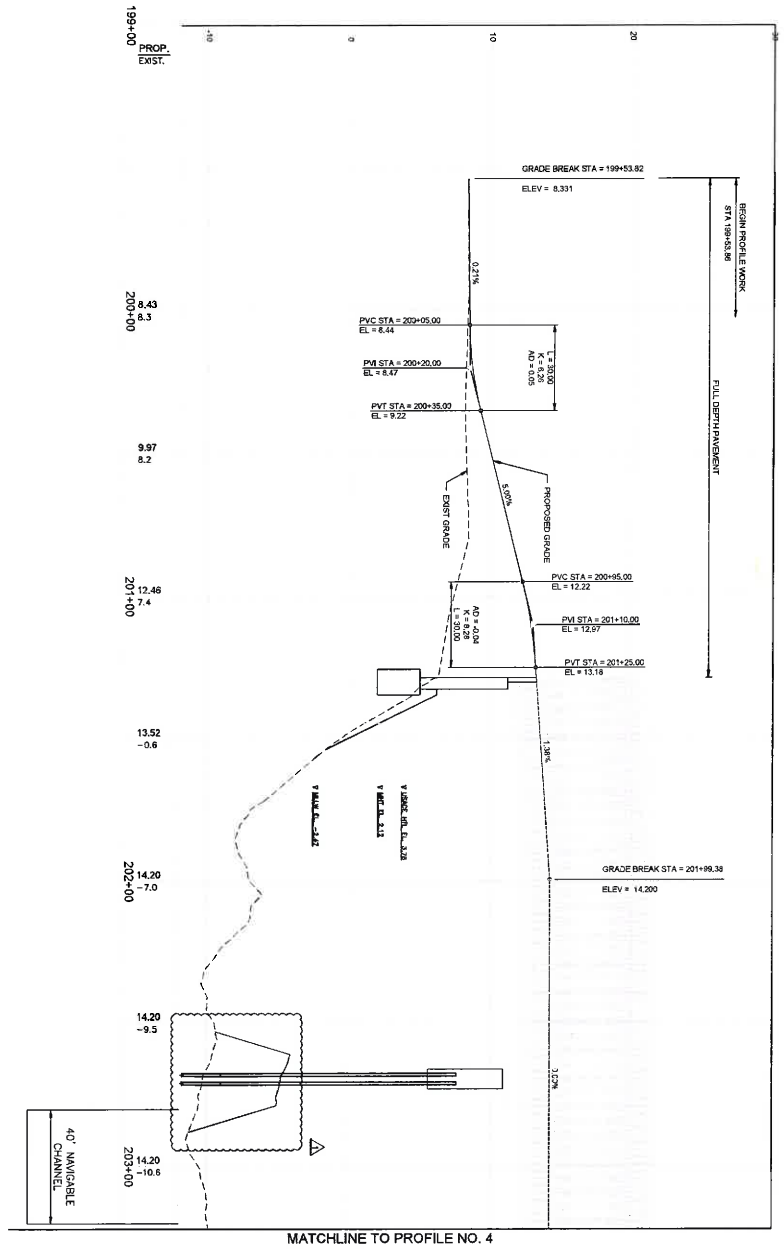


0100A\_V1\_013\_2007 LE20



PROJECT NO.	SHEET NO.	DATE	BY
2022	15	2022	26

R-1



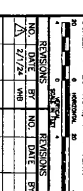
MATCHLINE TO PROFILE NO. 4



RHODE ISLAND  
DEPARTMENT OF TRANSPORTATION

DESIGNED BY: [Name]  
CHECKED BY: [Name]  
DATE: [Date]

SHEET: 15  
OF: 26

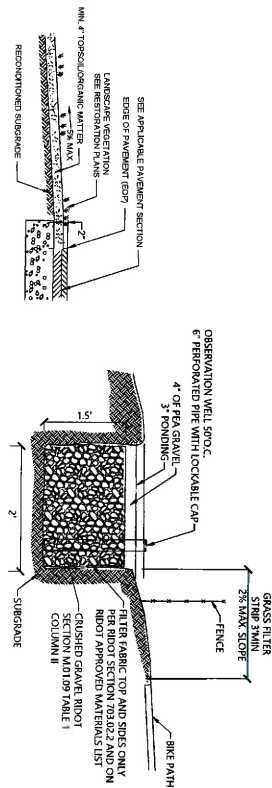
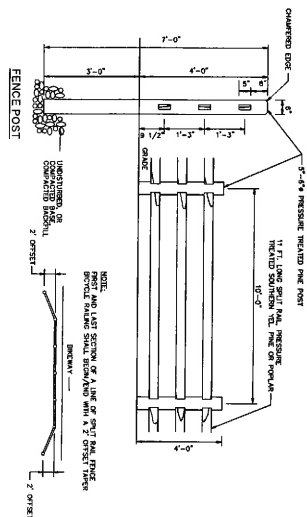


EAST BAY BIKE PATH BRIDGE REPLACEMENT  
(BARNSTON RIVER AND WARREN BRIDGES)  
VOLUME 1  
PROFILE NO. 3

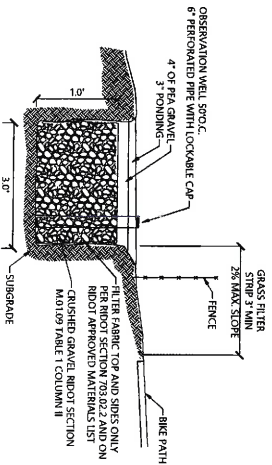
RECEIVED  
4/18/2024







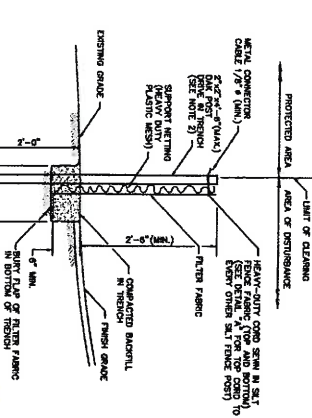
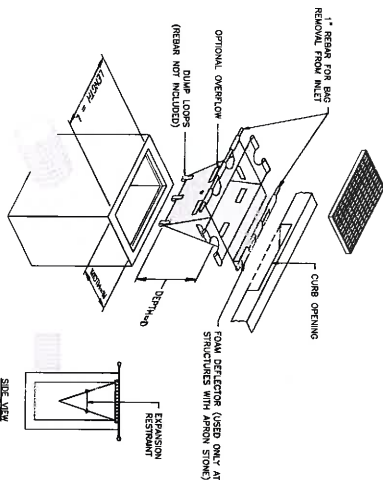
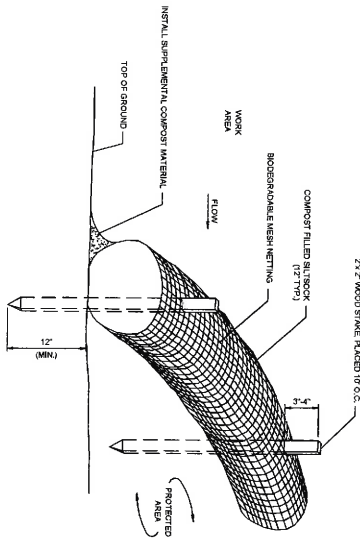
**QUALIFIED PERVIOUS AREA** 



NOT TO SCALE

INFILTRATION TRENCH - 2

112



1. FILTER SOCK SHALL OVERLAP A MINIMUM OF 12 INCHES.
2. FILTER SOCK SHALL BE INSPECTED PERIODICALLY AND AFTER ALL STORM EVENTS AND REPAIR OR REPLACEMENT SHALL BE PERFORMED PROMPTLY AS NEEDED.
3. COMPOST MATERIAL SHALL BE DISPOSED ON SITE, AS DETERMINED BY THE ENGINEER.
4. IF NON-Biodegradable NETTING IS USED THE NETTING SHALL BE COLLECTED AND DISPOSED OF OFF-SITE.

**COMPOST FILTER SOCK** 

1. **INLET SEDIMENT CONTROL DEVICE NOTES:**
  - 1.1. MAINTAIN CATCH BASIN DIMENSIONS AND PROVIDE APPROPRIATELY-SIZED DEVICES PER MANUFACTURER'S REQUIREMENTS.
  2. INSTALL INLET PROTECTION IN CATCH BASIN BEFORE COMMENCING ANY ROADWORK.
  3. GRATE TO BE PLACED OVER INLET PROTECTION.
  4. INLET PROTECTION SHALL BE INSPECTED PERIODICALLY AND SHALL BE REPAIRED OR REPLACED IF PERMANENTLY DAMAGED. DISTURBED AREAS SHALL BE REINSTATED PERMANENTLY STABILIZED WITH TOPSOIL AND GRASS.
2. **INLET SEDIMENT CONTROL DEVICE DETAIL**

**INLET SEDIMENT CONTROL DEVICE DETAIL**

- NOTES:
1. SHALL BE IN ACCORDANCE WITH SECTION 206 OF THE FL STANDARD SPECIFICATIONS.
  2. 2"x4"x8'-(MAX) OUR POSTS FOR SET FENCE SHALL BE (MAX) 1/2" N. W. S. IN RED OAK AND 4'-0" (MAX) AS SHOWN ON PLANS.
  3. 1"x3"x8'-(MAX) POSTS PERMITTED FOR PRE-FABRICATED SET FENCE.
  4. SET FENCE SHALL BE INSTALLED BEFORE ANY GRUBBING OR EARTH OBTAINATION TAKES PLACE.
- 
- SET FENCE
- POST
- DO NOT LOPE
- ROAD
- RAILROAD
- DETAIL "A"

**SILT FENCE DETAIL** **9.20**  
**NOT TO SCALE**



1044-2244  
Aetna Bridge Company  
200  
Providence, RI 02903  
401.727.2100



RHODE ISLAND  
DEPARTMENT OF TRANSPORTATION

RESTORATION PLAN NO. 1

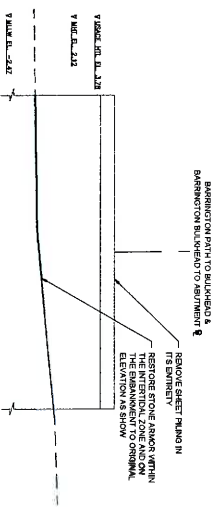
RESTORATION PLAN NO. 1

SECTION	DATE	BY	CHK	DATE	BY	CHK
NO. 1	4/18/24	MB	ST			

EAST BAY BIKE PATH BRIDGE REPLACEMENT  
(BARRINGTON RIVER AND VARRIN BRIDGES)  
VOLUME 1  
RHOE ISLAND  
RESTORATION PLAN NO. 1

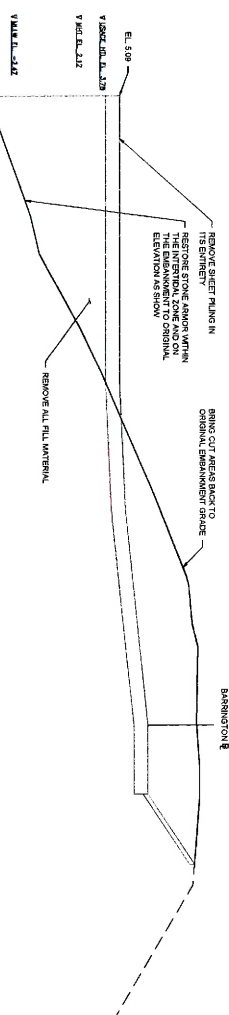
### SECTION A-A BULKHEAD

SCALE: 1"=4'



### SECTION B-B BULKHEAD

SCALE: 1"=4'



NO.	SYMBOL	DESCRIPTION	PLANT LIST	PLANT LIST	PLANT LIST
1	AC	AMERICAN SHADBLU	BBB	4' MIN.	
2	IV	EASTERN RED CEDAR	BBB	4' MIN.	
3	NP	NORTHERN BAYBERRY	BBB	3' MIN.	
4	IS	INHERENT HOLLY	BBB	3' MIN.	
5	CS	GRAY DOGWOOD	BBB	3' MIN.	

#### RESTORATION PLANT LIST

1. REMOVE SHEET PILING IN ITS ENTIRETY AFTER REMOVING INTERIOR FILL.
2. REMOVE FILL MATERIAL AND BRING CUT AREA BACK TO ORIGINAL ELEVATION AS SHOWN.
3. RESTORE STONE ANKER WITHIN THE INTERIOR ZONE AND ON THE ELEVATION AS SHOWN.
4. REGRADE, LOAN AND SEED EMBANKMENT FROM EMBANKMENT STONE UP TO SHEET PILING.
5. INSTALL TREE AND SHRUB PLANTINGS AS SHOWN.

#### RESTORATION NOTES

INLAND EDGE OF CRMC COASTAL FEATURE (TOP OF COASTAL EMBANKMENT)

MANMADE SHOULDER

BARRINGTON RIVER  
(CRMC TYPE 2  
HIGH INTENSITY BOATING WATERS)

EAST BAY BIKE PATH

INLAND EDGE OF CRMC COASTAL FEATURE (TOP OF COASTAL EMBANKMENT)

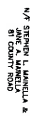
MANMADE SHOULDER

BARRINGTON RIVER  
(CRMC TYPE 2  
LOW INTENSITY USE WATERS)

TRANSPORTATION PLANNING  
APPROVED UNDER CRMC  
FILE NO. 2023-03-055  
DATE: 4/18/24  
SCALE: 1"=4'

R-1

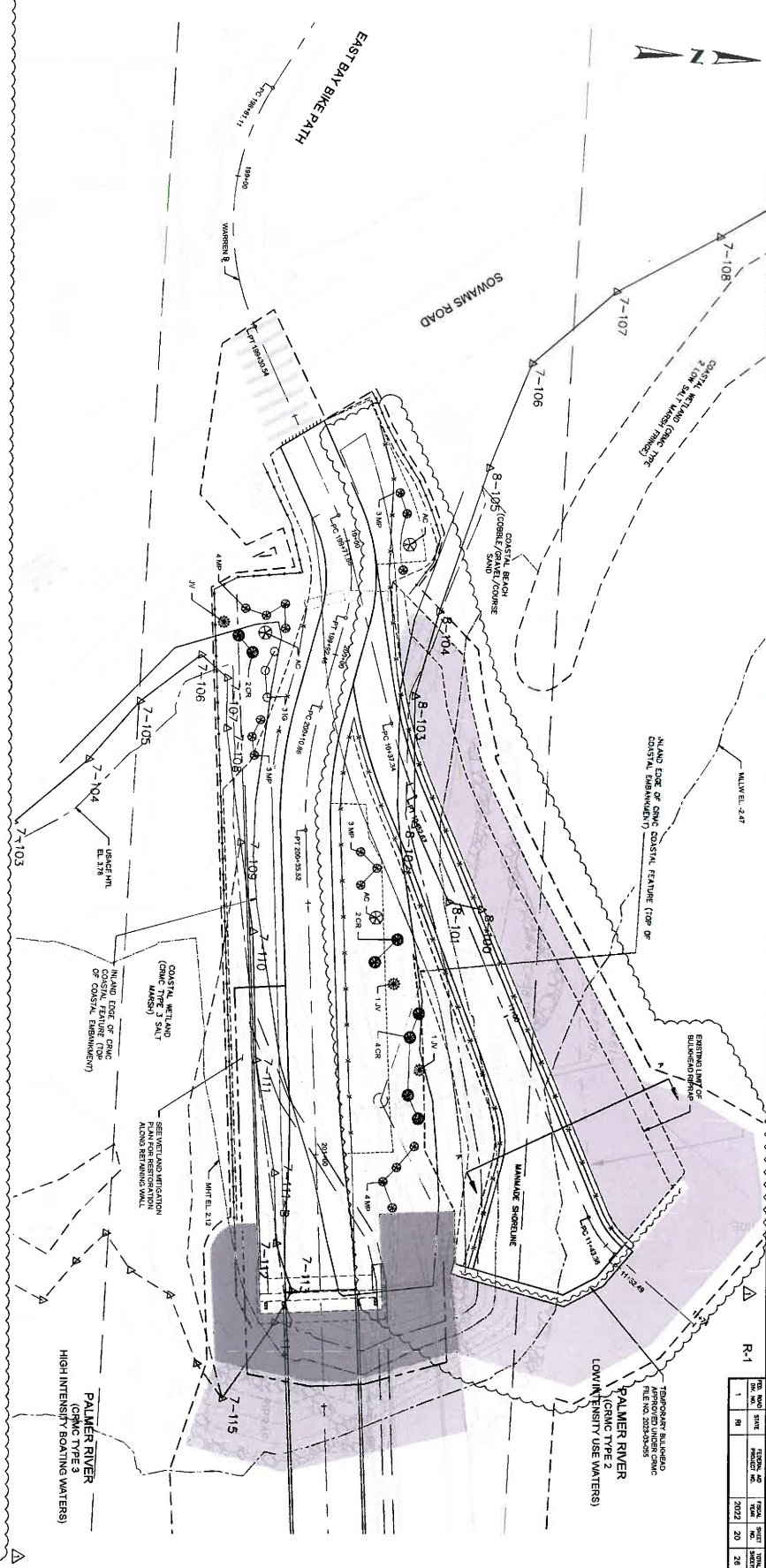
NO.	DATE	BY	CHK	DATE	BY	CHK
1	4/18/24	MB	ST	2024	18	28

N/F STEPHEN L. MANELLA &  
JAYE A. MANFIA

EAST BAY BIKE PATH BRIDGE REPLACEMENT  
(BARRINGTON RIVER AND WARREN BRIDGES)  
VOLUME 1  
BARRINGTON WARREN  
RHODE ISLAND  
RESTORATION PLAN NO. 2

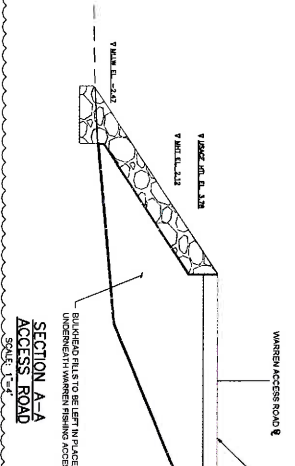


NO.	DATE	BY	REVISION	NO.	DATE	BY	REVISION
1	08/20/2021	MR	100% AD	2	08/20/2021	MR	100% AD
3	08/20/2021	MR	100% AD	4	08/20/2021	MR	100% AD



**RESTORATION PLANT LIST**

AC	AMERICAN SHADBLOW	8A8	4' MIN.
3	(AMERICAN SHADBLOW)	8A8	4' MIN.
3	EASTERN RED CEDAR	8A8	4' MIN.
17	(AMERICAN SHADBLOW)	8A8	4' MIN.
3	NORTHERN SIBIRIAN	8A8	4' MIN.
15	(AMERICAN SHADBLOW)	8A8	4' MIN.
3	INHERENT HOLY	8A8	4' MIN.
8	GRAY DOGWOOD	8A8	4' MIN.



- RESTORATION NOTES**
1. SHEET MUST BE LEFT IN PLACE.
  2. NON-APPROPRIATE PLANTS TO BE LEFT IN PLACE.
  3. RESERVE, LOAN, AND SEED AREA BETWEEN WARREN BIKE PATH AND WARREN FISHING ACCESS.
  4. RESERVE, LOAN, AND SEED SHOULDER BETWEEN EMBAKMENT CREST AND PAVEMENT OF WARREN FISHING ACCESS.
  5. INSTALL TREE AND SHRUB PLANTINGS AS SHOWN.



**RHODE ISLAND  
DEPARTMENT OF TRANSPORTATION**



**EAST BAY BIKE PATH REPLACEMENT  
(BARRINGTON RIVER AND WARREN RIBBONS)**

**RESTORATION PLAN NO. 3**

**VOLUME 1**

**BRIDGE ISLAND**

**0186A.LV\_000 RESTORATION**

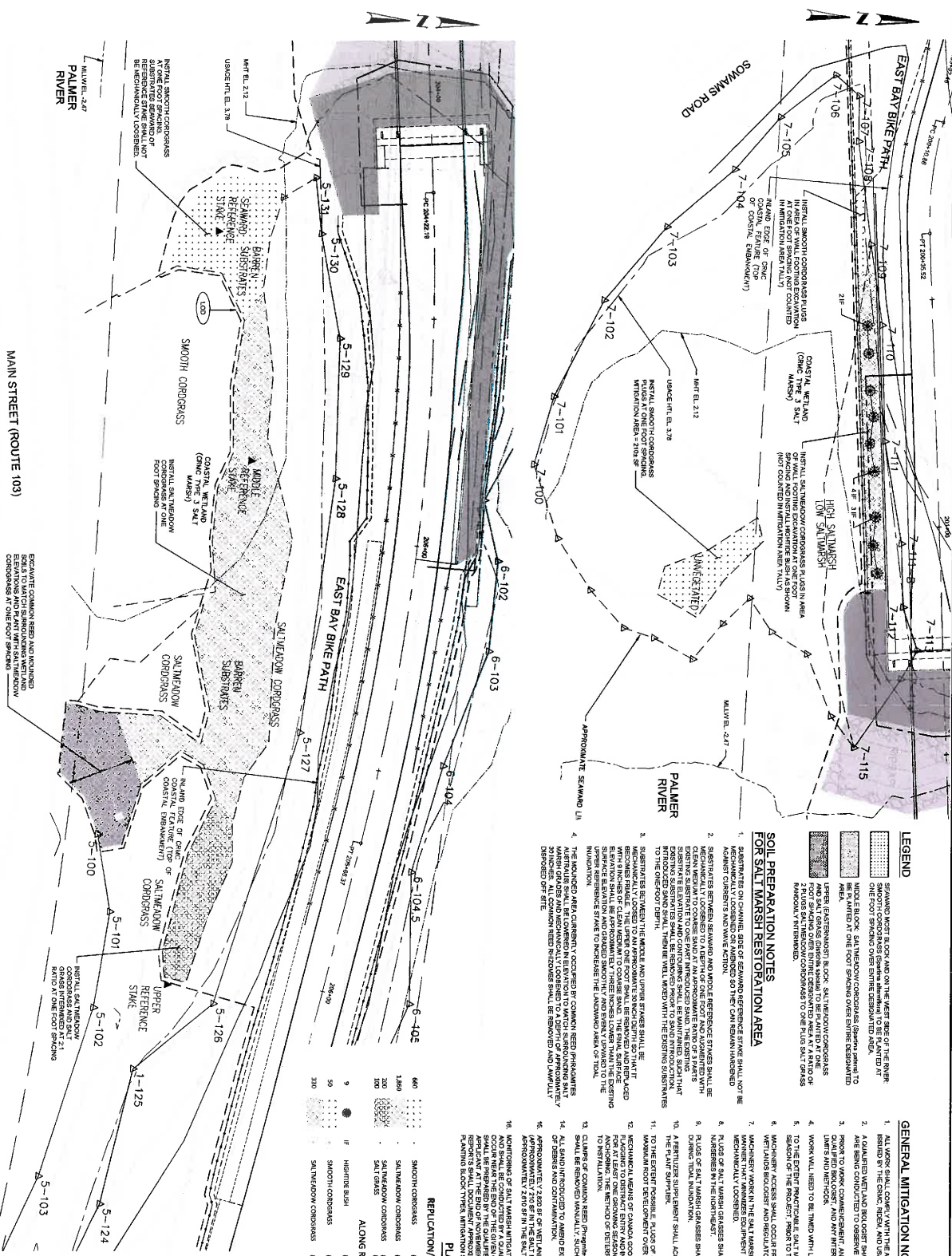


RHODE ISLAND  
DEPARTMENT OF TRANSPORTATION

DESIGNED BY:  
DATE: 05/21/24  
SHEET: 21 OF 26

NO.	DATE	BY	NO.	DATE	BY
1	05/21/24	ST	2	05/21/24	ST
3	05/21/24	ST	4	05/21/24	ST
5	05/21/24	ST	6	05/21/24	ST
7	05/21/24	ST	8	05/21/24	ST
9	05/21/24	ST	10	05/21/24	ST
11	05/21/24	ST	12	05/21/24	ST
13	05/21/24	ST	14	05/21/24	ST
15	05/21/24	ST	16	05/21/24	ST
17	05/21/24	ST	18	05/21/24	ST
19	05/21/24	ST	20	05/21/24	ST
21	05/21/24	ST	22	05/21/24	ST
23	05/21/24	ST	24	05/21/24	ST
25	05/21/24	ST	26	05/21/24	ST

EAST BAY BIKE PATH BRIDGE REPLACEMENT  
(BARRINGTON RIVER AND WARREN BRIDGES)  
WETLAND MITIGATION PLAN  
VOLUME 1  
SHEET 21 OF 26



LEGEND

- SMOOTH CORPORA (SOWAMUS ROAD) - SOWAMUS ROAD
- SMOOTH CORPORA (PALMER RIVER) - PALMER RIVER
- SMOOTH CORPORA (EAST BAY BIKE PATH) - EAST BAY BIKE PATH
- SMOOTH CORPORA (SOWAMUS ROAD) - SOWAMUS ROAD
- SMOOTH CORPORA (PALMER RIVER) - PALMER RIVER
- SMOOTH CORPORA (EAST BAY BIKE PATH) - EAST BAY BIKE PATH

SOIL PREPARATION NOTES FOR SALT MARSH RESTORATION AREA

1. SUBSTRATE ON EXISTING SLOPE OF SOWAMUS ROAD SHALL NOT BE AGAINST CURBS AND WAVE ACTION.
2. SUBSTRATE TO BE REMOVED TO ONE FOOT AND ADJUSTED WITH EXISTING SUBSTRATE TO ONE PART INTRODUCTION. THE EXISTING SUBSTRATE SHALL BE REMOVED PRIOR TO SOWAMUS ROAD TO THE EXISTING SLOPE.
3. SUBSTRATE TO BE REMOVED TO ONE FOOT AND ADJUSTED WITH EXISTING SUBSTRATE TO ONE PART INTRODUCTION. THE EXISTING SUBSTRATE SHALL BE REMOVED PRIOR TO SOWAMUS ROAD TO THE EXISTING SLOPE.
4. THE EXISTING SUBSTRATE SHALL BE REMOVED PRIOR TO SOWAMUS ROAD TO THE EXISTING SLOPE.

GENERAL MITIGATION NOTES

1. ALL WORK SHALL COMPLY WITH THE APPLICABLE STATE AND FEDERAL REGULATIONS AND ALL PERMITS AND PERMIT CONDITIONS ISSUED BY THE STATE, FEDERAL AND LOCAL AGENCIES.
2. A QUALIFIED WETLAND AND SLOPE SHALL BE PRESENT AT LEAST PART TIME DURING MITIGATION ACTIVITIES.
3. PRIOR TO WORK COMMENCEMENT, AN ON-SITE MEETING SHALL BE HELD WITH THE CONTRACTOR, LANDS AND WATERS, AND ANY OTHER AGENCIES INVOLVED IN THE MITIGATION PROCESS TO REVIEW WORK PLANS AND METHODS.
4. WORK SHALL BE DONE WITH LOW FLOW TIDES.
5. TO THE EXTENT POSSIBLE, SALT MARSH MITIGATION SHALL OCCUR DURING THE FIRST FULL GROWING SEASON OF THE PROJECT. PRIOR TO THE PROPOSED SALT MARSH MITIGATION.
6. MITIGATION ACCESS SHALL OCCUR FROM THE BAY PATH AT A LOCATION ACCEPTABLE TO THE QUALIFIED WETLAND AND SLOPE CONSULTANT.
7. MITIGATION WORK SHALL OCCUR SEPARATELY FROM SOWAMUS ROAD TO AVOID DAMAGE TO EXISTING SLOPE AND SLOPE.
8. MITIGATION WORK SHALL OCCUR SEPARATELY FROM SOWAMUS ROAD TO AVOID DAMAGE TO EXISTING SLOPE AND SLOPE.
9. MITIGATION WORK SHALL OCCUR SEPARATELY FROM SOWAMUS ROAD TO AVOID DAMAGE TO EXISTING SLOPE AND SLOPE.
10. MITIGATION WORK SHALL OCCUR SEPARATELY FROM SOWAMUS ROAD TO AVOID DAMAGE TO EXISTING SLOPE AND SLOPE.
11. MITIGATION WORK SHALL OCCUR SEPARATELY FROM SOWAMUS ROAD TO AVOID DAMAGE TO EXISTING SLOPE AND SLOPE.
12. MITIGATION WORK SHALL OCCUR SEPARATELY FROM SOWAMUS ROAD TO AVOID DAMAGE TO EXISTING SLOPE AND SLOPE.
13. MITIGATION WORK SHALL OCCUR SEPARATELY FROM SOWAMUS ROAD TO AVOID DAMAGE TO EXISTING SLOPE AND SLOPE.
14. MITIGATION WORK SHALL OCCUR SEPARATELY FROM SOWAMUS ROAD TO AVOID DAMAGE TO EXISTING SLOPE AND SLOPE.
15. MITIGATION WORK SHALL OCCUR SEPARATELY FROM SOWAMUS ROAD TO AVOID DAMAGE TO EXISTING SLOPE AND SLOPE.
16. MITIGATION WORK SHALL OCCUR SEPARATELY FROM SOWAMUS ROAD TO AVOID DAMAGE TO EXISTING SLOPE AND SLOPE.

REPLICATION/RESTORATION AREAS

NO.	DATE	BY	NO.	DATE	BY
1	05/21/24	ST	2	05/21/24	ST
3	05/21/24	ST	4	05/21/24	ST
5	05/21/24	ST	6	05/21/24	ST
7	05/21/24	ST	8	05/21/24	ST
9	05/21/24	ST	10	05/21/24	ST
11	05/21/24	ST	12	05/21/24	ST
13	05/21/24	ST	14	05/21/24	ST
15	05/21/24	ST	16	05/21/24	ST
17	05/21/24	ST	18	05/21/24	ST
19	05/21/24	ST	20	05/21/24	ST
21	05/21/24	ST	22	05/21/24	ST
23	05/21/24	ST	24	05/21/24	ST
25	05/21/24	ST	26	05/21/24	ST

PLANT LIST

NO.	DATE	BY	NO.	DATE	BY
1	05/21/24	ST	2	05/21/24	ST
3	05/21/24	ST	4	05/21/24	ST
5	05/21/24	ST	6	05/21/24	ST
7	05/21/24	ST	8	05/21/24	ST
9	05/21/24	ST	10	05/21/24	ST
11	05/21/24	ST	12	05/21/24	ST
13	05/21/24	ST	14	05/21/24	ST
15	05/21/24	ST	16	05/21/24	ST
17	05/21/24	ST	18	05/21/24	ST
19	05/21/24	ST	20	05/21/24	ST
21	05/21/24	ST	22	05/21/24	ST
23	05/21/24	ST	24	05/21/24	ST
25	05/21/24	ST	26	05/21/24	ST



**RHODE ISLAND**  
**DEPARTMENT OF TRANSPORTATION**

DESIGNED BY:  
CHECKED BY:DATE: \_\_\_\_\_  
SHEET: 22

SCALE: 1"=4'

4 0 4 0

PALE M EXE

[illegible]

# EAST BAY BIKE PATH BRIDGE REPLACEMENT

(BARRINGTON RIVER AND WARREN BRIDGES)

ARRINGTON WARREN

VOLUME 1

RHODE ISLAND

## CROSS SECTIONS

# INDEX

Sheet Number	Sheet Description
1	COVER
2	LIST OF ABBREVIATIONS AND LEGEND
3	JOB SPECIFIC GENERAL NOTES 1
4	JOB SPECIFIC GENERAL NOTES 2
5	JOB SPECIFIC GENERAL NOTES 3
6	JOB SPECIFIC GENERAL NOTES 4
7	BR. 083751 GENERAL PLAN
8	BR. 083751 BRIDGE TYPICAL SECTION
9	BR. 083751 PROFILE
10	BR. 083751 FOUNDATION PLAN
11	BR. 083751 WEST ABUTMENT PLAN AND ELEVATION
12	BR. 083751 EAST ABUTMENT PLAN AND ELEVATION
13	BR. 083751 ABUTMENT DETAILS
14	BR. 083751 RETAINING WALL PLAN 1
15	BR. 083751 RETAINING WALL PLAN 2
16	BR. 083751 RETAINING WALL PLAN 3
17	BR. 083751 RETAINING WALL PLAN 4
18	BR. 083751 PIER DETAILS
19	BR. 083751 FISHING PIER GENERAL PLAN
20	BR. 083751 FISHING PIER ELEVATION
21	BR. 083751 FISHING PIER TYPICAL SECTION
22	BR. 083851 BRIDGE GENERAL PLAN
23	BR. 083851 BRIDGE TYPICAL SECTION
24	BR. 083851 BRIDGE PROFILE
25	BR. 083851 FOUNDATION PLAN
26	BR. 083851 WEST ABUTMENT PLAN AND ELEVATION
27	BR. 083851 EAST ABUTMENT PLAN AND ELEVATION
28	BR. 083851 ABUTMENT DETAILS
29	BR. 083851 RETAINING WALL PLAN 1
30	BR. 083851 RETAINING WALL PLAN 2
31	BR. 083851 PIER DETAILS
32	BR. 083851 FISHING PIER GENERAL PLAN
33	BR. 083751 & 083851 RIP RAP DETAILS 1
34	BR. 083751 & 083851 RIP RAP DETAILS 2
35	BR. 083751 & 083851 BRIDGE RAIL DETAILS 1
36	BR. 083751 & 083851 RAIL DETAILS 2
37	BR. 083751 & 083851 RAIL DETAILS 3
38	BR. 083751 & 083851 RAIL DETAILS 4

STATE OF RHODE ISLAND



## DEPARTMENT OF TRANSPORTATION

### PLAN, PROFILE AND SECTIONS OF PROPOSED EAST BAY BIKE PATH BRIDGE REPLACEMENTS BRIDGE NOS. 083751 & 083851 RECONSTRUCTION PLANS ENVIRONMENTAL PERMITTING SET VOLUME 2

EAST BAY BIKE PATH OVER BARRINGTON RIVER AND PALMER RIVER

TOWNS OF BARRINGTON AND WARREN  
COUNTY OF BRISTOL

R.I. CONTRACT NO. 2022-DB-012 F.A. PROJECT NO. BRO-0838(002)



LAYOUT PLAN  
SCALE 1"=120'

#### R.I. STANDARD SPECIFICATIONS AND STANDARD DETAILS

SPECIFICATIONS TO GOVERN THIS PROJECT ARE THE R.I. STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, MARCH 2016, WITH ALL REVISIONS AND THE STATE AND FEDERAL SPECIAL PROVISIONS INCLUDED IN THE CONTRACT DOCUMENTS.

STANDARD DETAILS FOR THIS PROJECT ARE R.I. STANDARD DETAILS, 1988 EDITION, WITH ALL REVISIONS.



BASE OF LEVELS  
NAVD 88  
NAD 83 (2011)



Contract Number \_\_\_\_\_  
Number of Sheet \_\_\_\_\_  
Total Sheets \_\_\_\_\_





## LIST OF ABBREVIATIONS

ABANDONED	= ABD.	J	JOINT	= JT.	W	WEARING SURFACE	= W.S.
ABUTMENT	= ABUT.	L	LEFT	= LT.	W	WELED WIRE FABRIC	= W.W.F.
ADDITIONAL	= ADD'L	LG	LENGTH	= LG.	W	WEST	= W.
ALTERNATE	= ALT.	LTG	LIGHTING	= LTG.	WB	WESTBOUND	= WB
AMERICAN ASSOCIATION OF STATE HIGHWAY AND TRANSPORTATION OFFICIALS	= AASHTO	LONG	LONG	= LG.	W	WITH	= W.
ANCHOR BOLT	= A.B.	LONGITUDINAL	LONGITUDINAL	= LONGIT.	W	WORK FLANGE	= W.F.
AMERICAN PETROLEUM	= API	M	MEAN	= MATL.	W	WORKING POINT	= W.P.
APPROVED	= APPD.	MAXIMUM	MEAN HIGH TIDE	= MAX.			
APPROXIMATE	= APPROX.	M.H.T.	MEAN HIGH WATER	= M.H.T.			
AVERAGE	= AVG.	M.H.W.	MEAN LOW LOWER WATER	= M.L.L.W.			
B	= B	M.L.W.	MEAN LOW WATER	= M.L.W.			
BASELINE OF CONSTRUCTION	= B	M.S.L.	MEAN SEA LEVEL	= M.S.L.			
BACK TO BACK	= B TO B	MINIMUM	MINIMUM	= MIN.			
BEAM	= BM.	MISCELLANEOUS	MISCELLANEOUS	= MISC.			
BEARING	= BRG.	N	N	= N			
BEND POINT	= B.P.	NEAR FACE	NEAR FACE	= N.F.			
BETWEEN	= BTWN	NEAR SIDE	NEAR SIDE	= N.S.			
BIFURCIOUS	= BIT.	NORTH	NORTH	= N			
BIFURCIOUS COATED	= B.C.	NORTHBOUND	NORTHBOUND	= NB, N.B.			
CORRUGATED METAL PIPE	= B.C.C.M.P.	NOT IN CONTRACT	NOT IN CONTRACT	= N.I.C.			
BUILDING	= BLDG.	NOT TO SCALE	NOT TO SCALE	= N.T.S.			
BUILDING LINE	= B.L.	NUMBER	NUMBER	= NO.			
BOLT CIRCLE	= B.C.	O	O	= O			
BOTH SIDES	= B.S.	ON CENTER	ON CENTER	= O.C.			
BOTTOM	= BOT.	OPENING	OPENING	= OPNG.			
BOTTOM OF FOOTING	= B.O.F.	OPPOSITE	OPPOSITE	= OPP.			
C	= C	OCCUPATIONAL SAFETY & HEALTH ADMINISTRATION	OCCUPATIONAL SAFETY & HEALTH ADMINISTRATION	= OSHA			
CAST IN PLACE	= C.I.P.	OUTSIDE DIAMETER	OUTSIDE DIAMETER	= O.D.			
CENTER TO CENTER	= C TO C, C/C	OPTIONAL	OPTIONAL	= OPT.			
COLLECTOR/DISTRIBUTOR	= C/D	OVERHEAD WIRE	OVERHEAD WIRE	= O.H.W.			
CENTERLINE	= CL	P	P	= P			
CIRCLE	= CIR.	PARALLEL	PARALLEL	= PRL			
CONTROLLED LOW STRENGTH MATERIAL	= CLSM	PEDESTRIAN	PEDESTRIAN	= PED.			
CLEARANCE	= CLR.	PLATE	PLATE	= P			
COLUMN	= COL.	POINT OF VERTICAL CURVATURE	POINT OF VERTICAL CURVATURE	= P.V.C.			
CONCRETE	= CONC.	POINT OF VERTICAL TANGENCY	POINT OF VERTICAL TANGENCY	= P.V.T.			
CONDUIT	= COND.	POINT	POINT	= PT.			
CONNECTION	= CONN.	POINT OF CURVATURE	POINT OF CURVATURE	= PC			
CONSTRUCTION	= CONST.	POINT OF TANGENCY	POINT OF TANGENCY	= PT			
CONTINUOUS	= CONT.	POLYVINYL CHLORIDE	POLYVINYL CHLORIDE	= PVC			
CONTRACTOR	= CONTR.	POLYMER PER SQUARE INCH	POLYMER PER SQUARE INCH	= P.S.I.			
CORRUGATED METAL PIPE	= CMP	PRECAST CONCRETE INSTITUTE	PRECAST CONCRETE INSTITUTE	= PCI			
COUNTERSINK	= CSK.	PRECAST	PRECAST	= P/C			
COUNTERSINK	= C.S.K.	PRESTRESSED	PRESTRESSED	= P/S			
COURTLING	= CL	PROFILE GRADE LINE	PROFILE GRADE LINE	= PGL			
DETAIL	= DET.	PROPOSED	PROPOSED	= PROP.			
DIAGONAL	= DIAG.	R	R	= R			
DIAPHRAGM	= DIAPHM.	RADIUS	RADIUS	= RAD., R			
DIAMETER	= DIA.	RAILROAD	RAILROAD	= RR.			
DIMENSION	= DIM.	REHABILITATION	REHABILITATION	= REHAB.			
DRAIN	= DRN.	REINFORCED CONCRETE PIPE	REINFORCED CONCRETE PIPE	= RCP			
DRAWING	= DWG.	REINFORCING	REINFORCING	= REINF.			
DRILL & GROUT	= D&G	RELOCATED	RELOCATED	= RELOC.			
E	= E	REMOVE & DISPOSE	REMOVE & DISPOSE	= R&D			
EACH	= EA.	REMOVE & RESET	REMOVE & RESET	= R&R			
EACH FACE	= E.F.	REQUIRED	REQUIRED	= REQD.			
EACH WAY	= E.W.	RETAINING	RETAINING	= RET.			
EAST	= E.	RHODE ISLAND	RHODE ISLAND	= R.I.			
EASTBOUND	= EB	RIGHT	RIGHT	= RT.			
ELECTRIC	= ELEC.	RIGID STEEL CONDUIT	RIGID STEEL CONDUIT	= R.S.C.			
EMBANKMENT	= EMB.	ROAD WEATHER INFORMATION SYSTEM	ROAD WEATHER INFORMATION SYSTEM	= RWIS			
EMBEDMENT	= EMBD.	S	S	= S			
EXISTING	= EXIST.	SECTION	SECTION	= SECT.			
EXPANSION	= EXP.	SCHEDULE	SCHEDULE	= SCH.			
EQUAL	= EQ.	SCHEMATIC	SCHEMATIC	= SCHEM.			
F	= F	SHEET	SHEET	= SH., SH.T.			
FABRICATE	= FAB.	SHOULDER	SHOULDER	= SHLD.			
FACE TO FACE	= F TO F	SIMILAR	SIMILAR	= SIM.			
FAR FACE	= F.F.	SOLDIER PILE & LAGGING	SOLDIER PILE & LAGGING	= SPL			
FAR SIDE	= F.S.	SOUTH	SOUTH	= S.			
FLANGE	= FLG.	SOUTHBOUND	SOUTHBOUND	= SB, S.B.			
FLAT HEAD	= F.H.	SPACES	SPACES	= SP.			
FOOTING	= FTG.	SPACING	SPACING	= SPC.			
FOUNDATION	= FDN.	STANDARD	STANDARD	= STD.			
FURNISH, FABRICATE & ERECT	= F.F. & E.	STAY-IN-PLACE	STAY-IN-PLACE	= STI.P.			
G	= G	STANDARD	STANDARD	= STD.			
GAGE	= GA.	STATION	STATION	= STA.			
GALVANIZED	= GALV.	STAINLESS STEEL	STAINLESS STEEL	= S.S.			
GRADE	= GR.	STIFFENER	STIFFENER	= STIFF.			
GRATING	= CRTG.	SUPPORT OF EXCAVATION	SUPPORT OF EXCAVATION	= SOE, S.O.E.			
GROUND	= GND.	SYMMETRICAL	SYMMETRICAL	= SYM.			
H	= H	I	I	= I			
HEIGHT	= HGT., HT.	TANGENT	TANGENT	= TAN.			
HEXAGON	= HEX.	TEMPORARY	TEMPORARY	= TEMP.			
HIGH STRENGTH	= HS	TOP	TOP	= T			
HOLLOW TIE LINE	= H.T.L.	TOP AND BOTTOM	TOP AND BOTTOM	= T&B			
HOT MIXED ASPHALT	= HMA	TOP OF STEEL	TOP OF STEEL	= T.O.S.			
HOLLOW STRUCTURAL SECTION	= HSS	TOP OF WALL	TOP OF WALL	= T.O.W.			
HORIZONTAL	= HORIZ.	TRANSVERSE	TRANSVERSE	= TRANSV.			
I	= I	TUBE SECTION	TUBE SECTION	= TS			
INCH	= IN.	TYPICAL	TYPICAL	= TYP.			
INFORMATION	= INFO.	U	U	= U			
INSIDE DIAMETER	= I.D.	UNLESS NOTED OTHERWISE	UNLESS NOTED OTHERWISE	= U.N.O.			
INTELLIGENT TRANSPORTATION SYSTEMS	= I.T.S.	ULTRA HIGH PERFORMANCE CONCRETE	ULTRA HIGH PERFORMANCE CONCRETE	= UHPC			
INVERT	= INV.	UNITED STATES ARMY CORPS OF ENGINEERS	UNITED STATES ARMY CORPS OF ENGINEERS	= USACE			
		V	V	= V			
		VARIES	VARIES	= VAR.			
		VERTICAL CURVE	VERTICAL CURVE	= V.C.			
		VERTICAL	VERTICAL	= VERT.			



RHODE ISLAND  
DEPARTMENT OF TRANSPORTATION

DESIGNED BY:		SCALE:																																										
CHECKED BY:																																												
DATE:																																												
SHEET:	2	<table border="1"> <thead> <tr> <th colspan="3">REVISIONS</th> <th colspan="3">REVISIONS</th> </tr> <tr> <th>NO.</th> <th>DATE</th> <th>BY</th> <th>NO.</th> <th>DATE</th> <th>BY</th> </tr> </thead> <tbody> <tr><td> </td><td> </td><td> </td><td> </td><td> </td><td> </td></tr> <tr><td> </td><td> </td><td> </td><td> </td><td> </td><td> </td></tr> <tr><td> </td><td> </td><td> </td><td> </td><td> </td><td> </td></tr> <tr><td> </td><td> </td><td> </td><td> </td><td> </td><td> </td></tr> <tr><td> </td><td> </td><td> </td><td> </td><td> </td><td> </td></tr> </tbody> </table>	REVISIONS			REVISIONS			NO.	DATE	BY	NO.	DATE	BY																														
REVISIONS			REVISIONS																																									
NO.	DATE	BY	NO.	DATE	BY																																							
OF:	38																																											



### GENERAL NOTES:

- ALL CONSTRUCTION INDICATED ON THESE PLANS SHALL BE IN ACCORDANCE WITH:
  - THE 2004 EDITION (AMENDED MARCH 2018) OF, AND SUPPLEMENTS TO, THE RHODE ISLAND DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION (RI STANDARD SPECIFICATIONS).
  - THE AMERICAN ASSOCIATION OF STATE HIGHWAY AND TRANSPORTATION OFFICIALS (AASHTO) LRFD BRIDGE CONSTRUCTION SPECIFICATIONS, 4TH EDITION, 2017, INCLUDING THE LATEST INTERIM REVISIONS.
  - THE SPECIFICATIONS ACCOMPANYING THESE PLANS.IN CASE OF CONFLICT BETWEEN THE PLANS, SPECIFICATIONS OR MANUAL LISTED ABOVE, THE SPECIAL PROVISIONS OF THE SPECIFICATIONS ACCOMPANYING THESE PLANS SHALL GOVERN.
- ALL ELEVATIONS ARE REFERENCED TO THE NATIONAL GEODETIC VERTICAL DATUM OF NAVD 88.
- COORDINATES USED ON THESE PLANS ARE BASED ON THE STATEWIDE COORDINATE SYSTEM, THE NORTH AMERICAN DATUM OF 1983 (NAD 83 / 2011).
- DIMENSIONS, STATIONS, AND ELEVATIONS ARE SHOWN TO THE NEAREST ONE-HUNDREDTH OF A FOOT OR ONE-EIGHTH OF AN INCH, EXCEPT STRUCTURAL STEEL DIMENSIONS WHICH ARE TO THE NEAREST ONE-SIXTEENTH OF AN INCH.
- ALL ANGLES ARE SHOWN TO THE NEAREST SECOND.
- TOPOGRAPHIC CONDITIONS WERE OBTAINED FROM AERIAL PHOTOGRAMMETRY. ACCURACY OF VERTICAL TOPOGRAPHY IS WITHIN ONE-HALF OF A FOOT.
- FOR BENCH MARKS AND TIES SEE HIGHWAY LOCATION PLANS.
- IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO FIELD VERIFY ALL ELEVATIONS, DIMENSIONS, DETAILS, ANGLES, STRUCTURAL MEMBER SIZES, AND LAYOUTS AS SHOWN ON THESE PLANS. THIS PRIOR FIELD VERIFICATION IS ESPECIALLY PERTINENT FOR PRE-FABRICATED STRUCTURAL ITEMS AND WORK IN THE VICINITY OF UTILITIES.
- TEMPORARY PROTECTIVE SHIELDING:  
DEBRIS SHIELDS SHALL BE PROVIDED AND INSTALLED TO PROTECT MOTORISTS, WATER WAYS, ETC. FROM ANY DEMOLITION OR CONSTRUCTION DEBRIS.
- EXISTING DETAILS, DIMENSIONS AND ELEVATIONS PROVIDED IN THIS PLAN SET HAVE BEEN OBTAINED FROM THE ORIGINAL DRAWINGS AND SURVEY AND ARE NOT GUARANTEED.
- FIELD CONDITIONS MAY EXIST WHICH DEVIATE FROM THE TYPICAL AND THEORETICAL DIMENSIONS SHOWN ON THE PLANS. THE CONTRACTOR SHALL TAKE ALL FIELD MEASUREMENTS NECESSARY TO ASSURE PROPER FIT OF THE FINISHED WORK AND SHALL ASSUME FULL RESPONSIBILITY FOR THEIR ACCURACY.
- THE CONTRACTOR SHALL TAKE THE PROPER PRECAUTION TO ENSURE THE STABILITY OF ALL STRUCTURAL ELEMENTS DURING ALL PHASED CONSTRUCTION UNTIL THE TOTAL STRUCTURE IS IN PLACE.
- THE CONTRACTOR SHALL CALL DIG SAFE AT LEAST 72 HOURS PRIOR TO STARTING THE WORK TO VERIFY LOCATIONS OF EXISTING UTILITIES.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL COORDINATION WITH UTILITY OWNERS.
- ALL FOOTINGS SHALL BE APPROVED BY THE ENGINEER AS TO DIMENSIONS, ELEVATIONS, AND SUITABILITY OF FOUNDATION MATERIAL BEFORE THE PLACING OF CONCRETE.
- ALL WORKING POINTS ARE SHOWN AT THE CENTERLINES OF BEARINGS OF ABUTMENTS, UNLESS OTHERWISE NOTED.
- ALL ABUTMENTS AND WALLS ARE DRAWN LOOKING AT THE EXPOSED FACES.
- THE EXISTING UTILITIES SHOWN ON THE PLANS ARE APPROXIMATE AND WERE LOCATED USING THE BEST AVAILABLE INFORMATION. NO BUILDING SERVICE CONNECTIONS (ELECTRIC, TELEPHONE, GAS, WATER, SANITARY AND OTHERS) ARE SHOWN. THE CONTRACTOR IS TO ASSUME THAT SERVICES TO ALL BUILDINGS ARE PRESENT.
- BOTH FEDERAL AND STATE LAW (RI GENERAL LAW 39-1.2) REQUIRE NOTIFICATION OF APPROPRIATE UTILITY COMPANIES BEFORE DIGGING, TRENCHING, BLASTING, DEMOLISHING, BORING, BACK FILLING, GRADING, LANDSCAPING, OR OTHER EARTH MOVING OPERATIONS. IT IS THE CONTRACTOR'S RESPONSIBILITY TO NOTIFY ALL UTILITY COMPANIES (INCLUDING THROUGH THE "DIG SAFE" PROGRAM) TO ENSURE THAT ALL UTILITIES, BOTH UNDERGROUND AND OVERHEAD, HAVE BEEN MARKED BEFORE COMMENCEMENT OF SUCH WORK. THE CONTRACTOR SHOULD UNDERSTAND THAT NOT ALL UTILITIES SUBSCRIBE TO THE "DIG SAFE" PROGRAM. ANY DAMAGE TO EXISTING UTILITIES MARKED IN THE FIELD, OR AS A RESULT OF FAILING TO CONTACT THE APPROPRIATE UTILITY COMPANIES, SHALL BE REPAIRED OR REPLACED (AS DEEMED APPROPRIATE BY THE STATE AND/OR THE IMPACTED UTILITY COMPANY) AT NO ADDITIONAL COST TO THE STATE.

### DESIGN DATA

#### DESIGN SPECIFICATIONS

- THE AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS, 9TH EDITION, 2020, INCLUDING ALL INTERIM REVISIONS.
- THE AASHTO LRFD GUIDE SPECIFICATIONS FOR THE DESIGN OF PEDESTRIAN BRIDGES, 2009 INCLUDING ALL INTERIM REVISIONS.
- THE RHODE ISLAND LRFD BRIDGE DESIGN MANUAL 2007 EDITION INCLUDING ALL REVISIONS.
- ALL OTHER APPLICABLE DESIGN SPECIFICATIONS ARE REFERENCED IN SECTION 1 OF THE RHODE ISLAND LRFD BRIDGE DESIGN MANUAL DATED 2007.
- THE 2004 EDITION (AMENDED MARCH 2018) OF, AND SUPPLEMENTS TO, THE RHODE ISLAND DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION (RI STANDARD SPECIFICATIONS).

IN CASE OF CONFLICT, THE RHODE ISLAND LRFD BRIDGE DESIGN MANUAL SHALL GOVERN.

#### LOAD MODIFIERS

UNLESS NOTED OTHERWISE, THE LOAD MODIFIERS FOR THIS PROJECT ARE AS FOLLOWS:

- THE LOAD MODIFIER FOR DUCTILITY SHALL BE TAKEN AS 1.0 FOR ALL LIMIT STATES.
- THE LOAD MODIFIER FOR REDUNDANCY SHALL BE TAKEN AS 1.0 FOR ALL LIMIT STATES.
- THE LOAD MODIFIER FOR OPERATIONAL IMPORTANCE SHALL BE TAKEN AS 1.0 FOR ALL LIMIT STATES.

#### LOAD FACTORS

ALL LOAD FACTORS SHALL BE IN ACCORDANCE WITH THE AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS, EXCEPT AS MODIFIED IN THE RHODE ISLAND BRIDGE DESIGN MANUAL.

- THE LOAD FACTOR FOR LIVE LOAD FOR THE EXTREME EVENT I LIMIT STATE SHALL BE TAKEN AS ZERO.
- THE LOAD FACTOR FOR DEAD LOAD FOR THE EXTREME EVENT I AND EXTREME EVENT II LIMIT STATE SHALL BE TAKEN AS 1.0
- THE LOAD FACTOR FOR SETTLEMENT FOR ALL LIMIT STATES SHALL BE TAKEN AS 1.0

#### LIVE LOADS

- THE DESIGN VEHICULAR LIVE LOAD SHALL BE THE H15-44 DESIGNATION ADJUSTED FOR THE DYNAMIC LOAD ALLOWANCE, MULTIPLE PRESENCE FACTOR AND PER RI TAC 0347.

#### WIND LOADING DESIGN DATA

THE WIND LOADING DESIGN SHALL BE IN ACCORDANCE WITH THE AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS, THE RHODE ISLAND LRFD BRIDGE DESIGN MANUAL, AND AS MODIFIED HEREIN.

- EXCEPT DURING CONSTRUCTION, THE DESIGN WIND PRESSURE IS BASED ON A DESIGN WIND SPEED OF 140 MPH.
- THE DESIGN WIND PRESSURES DURING CONSTRUCTION SHALL BE AS SPECIFIED UNDER THE NOTES TITLED "GENERAL NOTES REGARDING TEMPORARY CONSTRUCTION CONDITIONS".

#### TRAFFIC DATA

NOT APPLICABLE

#### THERMAL DESIGN FORCE DATA

UNIFORM TEMPERATURE EFFECTS HAVE BEEN TAKEN INTO CONSIDERATION IN ACCORDANCE WITH THE PROCEDURE B OF THE AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS. THE MINIMUM DESIGN TEMPERATURE SHALL BE -10 DEGREES F, AND THE MAXIMUM TEMPERATURE SHALL BE 105 DEGREES F.

#### SEISMIC DESIGN DATA

PER RIDOT LRFD BRIDGE MANUAL AND AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS, EAST BAY BIKE PATH BRIDGES 083751 & 083851 SHALL MEET SEISMIC ZONE 1 DESIGN CRITERIA AND RFP ADDENDUM 3 CRITERIA.

ALL REFERENCES IN THESE GENERAL NOTE SHEETS AND THROUGHOUT THE CONTRACT DRAWINGS TO THE RHODE ISLAND STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION SHALL EXCLUDE THE METHOD OF MEASUREMENT SECTION AND BASIS OF PAYMENT SECTION FOR ITEMS PAID FOR BY THE LUMP SUM ITEMS IN THIS CONTRACT.

### FOUNDATION DESIGN DATA

**SPREAD FOUNDATIONS:**  
THE FACTORED BEARING RESISTANT FOUNDATION TYPES ARE AS FOLLOWS:

LOCATION	TYPE OF MATE
ABUTMENTS	UNDISTURBED NATURAL SOIL

**DEEP FOUNDATIONS:**  
THE FACTORED AXIAL AND UPLIFT R FOUNDATION TYPES ARE AS FOLLOWS:

LOCATION	TYPE
BR. 083751 PIER	DMP
BR. 083851 PIER	DMP
BR. 083751 ABUTMENT	DMP

LOCATION	TYPE
BR. 083751 PIER	DMP
BR. 083851 PIER	DMP
BR. 083751 ABUTMENT	DMP

- THE FACTORED DESIGN AXIAL R OF THE FACTORED GEOTECHNICAL INDICATED.
- THE FACTORED GEOTECHNICAL A<sub>1</sub> BASED ON THE NOMINAL AXIAL F RESISTANCE OF 3.6 KSF AND A LOADING).
- THE FACTORED GEOTECHNICAL A<sub>1</sub> BASED ON THE NOMINAL AXIAL F RESISTANCE OF 3.6 KSF AND A LOADING).
- THE FACTORED GEOTECHNICAL A<sub>1</sub> BASED ON THE NOMINAL AXIAL F RESISTANCE OF 3.6 KSF AND A LOADING).
- THE FACTORED GEOTECHNICAL A<sub>1</sub> BASED ON THE NOMINAL AXIAL F RESISTANCE OF 3.6 KSF AND A LOADING).

**NOTE:**  
MODULAR WALLS SHALL BE DESIGNED BY THE CONTRACTOR. THE CONTRACTOR SHALL SUBMIT FOR REVIEW CALCULATIONS AND WORKING DRAWING PROFESSIONAL ENGINEER REGISTERED



RHODE ISLAND  
DEPARTMENT OF TRANSPORTATION

DESIGNED BY:		SCALE:	
CHECKED BY:			
DATE:		REVISIONS	REVISIONS
SHEET:	3	NO.	DATE
OF:	38		



MATERIALS

STRUCTURAL STEEL:

- AASHTO DESIGNATION M 270, GRADE 50, GALVANIZED
- TRUSS BRIDGE STEEL, REFER TO CODE 801.9901 PREFABRICATED MODULAR BRIDGE FOR MATERIAL.
- API N80 CASING

REINFORCING STEEL:

- AASHTO DESIGNATION M 31, GRADE 60
- GALVANIZED COATING ASTM A767, CLASS I
- ASTM A615 GRADE 75 OR ASTM 722 GRADE 150 FOR MICRO-PILES

TREATED TIMBER

- 2x4 THRU 2x10, 4x4, 4x6 SHALL BE SOUTHERN PINE, GRADE NO. 1 OR BETTER

HARDWARE AND FASTENERS:

- CARRIAGE BOLTS, THREADED RODS, LAG SCREWS      ASTM A307 GRADE A
- HEX BOLTS      ASTM A307 GRADE A
- OGEE WASHERS      ASTM A48

COMPOSITE TOP RAIL:

- COMPOSITE TOP RAIL BOARDS SHALL BE CONSTRUCTED, MANUFACTURED, AND FABRICATED IN ACCORDANCE WITH ASTM D7032-6A STANDARD SPECIFICATIONS. BOARDS SHALL BE SOLID COMPOSITE AND CAPPED. COLOR OF TOP RAIL SHALL BE GRAY.

CONCRETE STRENGTHS:

- CLASS HP 3/4" F'C=5,000 PSI  
RETURN WALLS, BACKWALLS
- CLASS MC 3/4" F'C=3,500 PSI @ 28 DAYS, F'C=5,000 PSI @ 56 DAYS  
ALL COMPONENTS GREATER THAN 3 FEET IN THICKNESS, INCLUDING, BUT NOT LIMITED TO: ABUTMENT STEMS, PIER CAPS
- CLASS XX 3/4" F'C=4,000 PSI  
FOOTINGS
- 4,000 PSI NEAT CEMENT GROUT  
MICRO-PILES
- CLASS A CONCRETE 3/4" F'C=3,000 PSI  
PEDESTRIAN BRIDGE RAIL POSTS

CONCRETE NOTES:

- CLASSES OF CONCRETE SHALL BE HIGH PERFORMANCE CLASS HP, CLASS MC AND CLASS XX, AS DESCRIBED THE RHODE ISLAND STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND THE SPECIAL PROVISIONS OF THE SPECIFICATIONS. REFER TO THE "MATERIALS" NOTES FOR CLASSES OF CONCRETE SPECIFIED FOR VARIOUS COMPONENTS.
- THE CONTRACTOR MAY, AT THE APPROVAL OF THE ENGINEER, PROPOSE THE USE OF SELF-CONSOLIDATING CONCRETE FOR ANY CLASS OF CONCRETE ON THIS PROJECT. SECTION 606 "SELF CONSOLIDATING CONCRETE (SCC)" CONTAINS THE REQUIREMENTS FOR MODIFYING ALL CLASSES OF CONCRETE MIX DESIGN FOR SELF-CONSOLIDATING APPLICATIONS. THE MAXIMUM WATER-CEMENT RATIO FOR SCC SHALL BE 0.40.
- ALL PORTLAND CEMENT CONCRETE SHALL BE AIR-ENTRAINED.
- ALL REINFORCING STEEL SHALL BE GALVANIZED. ALL WIRE TIES AND MISCELLANEOUS HARDWARE USED FOR PLACEMENT OF GALVANIZED REINFORCING SHALL ALSO BE GALVANIZED. GALVANIZED COATING FOR REINFORCING STEEL SHALL CONFORM TO ASTM A767 CLASS 1.
- ALL LAP SPLICES NOT SHOWN ON THE PLANS SHALL BE LAPPED IN ACCORDANCE WITH THE AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS FOR CLASS B LAP SPLICES.

- UNLESS OTHERWISE INDICATED ON THE PLANS, ALL MAIN REINFORCING BARS SHALL HAVE THE FOLLOWING MINIMUM COVER:

CONCRETE CAST AGAINST OR PERMANENTLY EXPOSED TO EARTH (FOOTINGS, ABUTMENT WALL FACES, AND BACKWALLS)

COVER  
3"

ALL OTHER BARS

2"

COVER TO TIES AND STIRRUPS MAY BE 0.5 INCH LESS THAN ABOVE VALUES SPECIFIED FOR MAIN REINFORCING, BUT IN NO CASE LESS THAN 1.5 INCHES.

- UNLESS OTHERWISE NOTED ON THE PLANS, ALL ANCHOR BOLTS SHALL BE ASTM DESIGNATION F1554, GR 55, AND SHALL BE GALVANIZED IN ACCORDANCE WITH AASHTO DESIGNATION M 232 OR METALIZED IN ACCORDANCE WITH SECTION M.05. SWEDGED RODS SHALL BE AASHTO DESIGNATION M 270 GRADE 36 AND SHALL BE GALVANIZED IN ACCORDANCE WITH AASHTO DESIGNATION M 232.

- ALL ANCHOR BOLTS SHALL BE SET BY TEMPLATES PRIOR TO PLACEMENT OF CONCRETE UNLESS OTHERWISE INDICATED ON THE PLANS OR AUTHORIZED BY THE ENGINEER.

- HORIZONTAL CONSTRUCTION JOINTS OTHER THAN THOSE SHOWN ON PLANS WILL NOT BE PERMITTED WITHOUT A WRITTEN REQUEST BY THE CONTRACTOR AND PRIOR AUTHORIZATION BY THE ENGINEER.

- UNLESS OTHERWISE NOTED ON THE PLANS, ALL EXPOSED CONCRETE SURFACES VISIBLE IN ELEVATION TO ONE FOOT BELOW FINAL GROUND LINE, SHALL RECEIVE A CONCRETE SURFACE RUBBED FINISH IN ACCORDANCE WITH THE RI STANDARD SPECIFICATIONS.

- THE ENTIRE TOPSIDE SURFACES OF ABUTMENT BEAM SEATS, AS WELL AS VERTICAL FACES OF BACKWALLS SHALL BE PROVIDED WITH A FILM-FORMING SEALER (M12.03.1) CONCRETE SURFACE TREATMENT-PROTECTIVE COATING IN ACCORDANCE WITH SECTION 820 OF THE RI STANDARD SPECIFICATIONS.

- ALL EXPOSED EDGES AND REENTRANT CORNERS NOT OTHERWISE DETAILED ON THE PLANS SHALL HAVE A MINIMUM 1/4" CHAMFER.

- THE CONTRACTOR SHALL BE RESPONSIBLE FOR PREVENTING CONCRETE STAINS OR DISCOLORATIONS DURING CONSTRUCTION UNTIL SUCH TIME AS THE SURFACES ARE APPROVED AND ACCEPTED BY THE ENGINEER. ANY CONCRETE STAINS OR DISCOLORATIONS OCCURRING PRIOR TO ACCEPTANCE OF THE SURFACES SHALL BE REMOVED BY THE CONTRACTOR.

- ALL JOINT SEALANT SHALL BE POLYURETHANE, POLYURETHANE ELASTOMERIC, OR SILICONE SEALANT AS DESIGNATED ON THE PLANS. THE COLOR OF THE JOINT SEALANT, WHERE EXPOSED, SHALL BE NEUTRAL (LIGHT GRAY OR TAN). THE COLOR OF THE SEALANT, WHERE NOT EXPOSED, WILL BE AT THE DISCRETION OF THE CONTRACTOR.

- UNLESS OTHERWISE NOTED ON THE PLANS, JOINT FILLER IS TO BE A PREFORMED, NON-EXPANSIVE, NON-EXTRUDING TYPE IN ACCORDANCE WITH SECTION M.02.11.1 OF THE RI STANDARD SPECIFICATIONS.

- EMBEDMENT LENGTHS FOR DRILLED AND GROUTED DOWELS SHALL BE IN ACCORDANCE WITH SECTION 819 OF THE RI STANDARD SPECIFICATIONS, UNLESS OTHERWISE INDICATED ON THE PLANS.

- IN ACCORDANCE WITH THE RI STANDARD SPECIFICATIONS, ALL METAL TIES, NON-METALLIC TIES OR ANCHORAGES WHICH ARE REQUIRED FOR CONCRETE FORMWORK SHALL BE SO CONSTRUCTED THAT THEY CAN BE REMOVED TO AT LEAST TWO INCHES BELOW THE EXPOSED SURFACE OF THE CONCRETE WITHOUT CAUSING DAMAGE TO THE CONCRETE SURFACE. SNAP TIES MAY BE USED ONLY IF APPROVED BY THE ENGINEER. IF THE CONTRACTOR PROPOSES TO USE THEM, A CATALOG CUT AND OTHER NECESSARY INFORMATION MUST BE SUBMITTED TO THE ENGINEER TO DEMONSTRATE THAT THE TIES WILL SNAP-OFF FAR ENOUGH INTO THE CONCRETE TO ALLOW FOR PROPER PATCHING. SNAP TIES MUST PROVIDE ADEQUATE STRENGTH TO SUPPORT THE FORMS. ALL CAVITIES SHALL BE FILLED WITH AN APPROVED CEMENT MORTAR MEETING THE REQUIREMENTS OF ASTM C 928.

- HAND-HELD VIBRATORS SHALL BE EQUIPPED WITH RUBBER TIPPED HEADS WHEN USED TO CONSOLIDATE CONCRETE AROUND REINFORCEMENT AND EMBEDMENTS.

- WATER STOPS ARE REQUIRED FOR HORIZONTAL AND VERTICAL CONSTRUCTION JOINTS IN ABUTMENTS AND WALLS WHEN EXPOSED TO BACKFILL EARTH MATERIAL. WATER STOPS SHALL BE INSTALLED AT THE LOCATIONS DETAILED ON THE PLANS, AT THE LOCATIONS AS SPECIFIED ABOVE AND AT ALL LOCATIONS AS DIRECTED BY THE ENGINEER, ALL IN ACCORDANCE WITH SECTION 812 OF THE RI STANDARD SPECIFICATIONS.

- UNLESS OTHERWISE DIMENSIONED ON THE PLANS, ALL REINFORCEMENT BENDS SHOWN ARE STANDARD HOOKS.

- ALL EXPOSED FACES ABUTMENTS FROM THE BRIDGE SEATS TO THE GROUND SURFACE AND EXPOSED WALL SURFACES SHALL RECEIVE A CONCRETE SURFACE TREATMENT - PROTECTIVE SEALER THAT SHALL BE GRAY IN COLOR. A CLEAR, NON-SACRIFICIAL TYPE ANTI-GRAFFITI COATING THAT CONFORMS TO SECTION 842 SHALL BE APPLIED OVER THE FULLY CURED CONCRETE PROTECTIVE COATING.

- ANY METALLIC ELEMENTS THAT ARE TO BE LEFT IN PLACE AND NOT STATED HEREIN SHALL BE GALVANIZED. THIS INCLUDES, BUT IS NOT LIMITED TO REINFORCING STEEL, WIRE MESH, SNAP TIES, METAL TIES, ANCHORAGES FOR FORM WORK, SUPPORTS FOR MASS CONCRETE COOLING PIPES, ETC.

REINFORCEMENT

THE CONTRACTOR'S RE  
PREPARING HIS ORDER  
REINFORCEMENT DETAIL  
TIME TO PERMIT CARE



RHODE ISLAND  
DEPARTMENT OF TRANSPORTATION

DESIGNED BY:		SCALE:	
CHECKED BY:			
DATE:		REVISIONS	REVIS
SHEET:	4	NO.	DATE
OF:	38	BY	NO.
			DA



### STRUCTURAL STEEL NOTES

1. ALL STRUCTURAL STEEL SHALL BE IN ACCORDANCE WITH CODE 801.9901 PREFABRICATED MODULAR BRIDGE AND SECTION 824 OF THE RI STANDARD SPECIFICATIONS AS APPLICABLE.

### TIMBER CONSTRUCTION NOTES

1. ALL TIMBER SHALL BE PRESSURE TREATED. WOOD SHALL BE PRESSURE TREATED IN ACCORDANCE WITH THE AMERICAN WOOD PROTECTION ASSOCIATION (AWPA).
2. GROUP CONTACT POSTS SHALL BE TREATED WITH PRESERVATIVES TO THE REQUIREMENTS FOR GROUND CONTACT/FRESHWATER, GENERAL USE SERVICE CONDITIONS (UC4A) IN ACCORDANCE WITH AWPA STANDARD U1 OR ICC-ES EVALUATION REPORTS.
3. ALL BOLTS SHALL BE ASTM A307 OR AS ALTERNATE A3125, GRADE A325.
4. ALL BOLTED CONNECTIONS SHALL INCLUDE WASHERS AT BOLT HEADS AND NUTS.
5. LAG SCREWS SHALL BE LOW CARBON STEEL, ASTM A307 OR BETTER.
6. ALL BOLTS, WASHERS AND OTHER HARDWARE SHALL BE GALVANIZED IN ACCORDANCE WITH AASHTO M232.
7. TIMBER WHICH AT THE DISCRETION OF THE ENGINEER IS SEVERELY WARPED, BOWED, SPLIT, OR SPLINTERED SHALL NOT BE USED.
8. RAILS AND RAIL CAPS SHALL BE CONTINUOUS OVER 1 POST MINIMUM. BUTT JOINTS IN RAILS AND RAIL CAPS SHALL BE STAGGERED AS SHOWN ON SHEET 35 TYPICAL ELEVATION OF PEDESTRIAN BRIDGE RAIL.
9. ALL TIMBER RAILING COMPONENTS SHALL BE TREATED WITH PRESERVATIVES TO THE REQUIREMENTS FOR AN ABOVE GROUND, EXPOSE SERVICE CONDITION (UC3B) IN ACCORDANCE WITH AWPA STANDARD U1 OR ICC-ES EVALUATION REPORTS.
10. TREAT ALL CUT ENDS, HOLES, NOTCHES AND RECESSES WITH COPPER NAPHTHENATE PRESERVATIVE.
11. ALL TIMBER SIZES ARE NOMINAL DIMENSION LUMBER UNLESS OTHERWISE NOTED.
12. LUMBER SUPPLIED SHALL MEET THE REQUIREMENTS OF "SECTION 806 OF THE STANDARD SPECIFICATIONS" AND "THE 2018 NATIONAL DESIGN SPECIFICATIONS FOR WOOD CONSTRUCTION". CONDITION AND TREAT STRUCTURAL TIMBER AND LUMBER IN ACCORDANCE WITH THE "2018 NATIONAL DESIGN SPECIFICATIONS FOR WOOD CONSTRUCTION".
13. GALVANIZED CARRIAGE BOLTS TO BE USED FOR ALL RAILING/POST CONNECTIONS.

### DEMOLITION NOTES

1. DIMENSIONS ARE BASED ON ORIGINAL DESIGN DRAWINGS AND ARE FOR INFORMATION ONLY. THE CONTRACTOR SHALL VERIFY ALL DIMENSIONS IN THE FIELD PRIOR TO COMMENCEMENT OF CONSTRUCTION.
2. ALL ABUTMENTS SHALL REMAIN IN PLACE, SO AS NOT TO CAUSE ANY SHORELINE DISTURBANCES. ONCE THE ENVIRONMENTAL APPROVALS FOR CONSTRUCTION OF THE NEW BRIDGES HAS BEEN GRANTED, THE EXISTING ABUTMENTS MAY BE DEMOLISHED.
3. IF THE CONTRACTOR'S DEMOLITION OPERATIONS CAUSE ANY DAMAGE TO ACCESS ROUTES AND PROPERTIES OUTSIDE OF THE PROJECT WORK AREA, THE CONTRACTOR SHALL BE REQUIRED TO REPAIR THE AREA TO THE SATISFACTION OF THE ENGINEER.
4. ALL DEMOLITION MATERIALS SHALL BE CONTAINED, COLLECTED, AND LEGALLY DISPOSED. IF DEBRIS FALLS TO THE RIVER, THE CONTRACTOR SHALL IMMEDIATELY REMOVE THE DEBRIS FROM THE WATER.
5. TIMBER PILES SHALL BE REMOVED IN THEIR ENTIRETY. STEEL PILES, THE STONE PIER IN THE BARRINGTON RIVER, AND ANY TIMBER PILES THAT BREAK MUST BE TRIMMED A MINIMUM OF 2'-0" BELOW THE RIVERBED SUBSTRATE LINE.
6. ALL DEMOLITION MATERIALS SHALL BE TAKEN FROM THE SITE TO AN APPROVED DESTINATION AS THE WORK PROGRESSES.
7. THE CONTRACTOR IS NOTIFIED THAT THE EXISTING PAINT SYSTEM OF THE STEEL AND THE TIMBER MAY CONTAIN TOXIC SUBSTANCES, SUCH AS LEAD, CHROMIUM, OR CREOSOTE, WHICH MAY REQUIRE SPECIAL HANDLING AND MAY BE HAZARDOUS WASTE WHEN REMOVED. PROTECT PERSONS AND ENVIRONMENT DURING THE REMOVAL OF THE EXISTING STEEL, IN ACCORDANCE WITH SECTION 826 OF THE STANDARD SPECIFICATIONS.
8. THE CONTRACTOR SHALL SUBMIT TO THE ENGINEER, IN WRITING, HIS PROPOSED METHOD OF DEMOLITION. DEMOLITION OPERATIONS SHALL NOT BEGIN UNTIL HIS METHOD HAS BEEN APPROVED BY THE ENGINEER. THIS SUBMISSION SHALL INCLUDE THE DEMOLITION PLANS, EQUIPMENT, SEQUENCE AND METHOD THE CONTRACTOR PROPOSED TO USE, IN DETAIL.

THE DEMOLITION AND FALSEWORK SUBMITTALS MUST BE STAMPED BY A PROFESSIONAL ENGINEER REGISTERED IN THE STATE OF RHODE ISLAND. THE FURNISHING OF DEMOLITION AND FALSEWORK SUBMITTALS AND PLANS SHALL NOT SERVE TO RELIEVE THE CONTRACTOR OF ANY PART OF HIS/HER RESPONSIBILITY FOR THE SAFETY OF THE WORK OR FOR THE SUCCESSFUL COMPLETION OF THE WORK.



RHODE ISLAND  
DEPARTMENT OF TRANSPORTATION

DESIGNED BY:	SCALE:
CHECKED BY:	
DATE:	
SHEET:	
OF:	
5	
38	

REVISIONS				REVIS
NO.	DATE	BY	NO.	DAT



## 1. DESIGN WIND PRESSURES FOR CONSTRUCTION:

MINIMUM WIND PRESSURES TO BE USED BY THE CONTRACTOR FOR DESIGN DURING THE CONSTRUCTION CONTRACT (WITH THE EXCEPTION OF SIGNS) SHALL BE FROM THE FOLLOWING TABLE:

HEIGHT ABOVE GROUND	WIND PRESSURE (PSF)
UP TO 17'	33
OVER 17' AND UP TO 33'	37
OVER 33' AND UP TO 50'	41
OVER 50' AND UP TO 75'	44
OVER 75' AND UP TO 100'	47

## TABLE NOTES:

#### A. APPLICATION OF THE TABULAR PRESSURE:

- BRIDGE COMPONENTS DURING CONSTRUCTION, PRIOR TO THE INSTALLATION OF THE PERMANENT BRACING SYSTEMS, NOT INCLUDING CRANE LIFTING.
- FALSE WORK, SHORING, AND SCAFFOLDING AS DEFINED IN FHWA GUIDE DESIGN SPECIFICATION FOR BRIDGE TEMPORARY WORKS, EXCLUDING 3-DIMENSIONAL LATTICED OR TRUSSED FRAMES OR TOWERS;
- TEMPORARY SHIELDING.

WIND PRESSURES FOR ALL OTHER STRUCTURES SHALL BE CALCULATED BASED ON ASCE DESIGN LOADS ON STRUCTURES DURING CONSTRUCTION, SEI/ASCE 37-02 (ALL REFERENCES TO THE ASCE 7 IN THE SEI/ASCE 37-02 PUBLICATION, SHALL BE THE LATEST REVISION OF ASCE 7). THE EXPOSURE CATEGORY SHALL BE C.

B. WHERE APPLICABLE HIGHER AMTRAK WIND REQUIREMENTS SHALL SUPERSEDE THESE REQUIREMENTS.

C. FOR STRUCTURES SITUATED ABOVE LIVE INTERSTATE TRAFFIC, THE TABULAR VALUES SHALL BE INCREASED BY 5 PSF.

## 2. ERECTION OF BRIDGE COMPONENTS:

FOR THE ERECTION OF STRUCTURES, THE FOLLOWING SHALL APPLY:

- THE CONTRACTOR SHALL SUBMIT AN ERECTION PLAN THAT PROVIDES COMPLETE DETAILS OF THE PROCESS INCLUDING, BUT NOT LIMITED TO, TEMPORARY SUPPORTS, SCHEDULING AND OPERATION SEQUENCING, CRANE PLACEMENT, AND ASSUMED LOADS AND CALCULATED STRESSES STRAINING VARYING STAGES OF LIFTING. THIS APPLIES TO STRUCTURES OF ANY KIND. THE QUALITY OF THE ERECTION PLAN WILL BE THE CRITICAL FACTOR. IT MUST BE ADEQUATE FOR 125 PERCENT (150 PERCENT OVER AMTRAK) OF THE TOTAL PICK LOAD INCLUDING SPREADERS, RIGGING, HOOKS, AND ALL OTHER MATERIALS. THIS FACTOR OF SAFETY SHALL BE IN ADDITION TO ALL MANUFACTURERS' PUBLISHED FACTORS OF SAFETY.
- A REGISTERED PROFESSIONAL ENGINEER, LICENSED IN THE STATE OF RHODE ISLAND, WILL BE REQUIRED TO STAMP THE CONTRACTOR'S ERECTION PLAN.
- THE CONTRACTOR'S PROFESSIONAL ENGINEER WILL BE REQUIRED TO INSPECT AND PROVIDE WRITTEN APPROVAL OF INSTALLATION, PRIOR TO ALLOWING VEHICLES OR PEDESTRANS ON OR BELOW THE STRUCTURE. THE PROFESSIONAL ENGINEER MUST ALSO STAMP ALL CHANGES TO THE CONTRACTOR'S ERECTION PLAN. ADDITIONALLY, ALL PROPOSED CHANGES MUST BE SUBMITTED TO RIOT FOR REVIEW AND APPROVAL PRIOR TO IMPLEMENTATION.
- A MANDATORY PRE-ERECTION CONFERENCE WILL BE HELD AT LEAST TWO WEEKS PRIOR TO THE START OF THE GIRDER INSTALLATION TO DISCUSS THE PLAN AND PROCEDURES, WORK SCHEDULE, SOMEWHAT OF THE SAFETY AND TRAFFIC CONTROL. THE CONTRACTOR'S PROFESSIONAL ENGINEER AND ENGINEER SUBCONTRACTOR WILL BE REQUIRED TO ATTEND THIS MEETING, AS WILL THE RIOT RESIDENT ENGINEER, THE DESIGN PROJECT ENGINEER AND THE DESIGN CONSULTANT. BASED UPON DISCUSSIONS AT THIS MEETING AND REVIEW OF THE ERECTION PLAN, THE AUTIONER MAY ORDER THE CONTRACTOR TO MODIFY AND RESUBMIT THE ERECTION PLAN TO THE ENGINEER FOR REVIEW AND APPROVAL.
- THE CONTRACTOR WILL BE REQUIRED TO PERFORM DAILY INSPECTIONS OF THE ERECTED GIRDERS UNTIL THE BRIDGE DECK IS COMPLETELY POURED.

CONSTRUCTION NOTES:

1. THE CONTRACTOR IS RESPONSIBLE FOR THE IMPLEMENTATION, CONSTRUCTION, OPERATION AND SAFETY OF ALL EQUIPMENT AND PROCEDURES.
2. THE CONTRACTOR SHALL SUBMIT WORKING DOCUMENTS SHOWING PROPOSED METHODS OF LIFTING, SEQUENCING OF LIFTING, LOCATION OF CRANES, CRANE CAPACITIES, LOCATION OF THE LIFTING POINTS ON THE BRIDGE COMPONENTS, WEIGHTS OF THE COMPONENTS, LIFTING DEVICES AND LOAD DISTRIBUTION DEVICE DETAIL. THE METHOD AND ALL SUBMISSIONS SHALL BE PREPARED AND STAMPED BY A RHODE ISLAND REGISTERED PROFESSIONAL ENGINEER.
3. COORDINATE ALL CONSTRUCTION ACTIVITIES WITHIN THE WORKING AREA WITH RIOT REGARDING UTILITIES, PROTECTION OF TRAFFIC AND SCHEDULE.
4. THE CONTRACTOR SHALL EXERCISE EXTREME CARE TO AVOID DAMAGE TO EXISTING STRUCTURES. ALL STRUCTURES DAMAGED AS A RESULT OF THE CONTRACTOR'S OPERATION SHALL BE REPAIRED TO THE SATISFACTION OF THE ENGINEER.
5. ALL SHOP DRAWINGS SHALL BE SUBMITTED TO THE ENGINEER IN SUFFICIENT TIME TO PERMIT CAREFUL CHECKING AS NOT TO DELAY THE PROJECT.
6. ALL RIGGING IS TO BE IN EXCELLENT WORKING CONDITION.
7. UNLOADED CRANES ARE ALLOWED TO TRAVEL IN THE WORKING AREA.
8. CRANE DELIVERY LOCATIONS MAY VARY AS LONG AS MAXIMUM CRANE RADIUS IS NOT EXCEEDED.
9. TEMPORARY EXCAVATION SUPPORT SYSTEM SHALL BE DESIGNED, FURNISHED AND INSTALLED BY THE CONTRACTOR AND SUBMITTED TO THE ENGINEER FOR REVIEW AND APPROVAL PRIOR TO THE START OF ANY EXCAVATION.
10. CONTRACTOR SHALL SECURE ALL WORK AREAS AT ALL TIMES TO PREVENT UNAUTHORIZED ACCESS.
11. STOCKPILED SOIL SHALL BE NOT CLOSER THAN 30 FEET FROM PIERS, WALLS AND ABUTMENTS.

UTILITY NOTES:

1. EXISTING UTILITIES SHOWN ON THE PLANS ARE APPROXIMATE AND WERE LOCATED USING THE BEST AVAILABLE INFORMATION. NO BUILDING SERVICE CONNECTIONS (ELECTRIC, TELEPHONE, GAS, WATER, SANITARY AND OTHERS) ARE SHOWN. THE CONTRACTOR IS TO ASSUME THAT SERVICES TO ALL BUILDINGS ARE PRESENT.
2. BOTH FEDERAL AND STATE LAW (RI. GENERAL LAW 39-1.2) REQUIRE NOTIFICATION OF APPROPRIATE UTILITY COMPANIES BEFORE DIGGING, TRENCHING, BASTING, DEMOLISHING, BORING, BACKFILLING, GRADING, LANDSCAPING, OR OTHER EARTH MOVING OPERATIONS. IT IS THE CONTRACTOR'S RESPONSIBILITY TO NOTIFY ALL UTILITY COMPANIES (INCLUDING THROUGH THE "DIG SAFE" PROGRAM) TO ENSURE THAT ALL UTILITIES, BOTH UNDERGROUND AND OVERHEAD, HAVE BEEN MARKED BEFORE COMMENCEMENT OF SUCH WORK. THE CONTRACTOR SHOULD UNDERSTAND THAT NOT ALL UTILITIES SUBSCRIBE TO THE "DIG SAFE" PROGRAM. ANY DAMAGE TO EXISTING UTILITIES MARKED IN THE FIELD, OR AS A RESULT OF FAILING TO CONTACT THE APPROPRIATE UTILITY COMPANIES, SHALL BE REPAIRED OR REPLACED (AS DEEMED APPROPRIATE BY THE STATE AND/OR THE IMPACTED UTILITY COMPANY) AT NO ADDITIONAL COST TO THE STATE.
3. CONSTRUCTION EQUIPMENT OR PERSONNEL SHALL FOLLOW OSHA REGULATION IN REGARDS TO MINIMUM CLEARANCE TO ENERGIZED OVERHEAD LINES.
4. UNDERGROUND UTILITY LINES MAY BE IN CONFLICT WITH REQUIRED TEMPORARY OR PERMANENT CONSTRUCTION, OR THE EQUIPMENT NECESSARY TO PERFORM THIS REQUIRED CONSTRUCTION. DEPENDING UPON THE CONTRACTOR'S METHOD OF CONSTRUCTION, THESE UTILITIES MAY NEED TO BE RELOCATED FOR PORTIONS OF THE CONSTRUCTION PERIOD AND THEN MOVED BACK TO PERMANENT LOCATIONS WHICH MAY BE OTHER THAN CURRENT LOCATIONS. THE ACTUAL RELOCATIONS (TEMPORARY OR PERMANENT) ARE THE RESPONSIBILITY OF THE INDIVIDUAL UTILITY OWNER. HOWEVER, THE CONTRACTOR WILL BE REQUIRED TO COORDINATE THE EXACT LOCATION AND TIMING OF ALL UTILITY RELOCATIONS WITH THE INDIVIDUAL UTILITY OWNER, AND TO PHASE HIS CONSTRUCTION OPERATIONS AS REQUIRED TO ACCOMMODATE ALL (TEMPORARY AND PERMANENT) UTILITY RELOCATIONS. IN ADDITION TO FIELD MEETINGS AND CORRESPONDENCE, THIS COORDINATION MAY INCLUDE STAKING OF LOCATIONS, EXCAVATION AND TEMPORARY GRADING, PROVIDING ACCESS TO EXISTING AND FUTURE UTILITY POLE AND CONDUIT LOCATIONS, OR OTHER PHYSICAL WORK AS REQUIRED TO ALLOW FOR UTILITY RELOCATION WORK. THE CONTRACTOR SHALL ENGAGE IN THE NECESSARY COORDINATION OF UTILITY RELOCATIONS AND RELOCATION WORK AND NO ADDITIONAL COST TO THE PROJECT OR THE STATE, AND SHALL HAVE NO RIGHT TO ADDITIONAL COMPENSATION FOR DELAYS OR STAGING AND PHASING OF HIS WORK AS A RESULT OF UTILITY RELOCATION WORK.

TEMPORARY CONSTRUCTIC

1. STRUCTURAL STEEL SHAPES AND DESIGNATION A 709 GRADE 36 O
2. HIGH STRENGTH STEEL BOLTS SH CONTRACTOR SHALL REFER TO SE OF RHODE ISLAND STANDARD SPE AND INSTALLATION REQUIREMENTS DIAMETER HOLDS UNLESS OTHER
3. WASHERS MEETING ASTM DESIGN 1/16" IN DIAMETER GREATER THA ASSEMBLY.
4. WELDING SHALL BE IN ACCORDAN D15-2015 (INCLUDING ALL INTER CONNECTIONS SHALL BE W OTHERWISE NOTED.
5. WELDING ELECTRODES SHALL HAV
6. NO SHOP FILLET WELD SHALL BE
7. PRIOR TO FABRICATION, ALL MATE AL OIL, DIRT, GREASE, MILL SCALE SHALL BE REMOVED.
8. WHEN STEEL DIE STAMPS ARE US STRESS STAMPS.

## DESIGN TIDAL INFORMATION

UNITED STATES ARMY CORPS ENGINEER  
MEAN HIGH WATER (MHW)  
MEAN LOW WATER (MLW)  
MEAN LOW LOW WATER (MLLW)  
100 YEAR FLOOD (BARRINGTON RIVER)  
100 YEAR FLOOD (PALMER RIVER)

THE CONTRACTOR SHALL NOTE THAT H

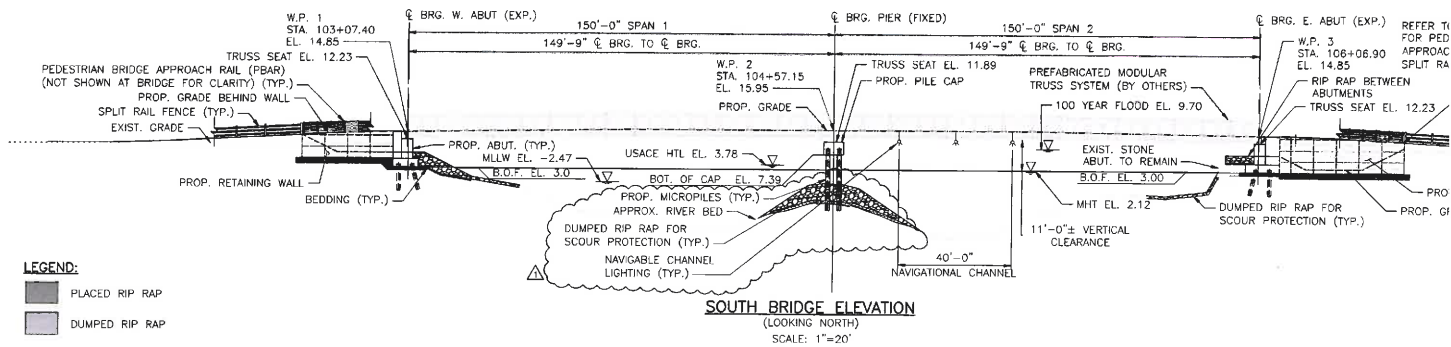
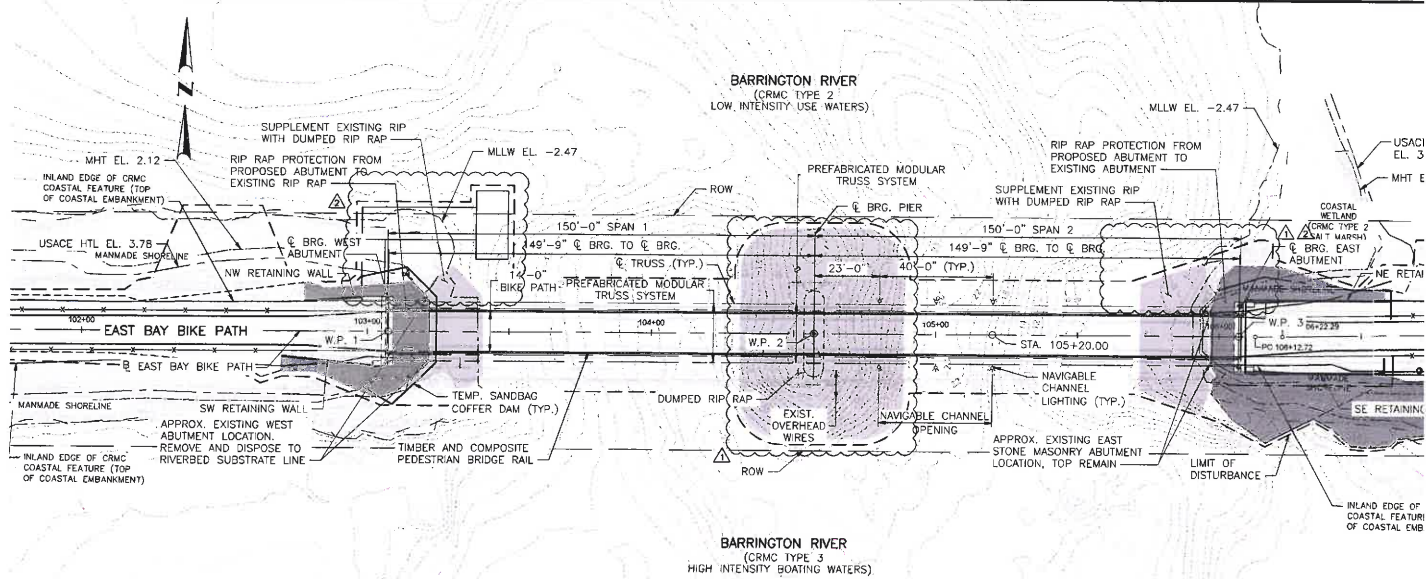


RHODE ISLAND  
DEPARTMENT OF TRANSPORTATION

DESIGNED BY:	SCALE:			
CHECKED BY:				
DATE:	REVISIONS			REVIS
SHEET:	NO.	DATE	BY	NO. DATE
OF:				







**LEGEND:**

- PLACED RIP RAP
- DUMPED RIP RAP

WORKING POINT		STATION
W.P. 1		103+07.4
W.P. 2		104+57.1
W.P. 3		106+05.5

**AETNA**  
Aetna Bridge Company

**vhb**  
1 Cedar Street  
Suite 400  
Providence, RI 02903  
401.272.8100

**RI**  
**DOT**

**RHODE ISLAND**  
**DEPARTMENT OF TRANSPORTATION**

PROJECT  
MODIFICATIONS SINCE  
OCTOBER 24, 2023  
CRMC FULL COUNCIL  
PUBLIC HEARING

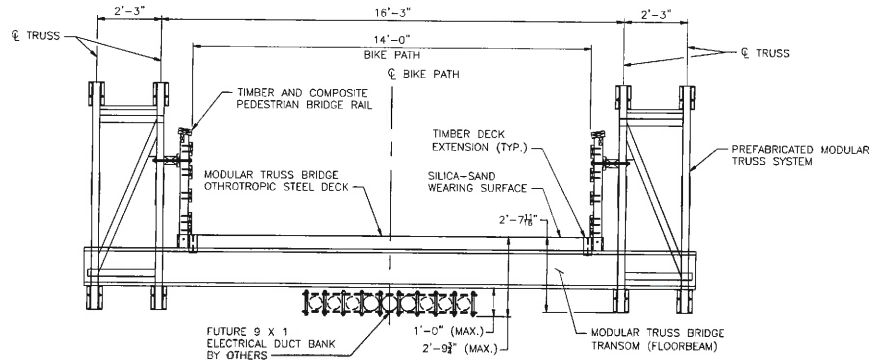
DESIGNED BY:  
CHECKED BY:  
DATE:  
SHEET:  
OF:

SCALE:

REVISIONS				REVISIONS			
NO.	DATE	BY	NO.	DATE	BY	NO.	DATE
1	2/2/24	vhb	2	4/15/24	vhb		

RECEIVED  
4/18/2024  
Rhode Island Department of Transportation

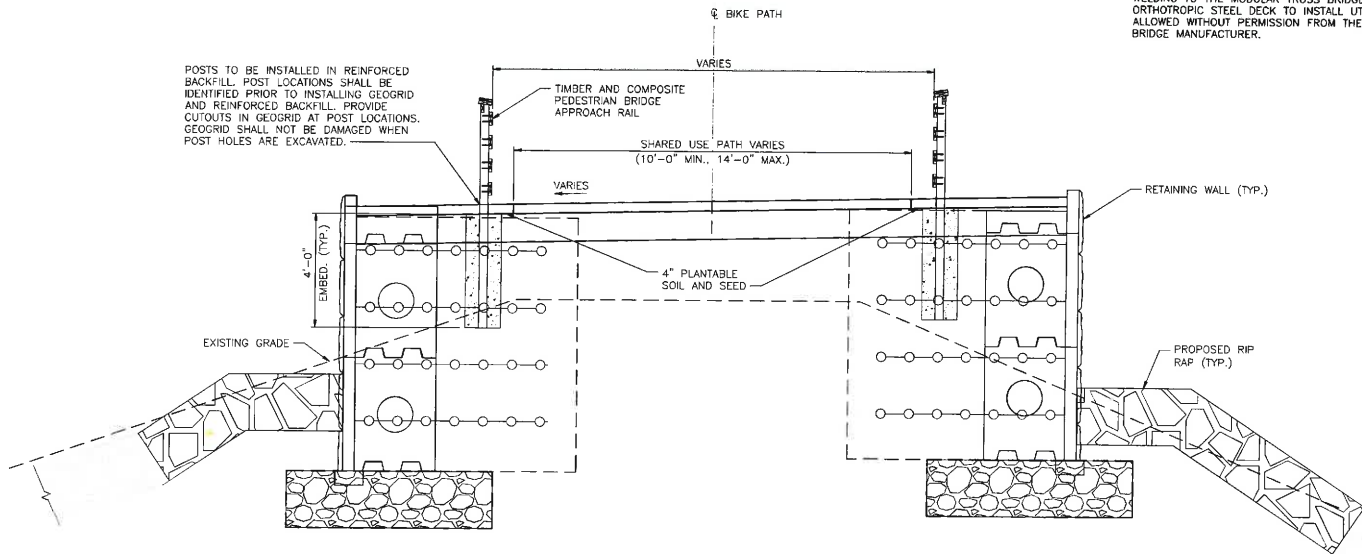




**TYPICAL TRUSS SECTION**  
NOT TO SCALE

**NOTES:**

1. FUTURE DUCT BANK AND SUPPORTS BY OTHERS TO BE WELDED TO THE MODULAR TRUSS BRIDGE ORTHOTROPIC STEEL DECK TO INSTALL UTILITIES ALLOWED WITHOUT PERMISSION FROM THE BRIDGE MANUFACTURER.



**PROPOSED APPROACH SECTION**  
NOT TO SCALE



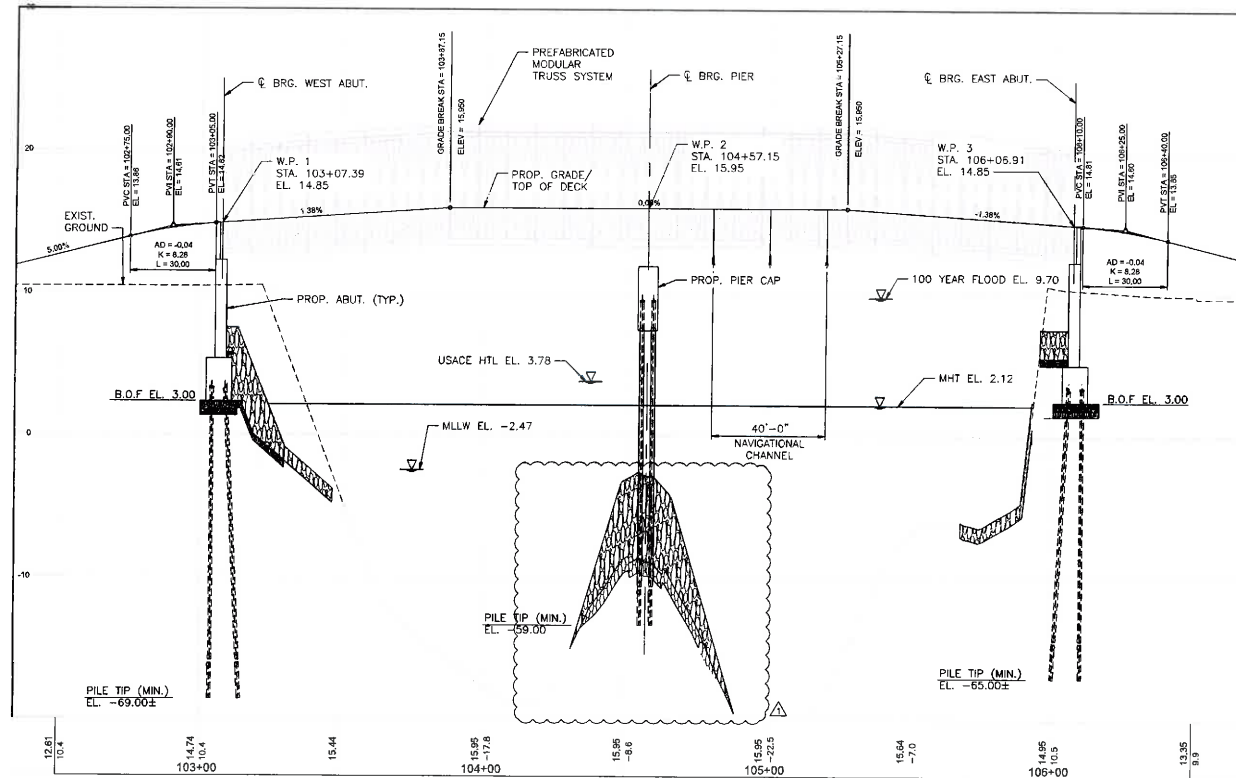
**vhb**  
1 Cedar Street  
Suite 400  
Providence, RI 02903  
401.272.8100



**RHODE ISLAND**  
**DEPARTMENT OF TRANSPORTATION**

DESIGNED BY:		SCALE:				
CHECKED BY:						
DATE:		REVISIONS			REVIS	
SHEET:	8	NO.	DATE	BY	NO.	DA
OF:	38					

RECEIVED  
4/18/2024  
CONTRACT INFORMATION  
MANAGEMENT SYSTEM



**B BARRINGTON PROFILE**  
 SCALE HORIZONTAL: 1"=20'  
 SCALE VERTICAL: 1"=4'

**NOTE:**  
 REFER TO H



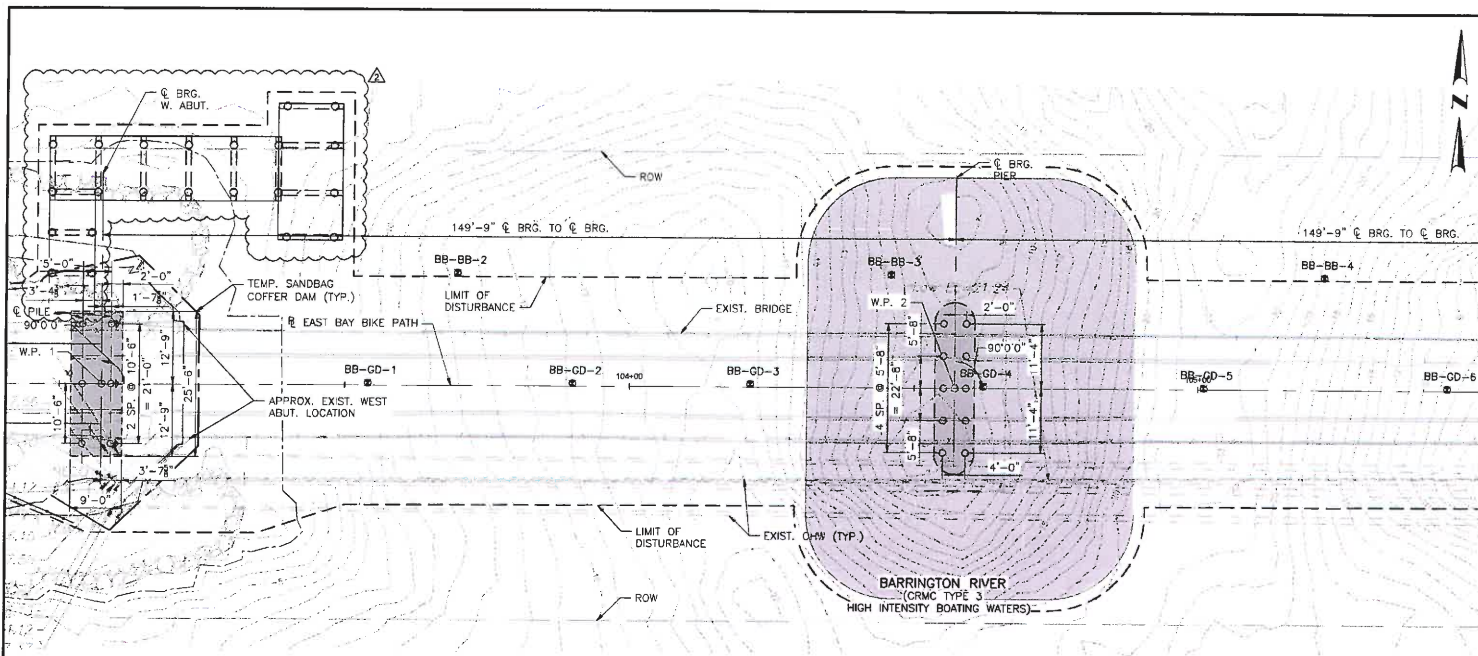
RHODE ISLAND  
 DEPARTMENT OF TRANSPORTATION

PROJECT  
 MODIFICATIONS SINCE  
 OCTOBER 24, 2023  
 GRAD FULL COUNCIL  
 PUBLIC HEARING

DESIGNED BY:  
 CHECKED BY:  
 DATE:  
 SHEET:  
 OF:

REVISIONS				REVIS
NO.	DATE	BY	NO.	DATE
1	3/1/24	vhb		

RECEIVED  
 4/18/2024  
 CIVIL ENGINEERING  
 BARRINGTON PROFILE



**FOUNDATION PLAN**  
SCALE: 1"=10'

**LEGEND**

- PROPOSED FOUNDATION
- PIER CAP
- PROPOSED DRILLED MICROPILE (9.625" DIA.) (BATTERED 4:12)
- PROPOSED DRILLED MICROPILE (9.625" DIA. AT ABUTMENT, 14" DIA. AT PIER) (VERTICAL)
- PROPOSED DRILLED STEEL PIPE PILE AT FISHING PIER

EARLY RELEASE FOR  
CONSTRUCTION  
(ERC 2)  
JUNE 2023



**RHODE ISLAND**  
**DEPARTMENT OF TRANSPORTATION**

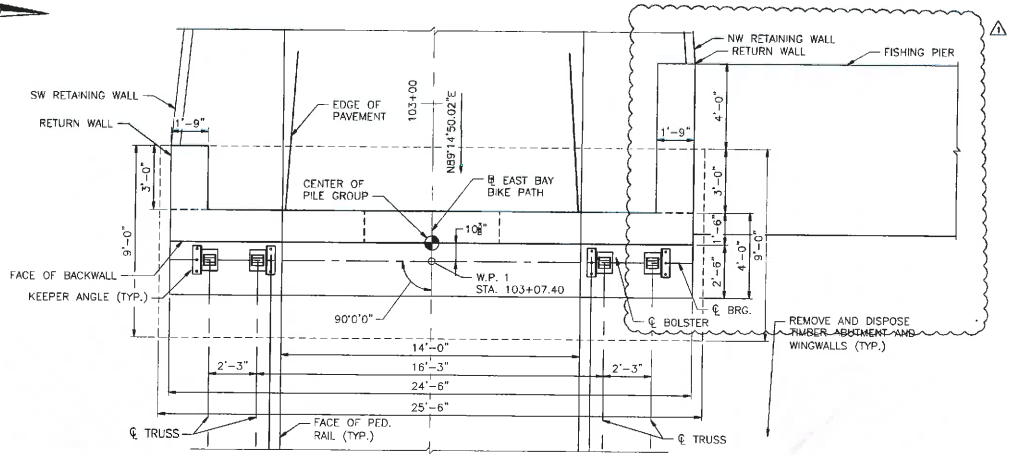
PROJECT  
MODIFICATIONS SINCE  
OCTOBER 24, 2023  
CRMC FULL COUNCIL  
PUBLIC HEARING

DESIGNED BY:  
CHECKED BY:  
DATE:  
SHEET:  
OF:

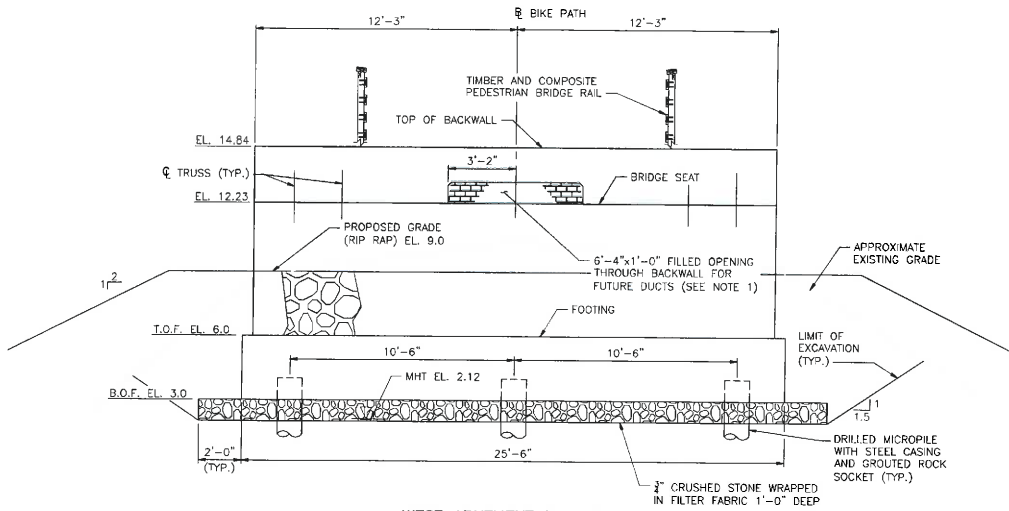
SCALE:

REVISIONS				REVISED	
NO.	DATE	BY	NO.	DA	
10	2/7/24	vhb			
36	4/18/24	vhb			

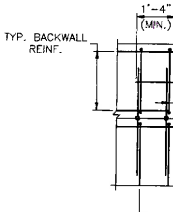




**WEST ABUTMENT PLAN**  
SCALE: 3/8"=1'-0"



**WEST ABUTMENT ELEVATION**  
SCALE: 3/8"=1'-0"



BACK

FRONT OF BA  
FRONT FACE OF ABUT. STEM

**NOTE:**  
1. FILL FUTURE DUCT OPE AND MORTAR (M.04.03).

**AETNA**  
Aetna Bridge Company

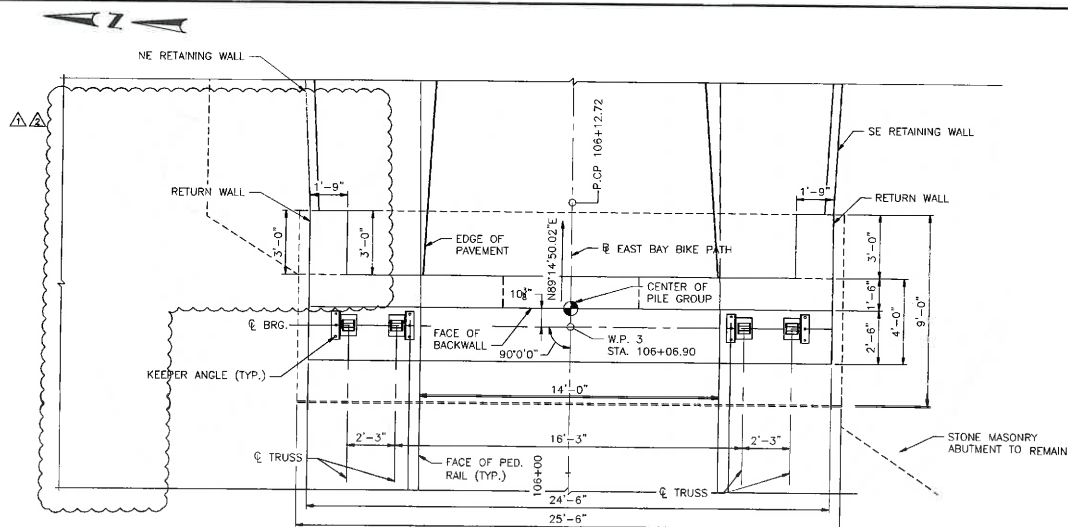
**vhb**  
1 Cedar Street  
Suite 400  
Providence, RI 02903  
401.272.8100



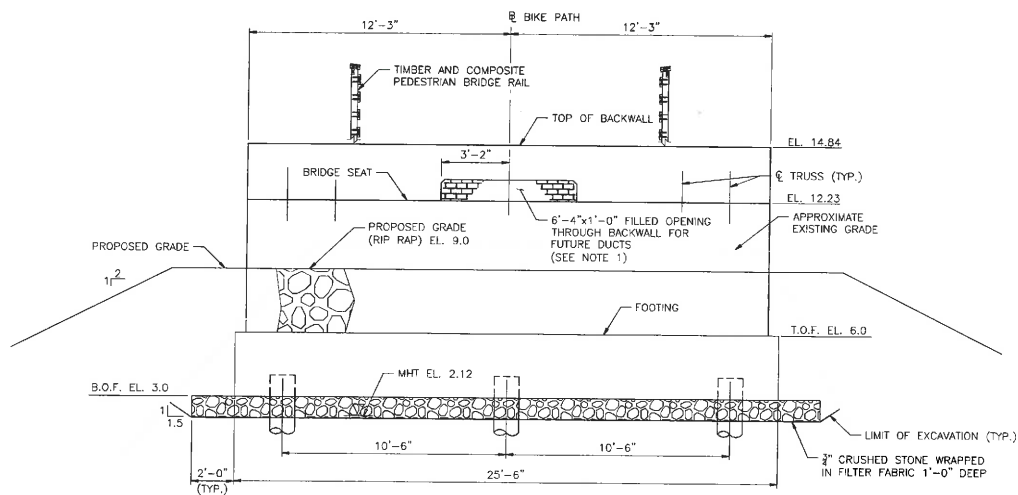
**RHODE ISLAND**  
**DEPARTMENT OF TRANSPORTATION**

DESIGNED BY:	SCALE:
CHECKED BY:	
DATE:	
SHEET:	
OF:	
REVISIONS	
NO.	DATE
11	4/16/24
38	

RECEIVED  
4/18/2024  
CONTRACT ADMINISTRATION



**EAST ABUTMENT PLAN**  
SCALE: 3/8"=1'-0"



**EAST ABUTMENT ELEVATION**  
SCALE: 3/8"=1'-0"

**NOTE:**

1. FILL FUTURE DUCT OPENING AND MORTAR (M.04.03.5)
2. SEE BR. 083751 WEST ABUTMENT BACKWALL OPENING DETAIL

**AETNA**  
Aetna Bridge Company

**vhb**  
1 Cedar Street  
Suite 400  
Providence, RI 02903  
401.272.6100

**RI**  
**DOT**

RHODE ISLAND  
DEPARTMENT OF TRANSPORTATION

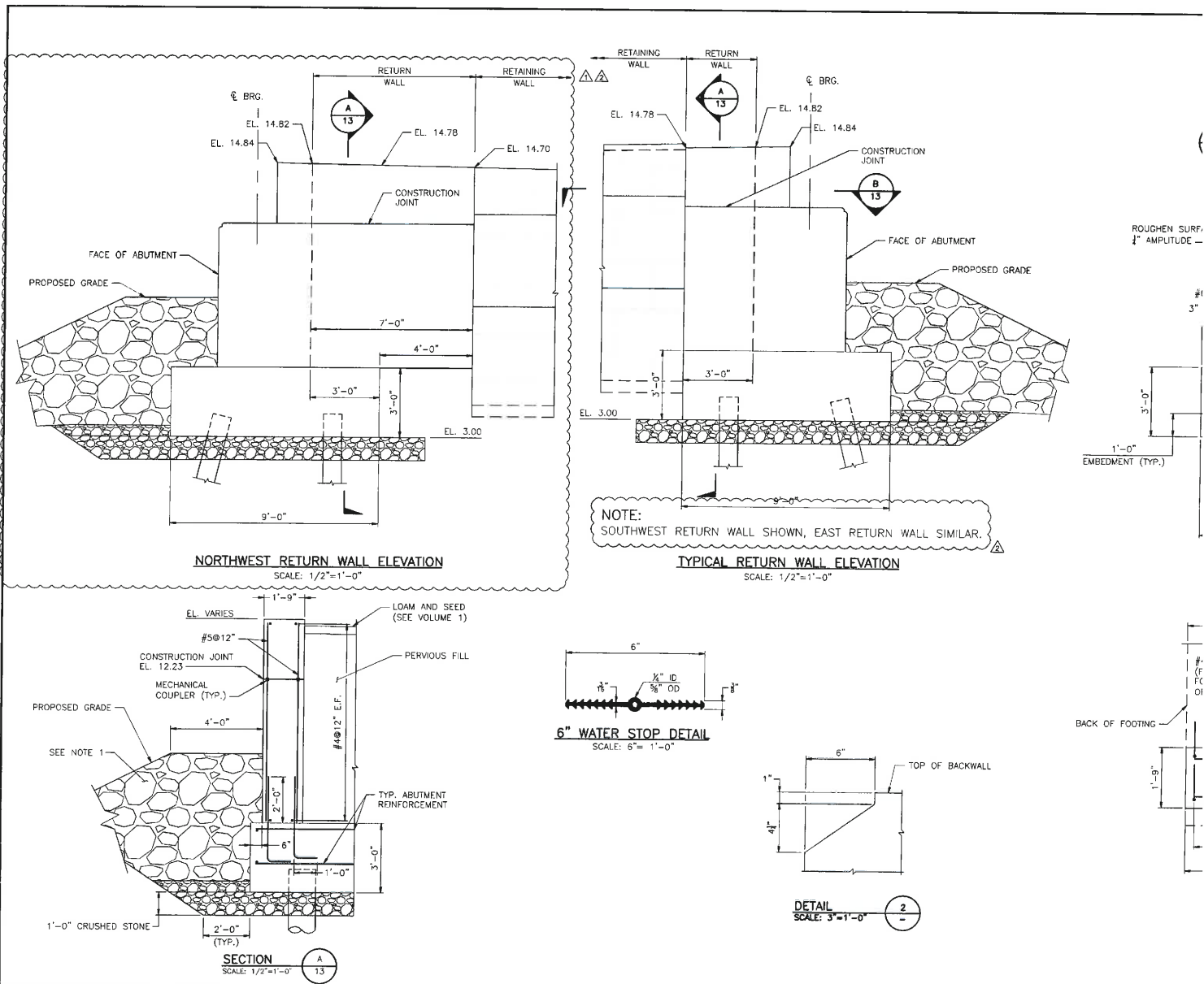
PROJECT  
MODIFICATIONS SINCE  
OCTOBER 24, 2023  
CMAA FULL COUNCIL  
PUBLIC HEARING

DESIGNED BY:  
CHECKED BY:  
DATE:  
SHEET:  
OF:

SCALE:

REVISIONS				REVS	
NO.	DATE	BY	NO.	DA	
12	12/1/24	YHB			
38	4/16/24	YHB			

RECEIVED  
4/18/2024  
COUNCIL, PROVISIONAL  
MANAGEMENT BOARD



**AETNA**  
Aetna Bridge Company

**vhb**  
1 Cedar Street  
Suite 400  
Providence, RI 02903  
401.272.8100

**RI**  
**DOT**

RHODE ISLAND  
DEPARTMENT OF TRANSPORTATION

PROJECT  
MODIFICATIONS SINCE  
OCTOBER 24, 2023  
SINCE FULL COUNCIL  
PUBLIC HEARING

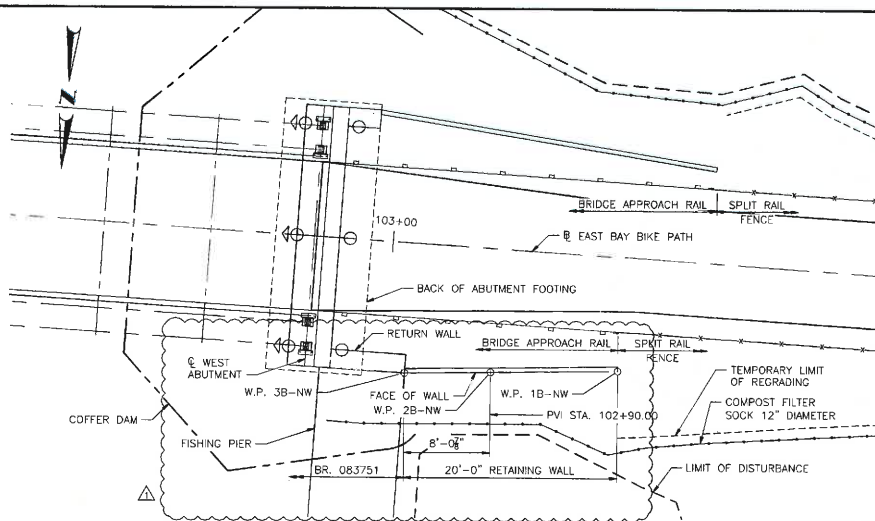
DESIGNED BY:  
CHECKED BY:  
DATE:  
SHEET:  
OF:

SCALE:

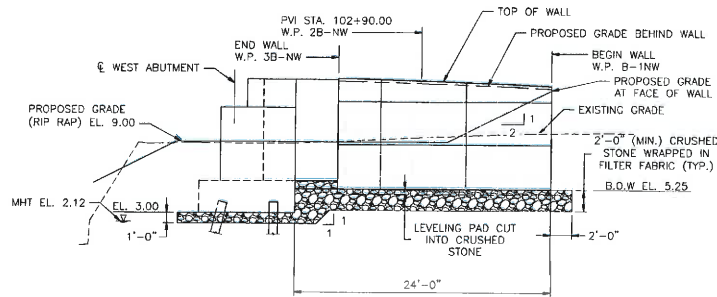
REVISIONS				REVS
NO.	DATE	BY	NO.	DA
13	2/2/24	WB		
38	1/17/24	WB		

RECEIVED  
4/18/2024  
CONTRACT ADMINISTRATION  
BRIDGE DIVISION



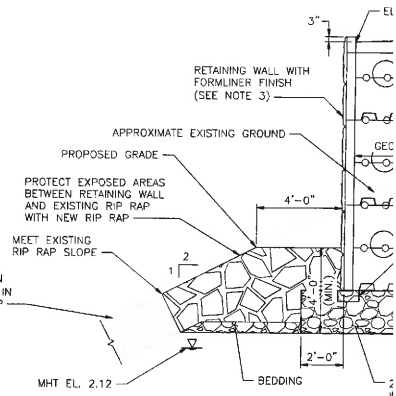


**NORTHWEST RETAINING WALL PLAN**  
SCALE: 3/16"=1'-0"



**NORTHWEST RETAINING WALL ELEVATION**  
SCALE: 3/16"=1'-0"

POSTS TO BE INSTALLED IN REINFORCED BACKFILL. POST LOCATIONS SHALL BE IDENTIFIED PRIOR TO INSTALLING GEOGRID AND REINFORCED BACKFILL. PROVIDE CUTOUTS IN GEOGRID AT POST LOCATIONS. GEOGRID SHALL NOT BE DAMAGED WHEN POST HOLES ARE EXCAVATED.



**TYPICAL RETAINING WALL**  
SCALE: 3/16"=1'-0"

NORTHWEST RETAINING			
WORKING POINT	STATION	OFFSET	NO.
W.P. 1B-NW	102+78.09	-10.69	2384
W.P. 2B-NW	102+90.00	-11.62	2384
W.P. 3B-NW	102+98.04	-12.25	2384

\* ELEVATION IS GIVEN AT PROPOSED GRADE BEHIND WALL. SEE TYPICAL RETAINING WALL.

MINIMUM REQUIRED REINFORCEMENT (GEOGRID) LENGTHS	
RETAINING WALL LOCATION	LENGTH (L)*
NORTHWEST	0.7H
SOUTHWEST	1.15H
NORTHEAST	1.0H
SOUTHEAST	0.8H

\*H = EL. A - B.O.W. EL.

**NOTE:**

NOTE: GEOGRID LENGTH "L" SHALL BE THE MINIMUM LENGTHS SHOWN TO SATISFY GLOBAL STABILITY. WALL DESIGNER'S DESIGN SHALL INCLUDE AN INTERNAL STABILITY ANALYSIS.

**NOTES:**

1. REF. DATA
2. PANE QUAN
3. THE FORM
4. FILTER WALL



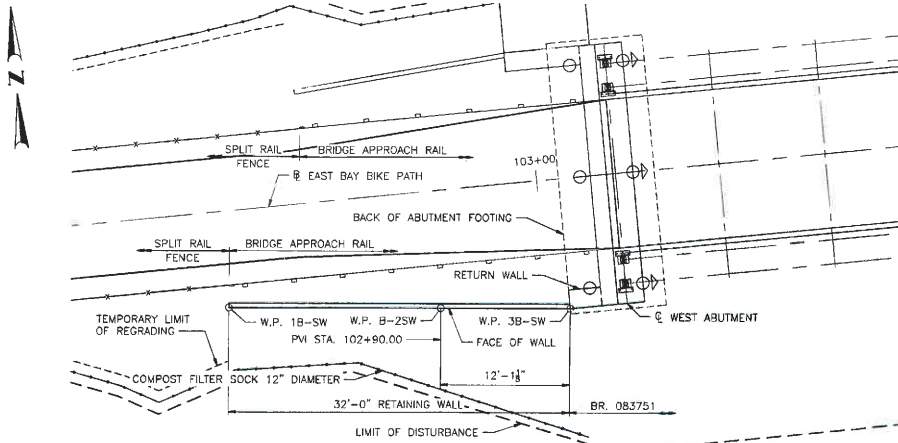
RHODE ISLAND  
DEPARTMENT OF TRANSPORTATION

PROJECT MODIFICATIONS SINCE OCTOBER 24, 2023  
CIRC. FULL COUNCIL PUBLIC HEARING

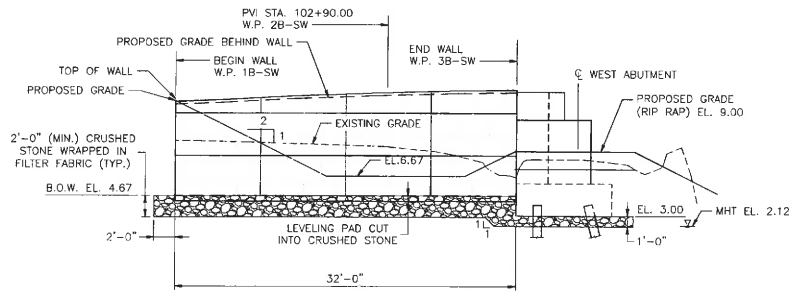
DESIGNED BY:  
CHECKED BY:  
DATE:  
SHEET: 14  
OF: 38

REVISIONS			
NO.	DATE	BY	REVIS
1	4/18/24	vhb	

RECEIVED  
4/18/2024  
CONTRACT ADMINISTRATION



**SOUTHWEST RETAINING WALL PLAN**  
SCALE: 3/16"=1'-0"



**SOUTHWEST RETAINING WALL ELEVATION**  
SCALE: 3/16"=1'-0"

SOUTHWEST R		
WORKING POINT	STATION	OFFSET
W.P. 1B-SW	102+70.19	9.09
W.P. 2B-SW	102+90.00	11.06
W.P. 3B-SW	103+02.04	12.25

\* ELEVATION IS GIVEN AT PROPOSED GRADE BEHIND WALL. SEE TYP.

**NOTES:**

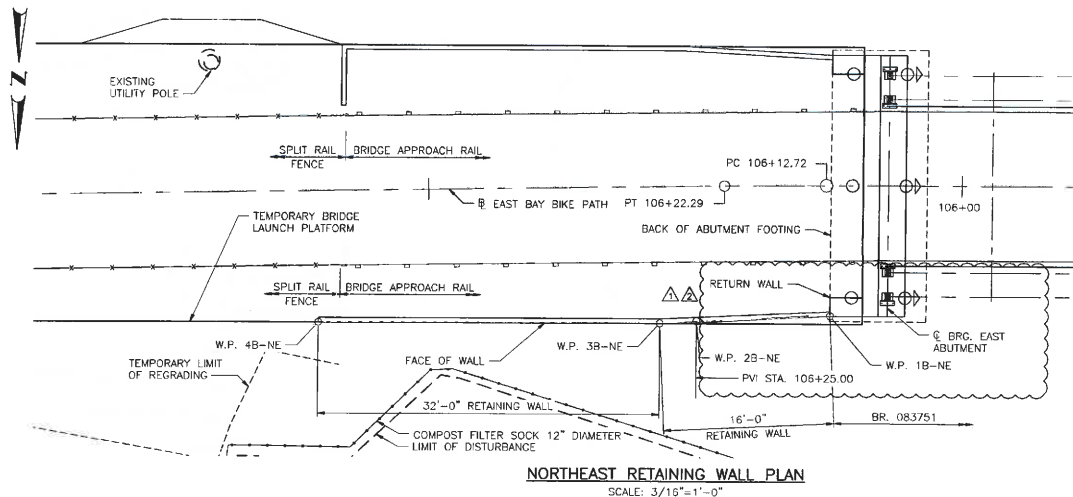
- SEE BR. 083751 RETAINING WALL 1 SHEET FOR TYPICAL RETAIN



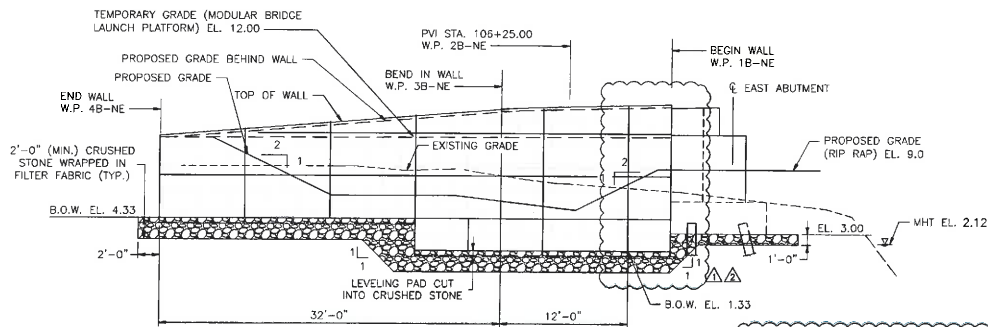
RHODE ISLAND  
DEPARTMENT OF TRANSPORTATION

DESIGNED BY:	SCALE:
CHECKED BY:	
DATE:	
SHEET: 15	
OF: 38	





**NORTHEAST RETAINING WALL PLAN**  
SCALE: 3/16"=1'-0"



**NORTHEAST RETAINING WALL ELEVATION - DEVELOPED**  
SCALE: 3/16"=1'-0"

WORKING POINT	STATION	OFFSET	NO.
W.P. 1B-NE	106+12.27	-12.25	238
W.P. 2B-NE	106+25.00	-12.79	238
W.P. 3B-NE	106+26.45	-12.86	238
W.P. 4B-NE	106+58.45	-12.35	238

\* ELEVATION IS GIVEN AT PROPOSED GRADE BEHIND WALL. SEE TYPICAL RETAINING WALL 1 SHEET FOR TYPICAL RETAINING WALL 1.

**NOTES:**

- SEE BR. 083751 RETAINING WALL 1 SHEET FOR TYPICAL RETAINING WALL 1.



**RHODE ISLAND**  
**DEPARTMENT OF TRANSPORTATION**

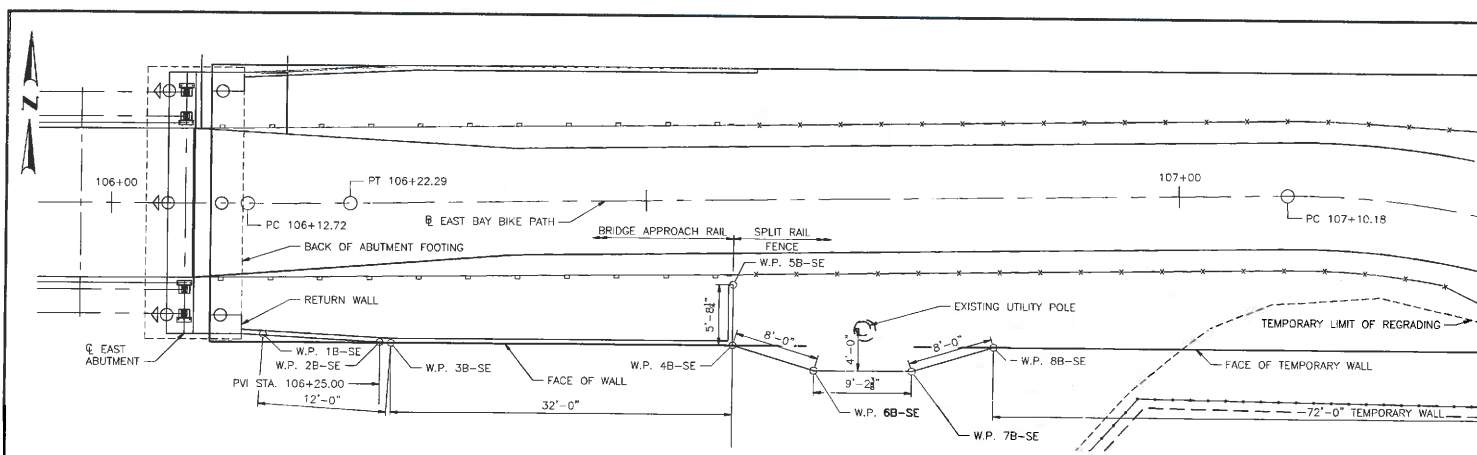
PROJECT  
MODIFICATIONS SINCE  
OCTOBER 24, 2023  
GRAP FULL COUNCIL  
PUBLIC HEARING.

DESIGNED BY:  
CHECKED BY:  
DATE:  
SHEET:  
OF:

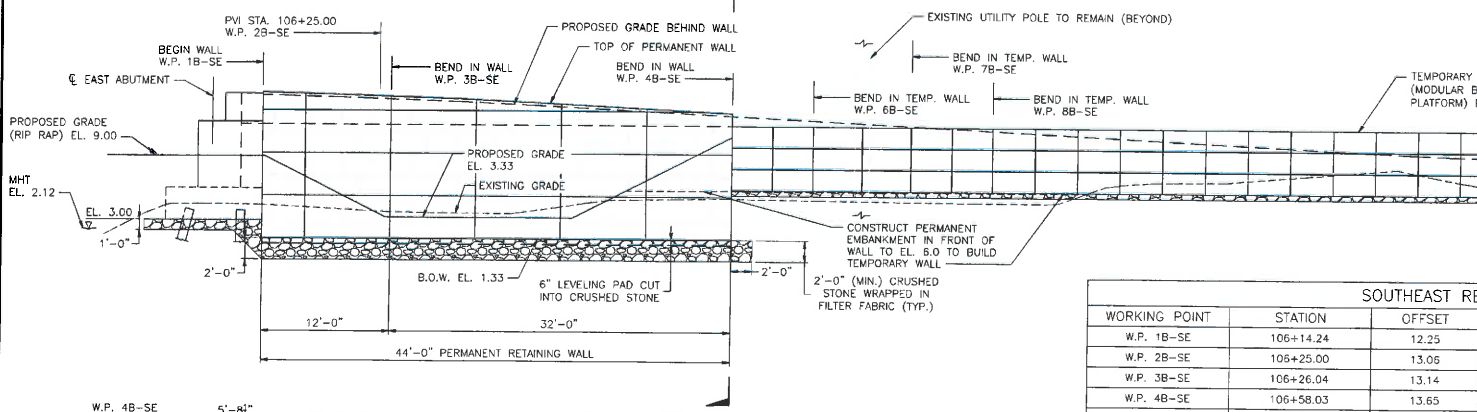
SCALE:

NO.	DATE	BY	NO.	DATE
1	3/2/24	vhb		
2	4/18/24	vhb		

RECEIVED  
4/18/2024  
COUNTY INFORMATION  
MANAGEMENT SYSTEMS



**SOUTHEAST RETAINING WALL PLAN**  
SCALE: 3/16"=1'-0"



**SOUTHEAST RETAINING WALL ELEVATION**  
SCALE: 3/16"=1'-0"

WORKING POINT	STATION	OFFSET
W.P. 1B-SE	106+14.24	12.25
W.P. 2B-SE	106+25.00	13.05
W.P. 3B-SE	106+26.04	13.14
W.P. 4B-SE	106+58.03	13.65
W.P. 5B-SE	106+58.12	7.96
W.P. 6B-SE	106+65.65	16.08
W.P. 7B-SE	106+74.85	16.23
W.P. 8B-SE	106+82.54	14.04
W.P. 9B-SE	107+57.66	2.75

\* ELEVATION IS TAKEN AT TOP OF PROPOSED GRADE BEHIND WALL.  
\*\* THIS GRADE IS TEMPORARY FOR THE LAUNCHING PLATFORM.

**NOTES:**  
1. SEE BR. 083751 RETAINING WALL 1

**AETNA**  
Aetna Bridge Company

**vhb**  
1 Cedar Street  
Suite 400  
Providence, RI 02903  
401.272.8100

**RI**  
**DOT**

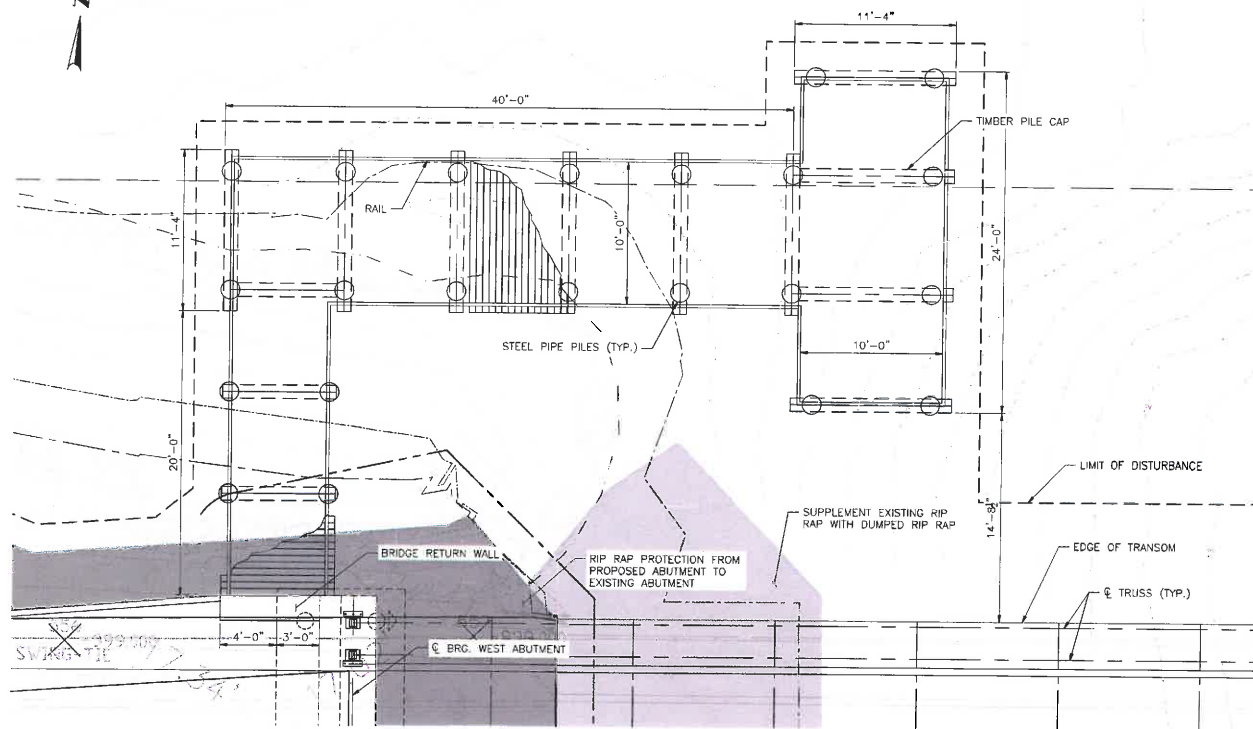
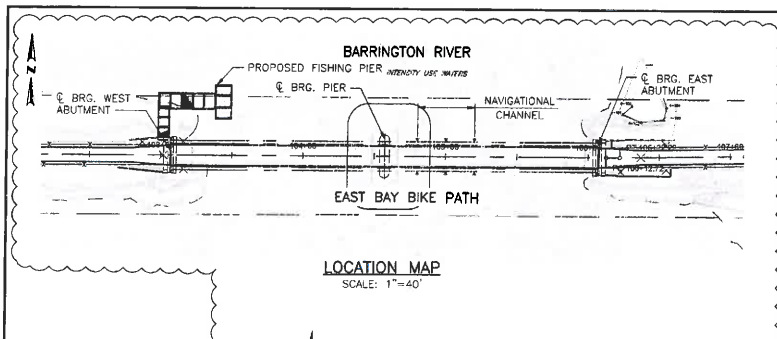
**RHODE ISLAND**  
**DEPARTMENT OF TRANSPORTATION**

DESIGNED BY:	SCALE:
CHECKED BY:	
DATE:	
SHEET:	
OF:	

RECEIVED  
4/18/2024  
CONSTRUCTION MANAGEMENT DIVISION







**AETNA**  
Aetna Bridge Company

**vhb**  
1 Cedar Street  
Suite 400  
Providence, RI 02903  
401.272.8100

**RI**  
**DOT**

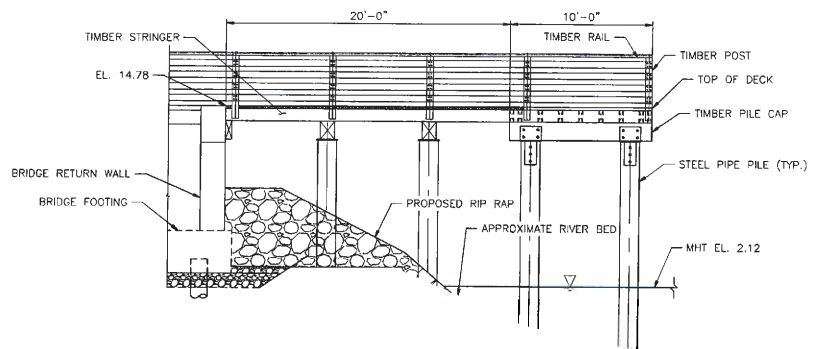
RHODE ISLAND  
DEPARTMENT OF TRANSPORTATION

PROJECT  
MODIFICATIONS SINCE  
OCTOBER 24, 2023  
C.M.C. FULL COUNCIL  
PUBLIC HEARING

DESIGNED BY:  
CHECKED BY:  
DATE:  
SHEET:  
OF:

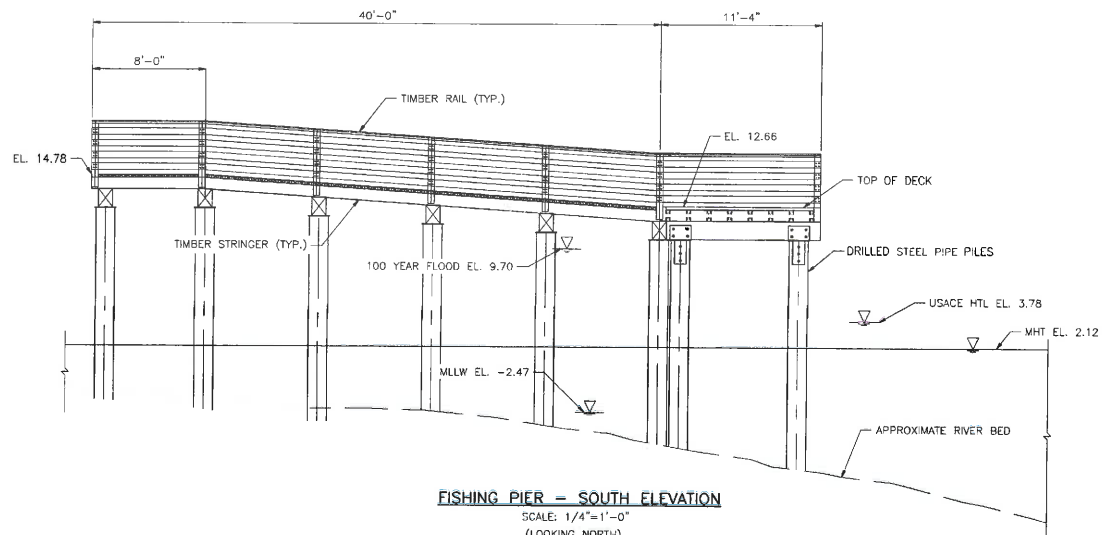
REVISIONS					REVIS
NO.	DATE	BY	NO.	DA	
19	2/2/24	vhb			
38	4/15/24	vhb			

RECEIVED  
4/18/2024  
COUNTY ENGINEER  
MANAGEMENT SYSTEMS



**FISHING PIER - EAST ELEVATION**

SCALE: 1/4"=1'-0"  
(LOOKING WEST)



**FISHING PIER - SOUTH ELEVATION**

SCALE: 1/4"=1'-0"  
(LOOKING NORTH)



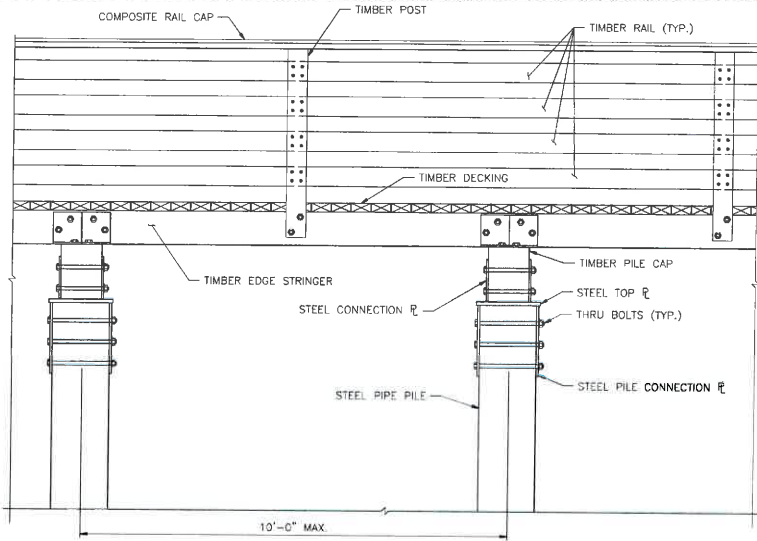
RHODE ISLAND  
DEPARTMENT OF TRANSPORTATION

PROJECT  
MODIFICATIONS SINCE  
OCTOBER 24, 2023  
ORIG. FULL COUNCIL  
PUBLIC HEARING.

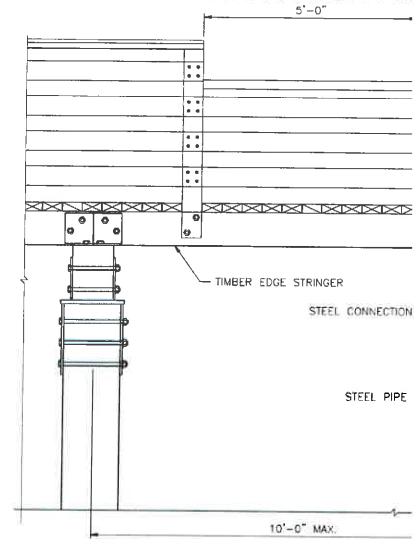
DESIGNED BY:  
CHECKED BY:  
DATE:  
SHEET:  
OF:

SCALE:				
REVISIONS				
NO.	DATE	BY	NO.	DATE
1	2/7/24	vhb		
2	4/17/24	vhb		

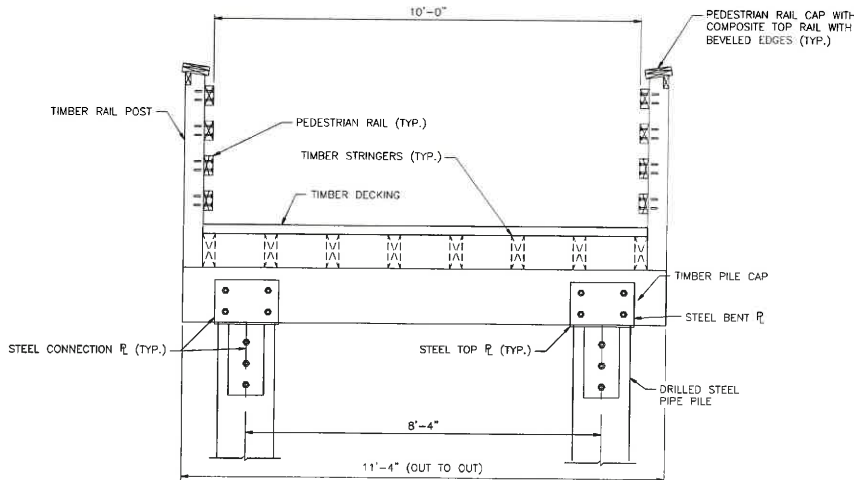
RECEIVED  
4/18/2024  
COUNCIL MEMBERS  
MANAGEMENT COUNCIL



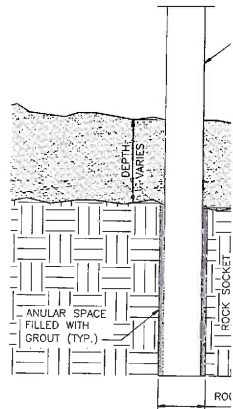
**TYPICAL LONGITUDINAL SECTION**  
SCALE: 3/4"=1'-0"



**HANDICAP RAMP**  
SCALE: 3/4"=1'-0"



**TYPICAL PIER SECTION**  
SCALE: 3/4"=1'-0"



**PILE ROCK SOCKET**  
SCALE: 1/2"=1'-0"



**RHODE ISLAND**  
**DEPARTMENT OF TRANSPORTATION**

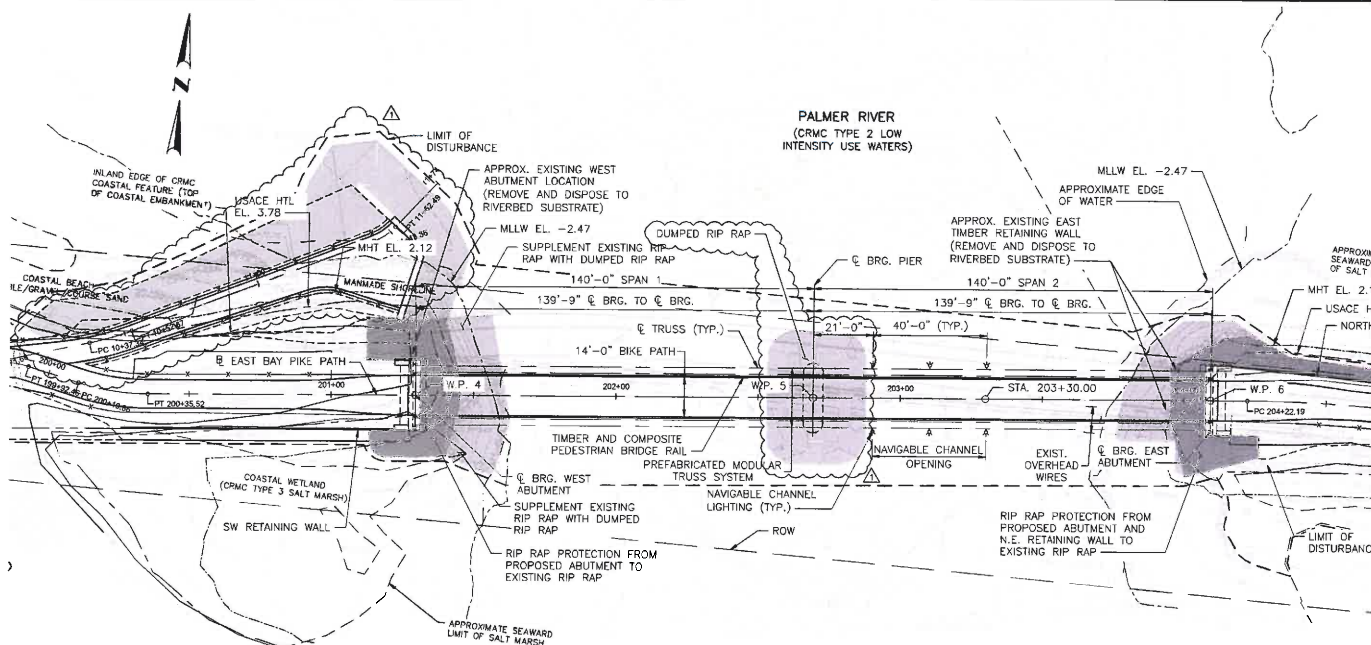
PROJECT  
MODIFICATIONS SINCE  
OCTOBER 24, 2023  
CIVIC RAIL COUNCIL  
PUBLIC HEARING

DESIGNED BY:  
CHECKED BY:  
DATE:  
SHEET:  
OF:

SCALE:

REVISIONS					REVIS
NO.	DATE	BY	NO.	DATE	
21	2/1/24	vhb			
38					

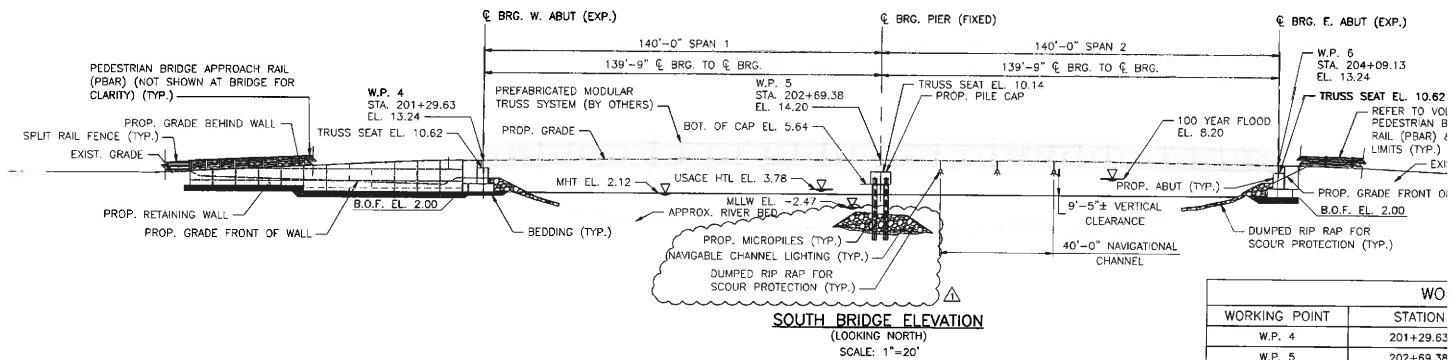




**LEGEND:**

- PLACED RIP RAP
- DUMPED RIP RAP

**ROUTE 114**  
**PALMER RIVER**  
 (CRMC TYPE 3  
 HIGH INTENSITY BOATING WATERS)  
**BRIDGE GENERAL PLAN**  
 SCALE: 1"=20'



WORKING POINT		STATION
W.P. 4		201+29.63
W.P. 5		202+69.38
W.P. 6		204+09.13



**vhb**  
 1 Cedar Street  
 Suite 400  
 Providence, RI 02903  
 401.272.8100



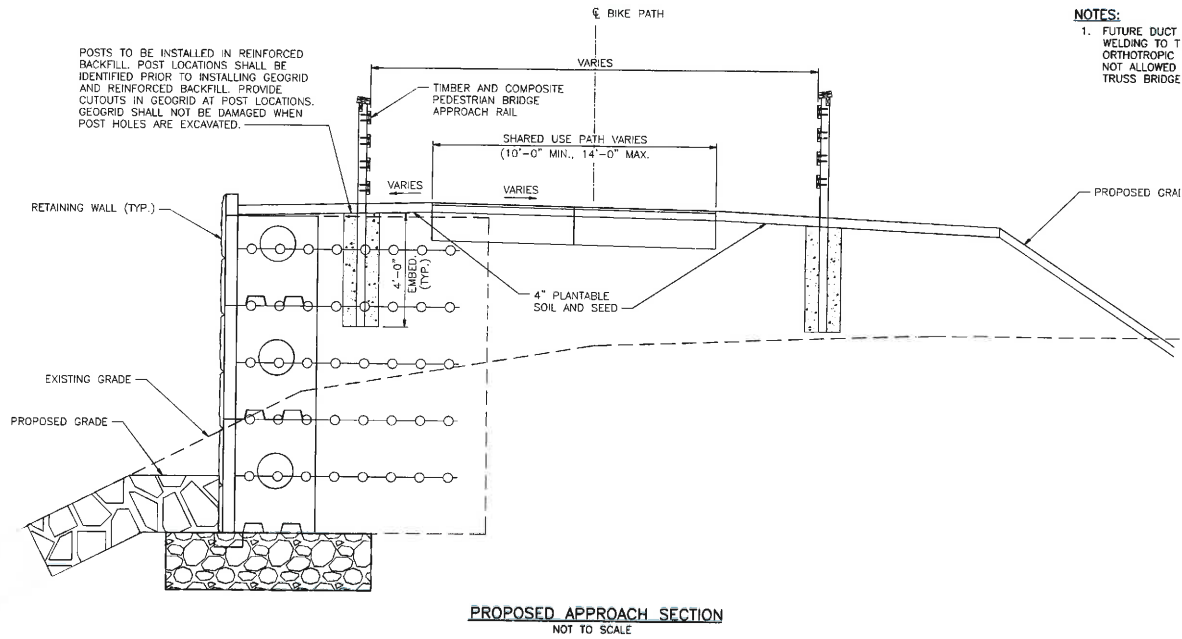
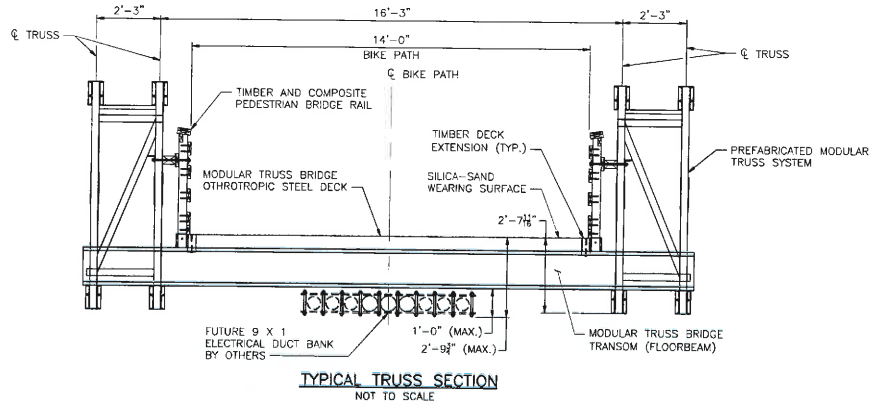
**RHODE ISLAND**  
**DEPARTMENT OF TRANSPORTATION**

PROJECT  
 MODIFICATIONS SINCE  
 OCTOBER 24, 2023  
 CRMC FULL COUNCIL  
 PUBLIC HEARING

DESIGNED BY:		CHECKED BY:		DATE:		SHEET:		OF:	
						22		38	

REVISIONS				BY	NO.	DATE
1				vhb		2/2/24

RECEIVED  
 4/18/2024  
 CRMC FULL COUNCIL  
 PUBLIC HEARING



**NOTES:**  
1. FUTURE DUCT WELDING TO 1 ORTHOTROPIC NOT ALLOWED TRUSS BRIDGE

**AETNA**  
Aetna Bridge Company

**vhb**  
1 Cedar Street  
Suite 400  
Providence, RI 02903  
401.272.8100

**RI**  
**DOT**

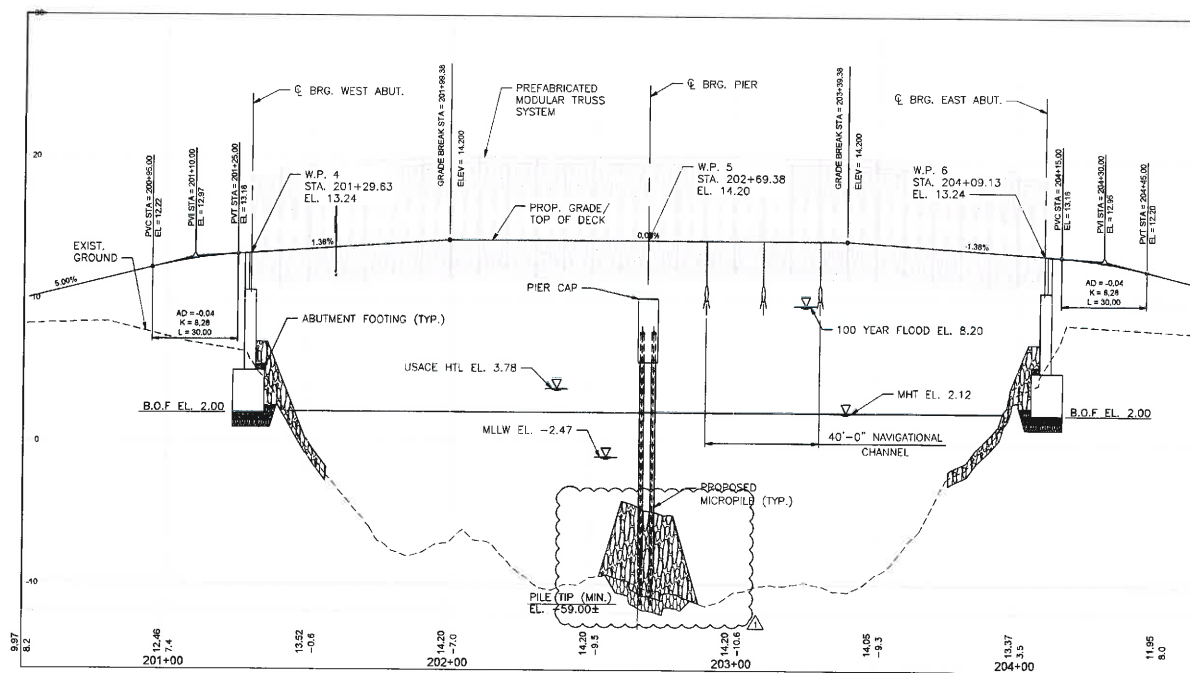
RHODE ISLAND  
DEPARTMENT OF TRANSPORTATION

DESIGNED BY:	SCALE:
CHECKED BY:	
DATE:	
SHEET: 23	
OF: 38	

REVISIONS				REVIS
NO.	DATE	BY	NO.	DA

RECEIVED  
4/18/2024  
COUNTY INFORMATION  
MANAGEMENT SYSTEM





**WARREN PROFILE**  
 SCALE HORIZONTAL: 1"=20'  
 SCALE VERTICAL: 1"=4'

NOT  
 REFEI



**vhb**  
 1 Cedar Street  
 Suite 400  
 Providence, RI 02903  
 401.272.8100



**RHODE ISLAND**  
 DEPARTMENT OF TRANSPORTATION

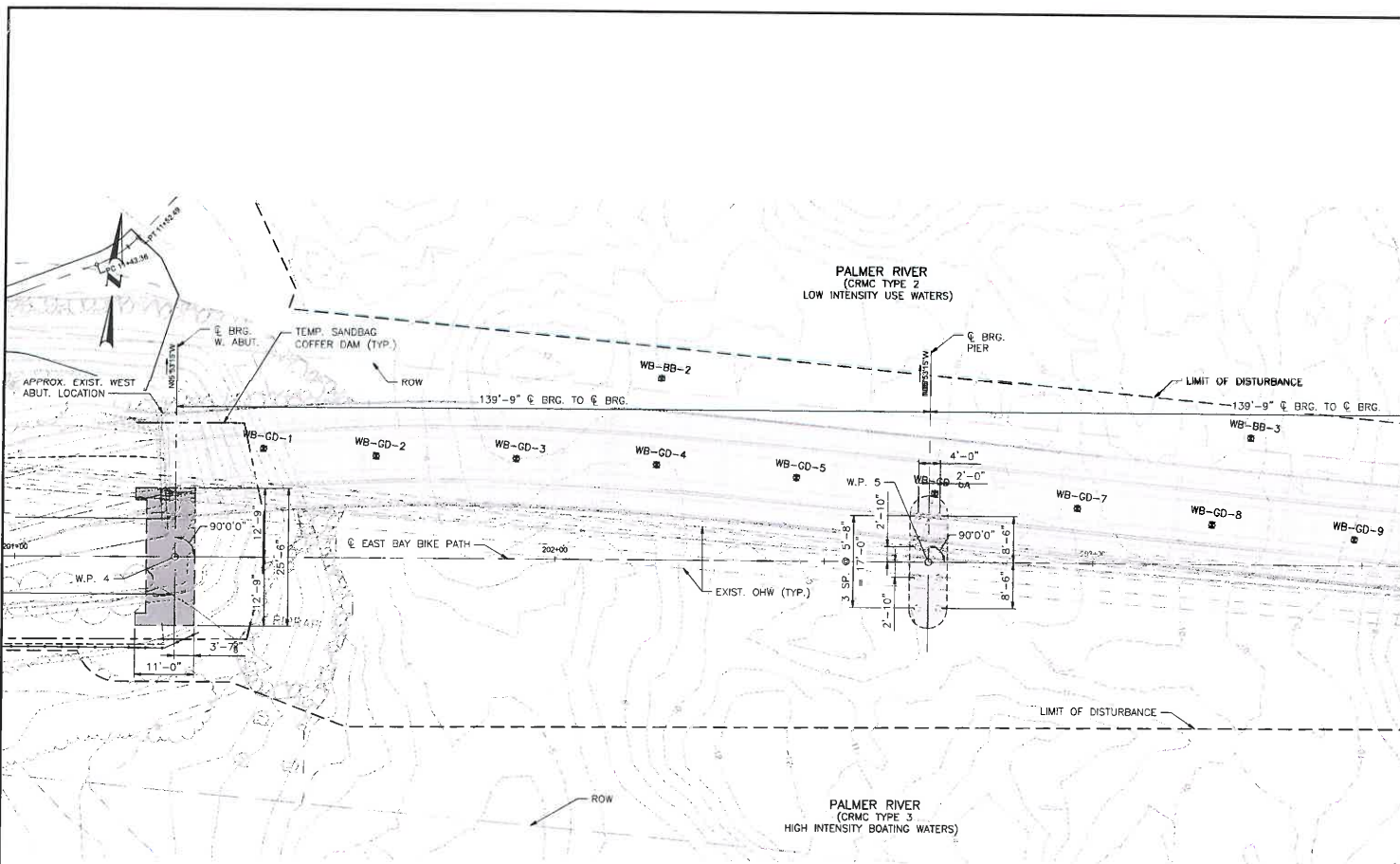
PROJECT  
 MODIFICATIONS SINCE  
 OCTOBER 24, 2023  
 CRMC FULL COUNCIL  
 PUBLIC HEARING

DESIGNED BY:  
 CHECKED BY:  
 DATE:  
 SHEET:  
 OF:

SCALE:

REVISIONS				REVIS
NO.	DATE	BY	NO.	DA
1	2/1/24	vhb		





FOUNDATION PLAN  
SCALE: 1"=10'

EARLY RELEASE FOR  
CONSTRUCTION  
(ERC 2)  
JUNE 2023

LEGEND

- PROPOSED FOUNDATION
- PIER CAP
- PROPOSED DRILLED MICROPILE (14" DIA.) (VERTICAL)

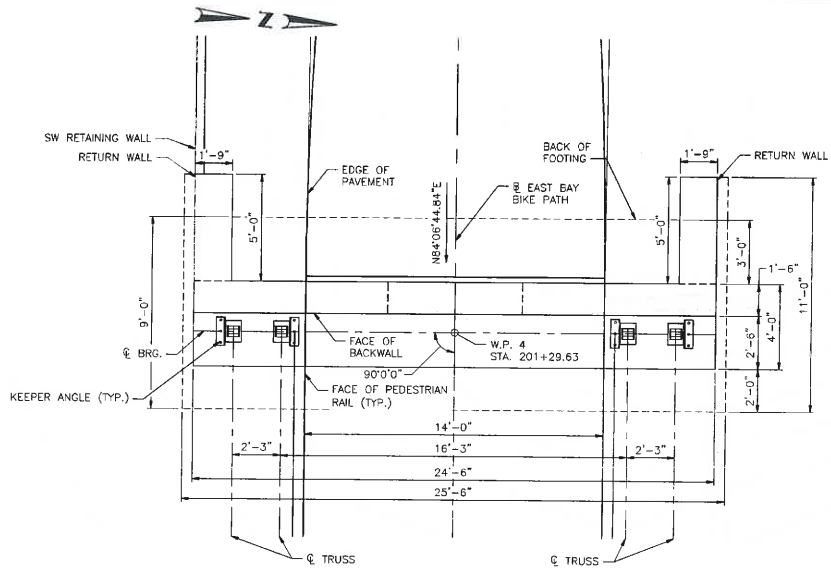


RHODE ISLAND  
DEPARTMENT OF TRANSPORTATION

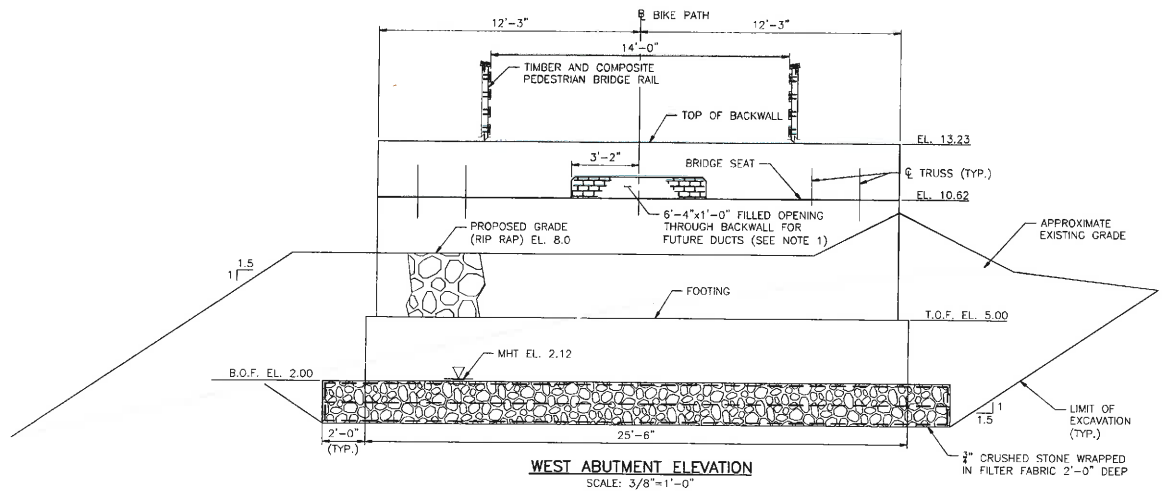
DESIGNED BY:		SCALE:	
CHECKED BY:			
DATE:			
SHEET:	25		
OF:	38		

REVISIONS				REVIS
NO.	DATE	BY	NO.	DA





**WEST ABUTMENT PLAN**  
SCALE: 3/8"=1'-0"



**WEST ABUTMENT ELEVATION**  
SCALE: 3/8"=1'-0"

**NOTE:**

1. FILL FUTURE AND MORTAR
2. CRUSHED ST MIRAFI HP 5
3. SEE BR. 08, OPENING DE



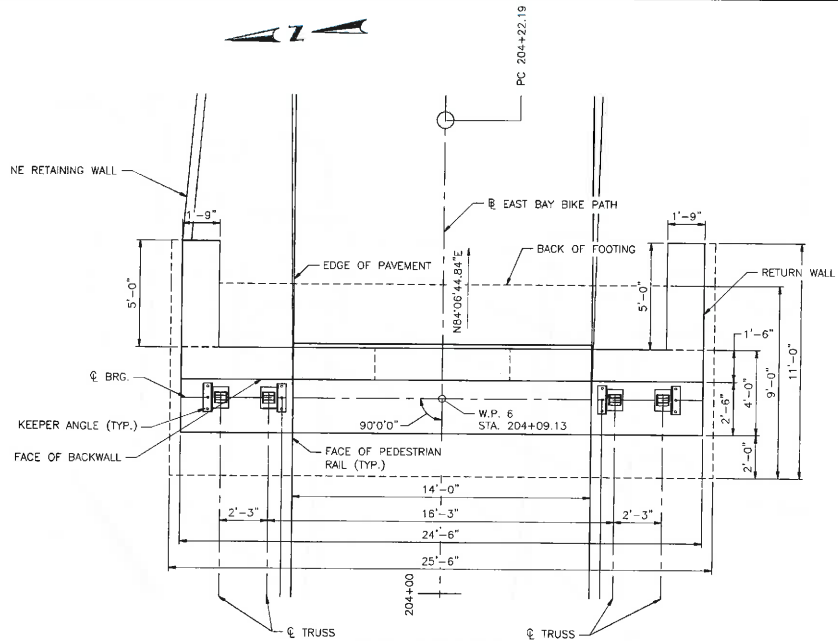
**vhb**  
1 Cedar Street  
Suite 400  
Providence, RI 02903  
401.272.8100



**RHODE ISLAND**  
**DEPARTMENT OF TRANSPORTATION**

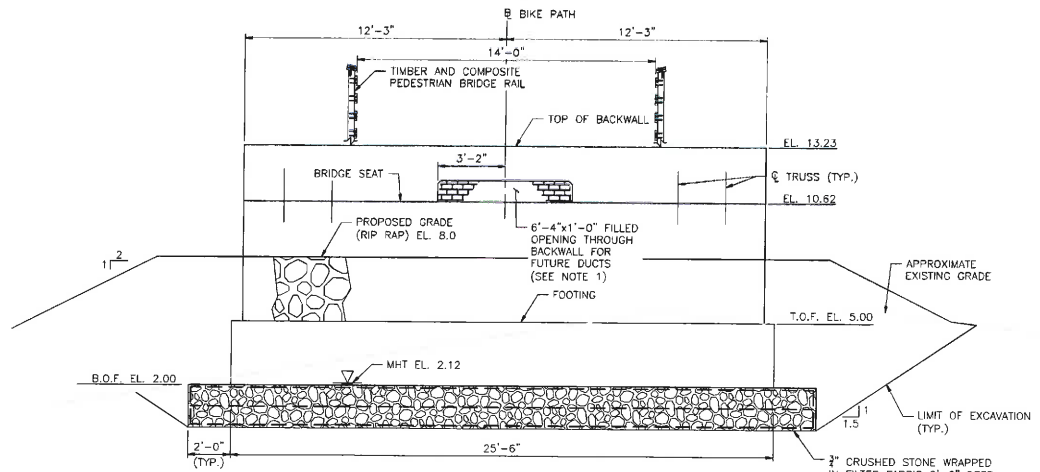
DESIGNED BY:	SCALE:					
CHECKED BY:						
DATE:	REVISIONS			REVIS		
SHEET:	26	NO.	DATE	BY	NO.	DA
OF:	38					

RECEIVED  
4/18/2024  
CONTRACT ADMINISTRATION  
MANAGEMENT OFFICE



**EAST ABUTMENT PLAN**

SCALE: 3/8"=1'-0"



**EAST ABUTMENT ELEVATION**

SCALE: 3/8"=1'-0"

**NOTE:**

1. FILL FUTURE DUCTS WITH FILL AND MORTAR (SEE BR. 0837: OPENING DETAIL)
2. CRUSHED STONE MIRAFI HP 570
3. SEE BR. 0837: OPENING DETAIL



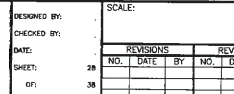
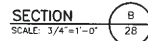
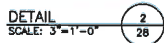
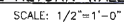
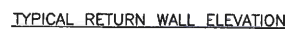
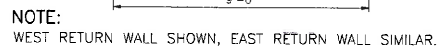
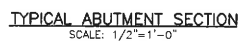
**vhb**  
1 Cedar Street  
Suite 400  
Providence, RI 02903  
401.272.6100

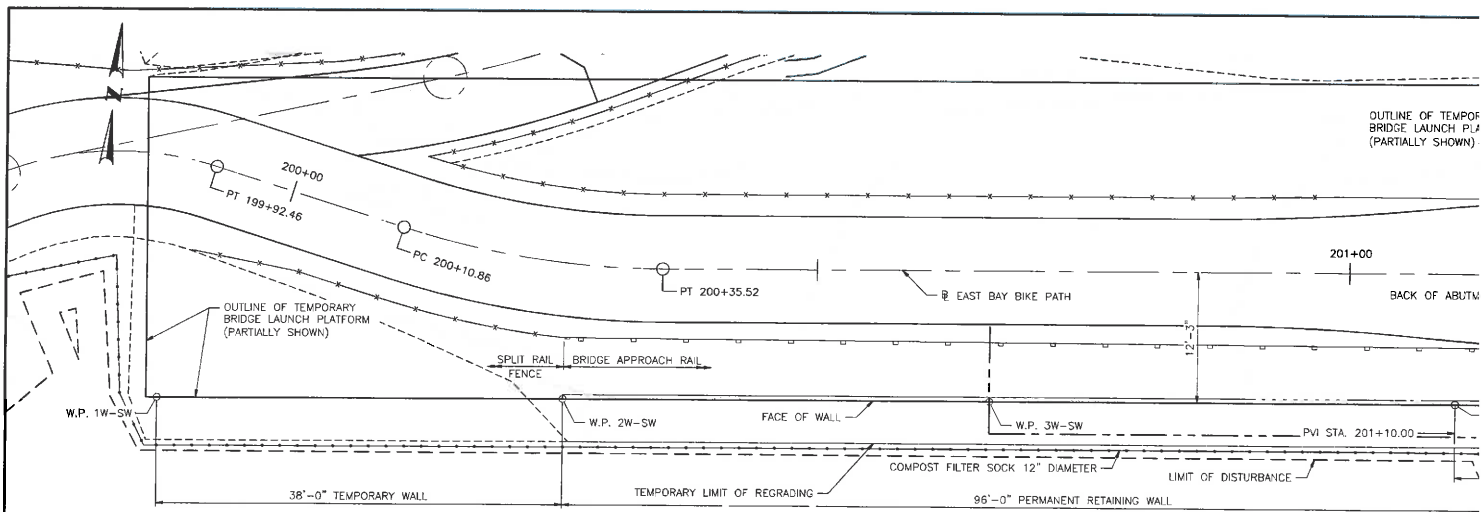


**RHODE ISLAND**  
**DEPARTMENT OF TRANSPORTATION**

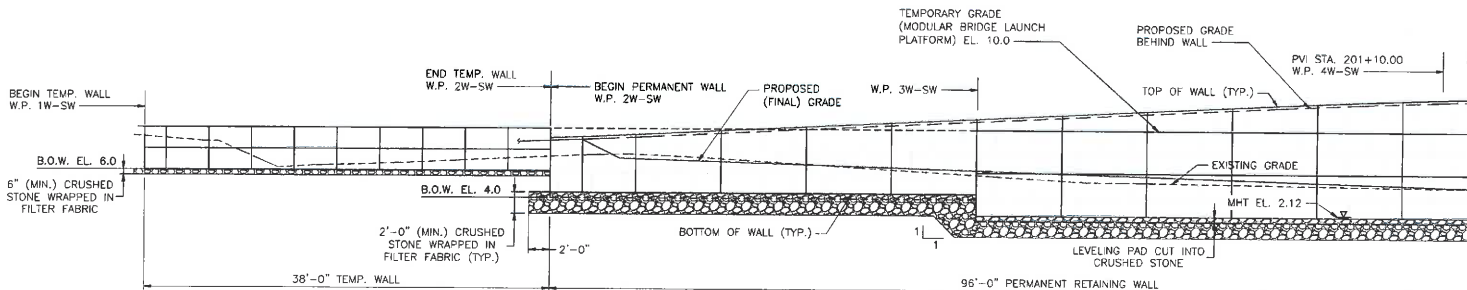
DESIGNED BY:		SCALE:																					
CHECKED BY:																							
DATE:																							
SHEET:	27																						
OF:	38																						
<table border="1"> <thead> <tr> <th colspan="3">REVISIONS</th><th>REVISED BY</th></tr> <tr> <th>NO.</th><th>DATE</th><th>BY</th><th>NO.</th></tr> </thead> <tbody> <tr> <td> </td><td> </td><td> </td><td> </td></tr> <tr> <td> </td><td> </td><td> </td><td> </td></tr> <tr> <td> </td><td> </td><td> </td><td> </td></tr> </tbody> </table>				REVISIONS			REVISED BY	NO.	DATE	BY	NO.												
REVISIONS			REVISED BY																				
NO.	DATE	BY	NO.																				







**SOUTHWEST RETAINING WALL PLAN**  
SCALE: 3/16"=1'-0"



**SOUTHWEST RETAINING WALL ELEVATION**  
SCALE: 3/16"=1'-0"

SOUTHWEST RETAINING WALL					
WORKING POINT	STATION	OFFSET	NORTHING	EASTING	EL. A*
W.P. 1W-SW	199+93.82	22.24	238477.9197	3852381.3009	10.00**
W.P. 2W-SW	200+27.52	12.72	238481.8185	385319.1003	8.85
W.P. 3W-SW	200+66.27	12.25	238485.9228	385358.8892	10.78
W.P. 4W-SW	201+10.00	12.25	238490.4098	385402.3925	12.83
W.P. 5W-SW	201+22.27	12.25	238491.6683	385414.5937	13.13

\* ELEVATION IS GIVEN AT PROPOSED GRADE BEHIND WALL. SEE TYPICAL RETAINING WALL SECTION.  
\*\* THIS GRADE IS TEMPORARY FOR THE LAUNCHING PLATFORM

- NOTES:**
1. REFER TO HIGHWAY DATA FOR BIKE PA
  2. PANEL SLIP JOINTS QUANTITY SHALL BE
  3. THE EXPOSED FACI FORMLINER FINISH.
  4. FILTER FABRIC FOR WALLS SHALL BE &
  5. SEE BR. 083851 R SECTION.

**AETNA**  
Aetna Bridge Company

**vhb**  
1 Cedar Street  
Suite 400  
Providence, RI 02903  
401.272.8100

**RI**  
**DOT**

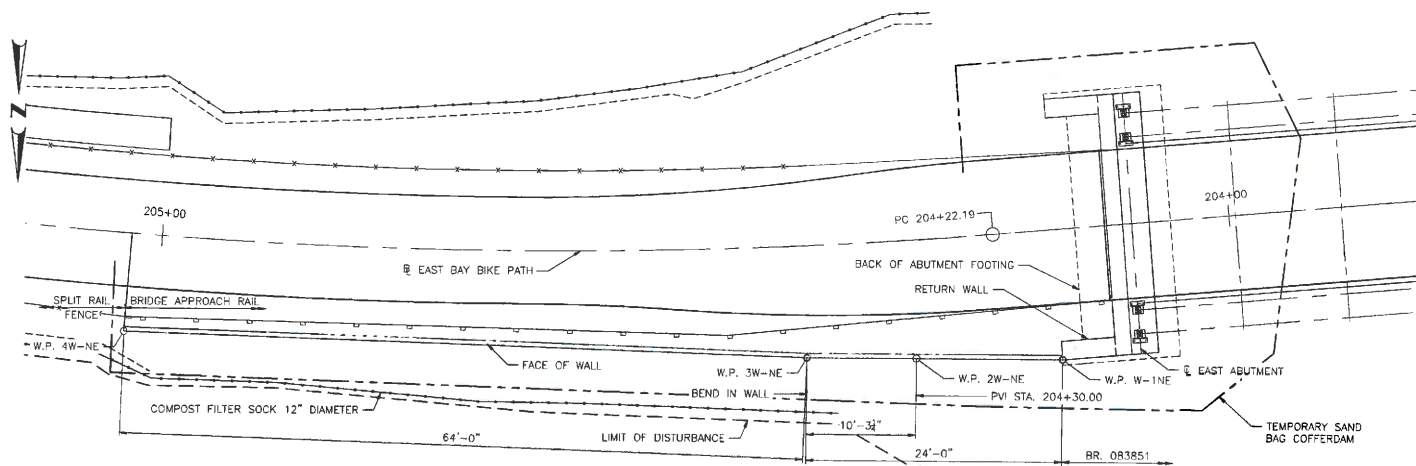
RHODE ISLAND  
DEPARTMENT OF TRANSPORTATION

DESIGNED BY:	SCALE:
CHECKED BY:	
DATE:	
SHEET:	26
OF:	38

REVISIONS		REVISED	
NO.	DATE	BY	NO.

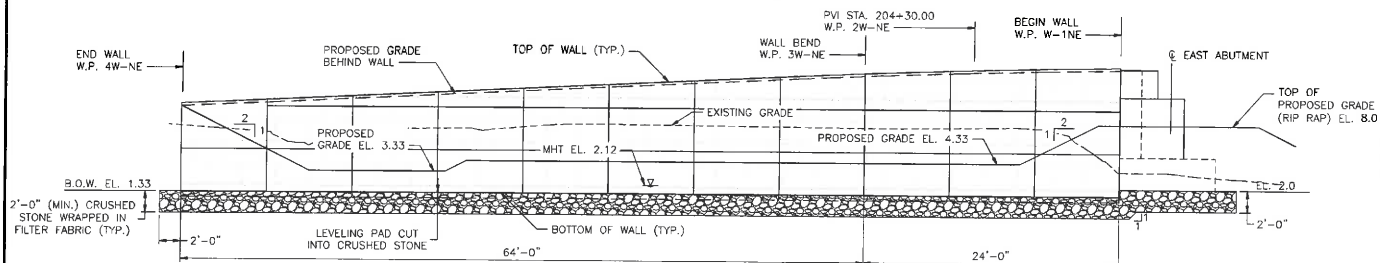
RECEIVED  
4/18/2024  
CONTRACT ADMINISTRATION





**NORTHEAST RETAINING WALL PLAN**

SCALE: 3/16"=1'-0"



**NORTHEAST RETAINING WALL ELEVATION - DEVELOPED**

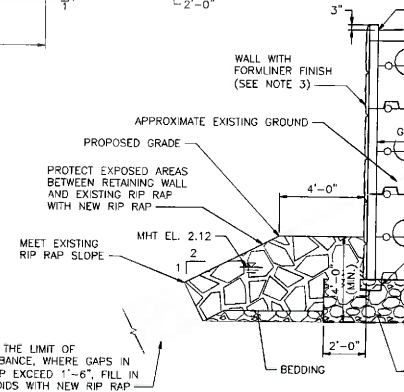
SCALE: 3/16"=1'-0"

**NOTES:**

1. SEE BR. 083851 RETAINING WALL PLAN 1 FOR NOTES.

NORTHEAST RETAINING WALL					
WORKING POINT	STATION	OFFSET	NORTHING	EASTING	EL. A*
W.P. 1W-NE	204+16.50	-12.25	238546.2201	385704.7577	13.15
W.P. 2W-NE	204+30.00	-11.13	238546.4429	385718.4901	12.84
W.P. 3W-NE	204+40.03	-10.50	238546.6093	385728.7545	12.47
W.P. 4W-NE	205+02.83	-9.20	238545.5894	385792.7464	9.34

\* ELEVATION IS GIVEN AT PROPOSED GRADE BEHIND WALL. SEE TYPICAL RETAINING WALL SECTION.



**TYPICAL RETAINING WALL SECTION**

SCALE:

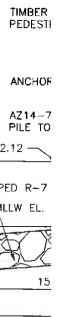
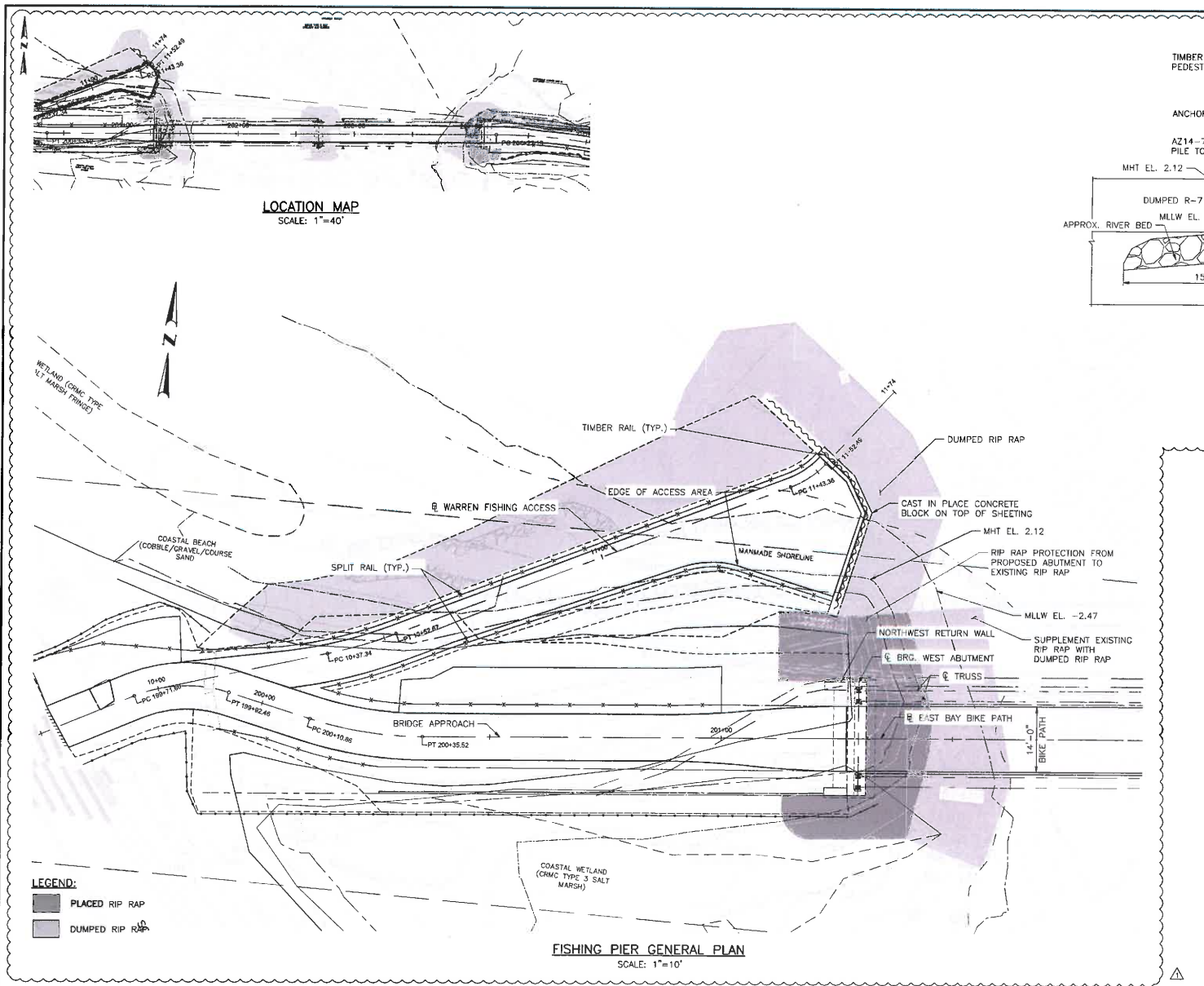


RHODE ISLAND  
DEPARTMENT OF TRANSPORTATION

DESIGNED BY:	SCALE:
CHECKED BY:	
DATE:	
SHEET:	30
OF:	38







RECEIVED  
4/18/2024  
Rhode Island Department of Transportation



**vhb**  
1 Cedar Street  
Suite 400  
Providence, RI 02903  
401.272.8100



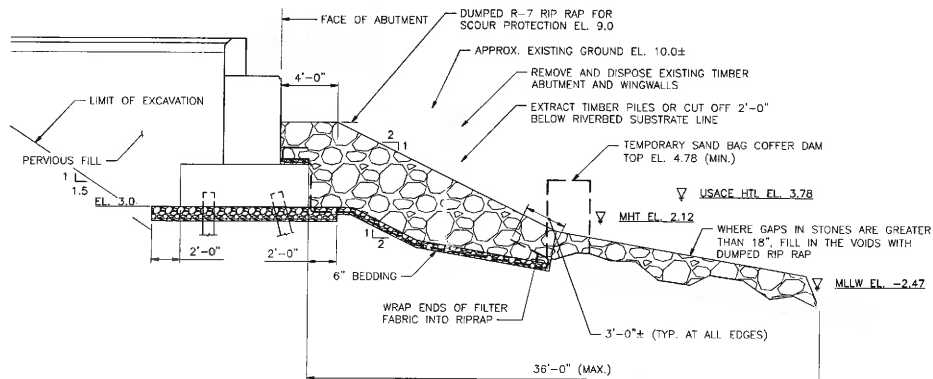
**RHODE ISLAND**  
**DEPARTMENT OF TRANSPORTATION**

PROJECT  
MODIFICATIONS SINCE  
OCTOBER 24, 2023  
CRMC FULL COUNCIL  
PUBLIC HEARING

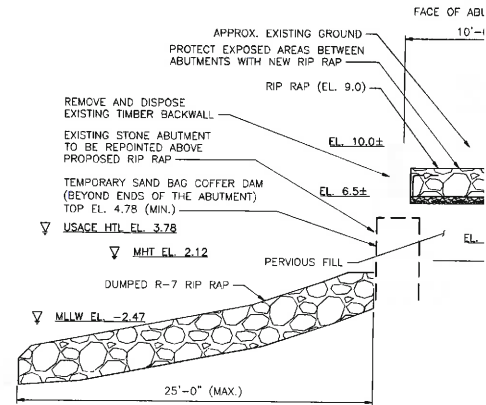
DESIGNED BY:  
CHECKED BY:  
DATE:  
SHEET:  
OF:

SCALE:

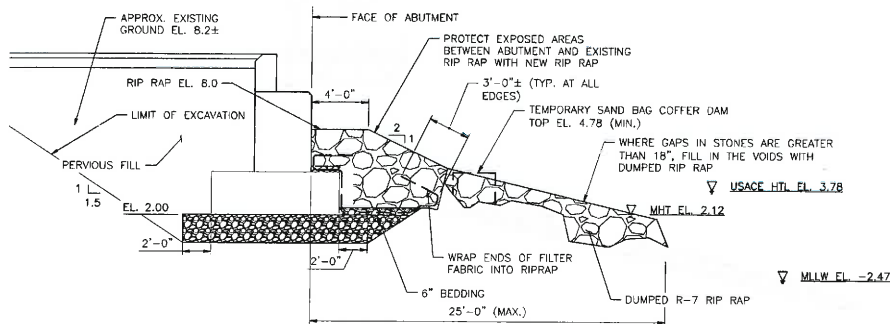
REVISIONS				REVIS
NO.	DATE	BY	NO.	DA
32	2/7/24	vmb		
33				



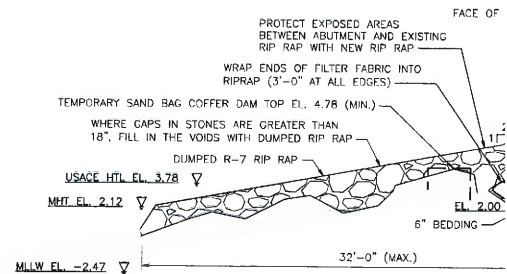
**BRIDGE NO. 083751 WEST ABUTMENT**  
SCALE: 1/4"=1'-0"



**BRIDGE NO. 083751 EAST**  
SCALE: 1/4"=1'-0"



**BRIDGE NO. 083851 WEST ABUTMENT**  
SCALE: 1/4"=1'-0"



**BRIDGE NO. 08385**  
SCALE: 1/4"=1'-0"

**NOTES:**

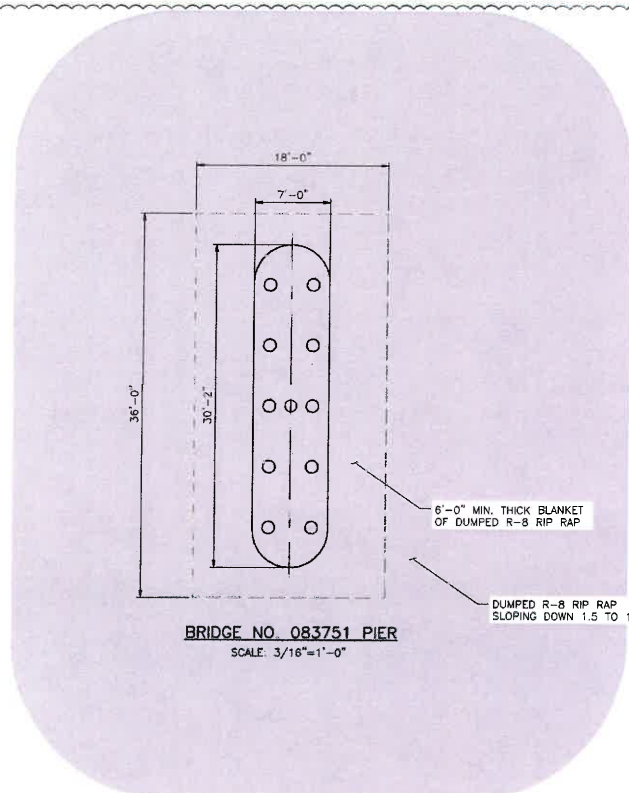
1. WATER INSIDE COFFER DAMS SHALL BE DIRECTED TO (SIMILAR) IN ACCORDANCE WITH SECTION 208 OF THE FREE OF SUSPENDED SEDIMENTS PRIOR TO DISCHARGE
2. TEMPORARY DEWATERING BASINS (RI STANDARD 9.7.0 BRIDGE ABUTMENT FOOTINGS AND LOWER SECTIONS OF THE LIMITS OF THE ACTIVE WORK AREAS ON THE CAU MAY BE MODIFIED TO BEST ACCOMMODATE THE WORK
3. 6" BEDDING UNDER RIP RAP SHALL BE 1.5" STONE B



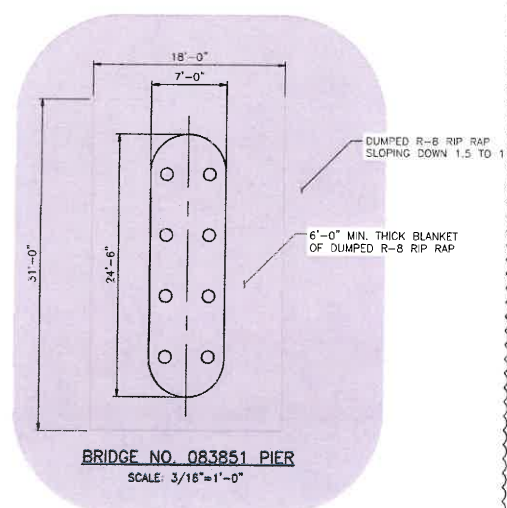
RHODE ISLAND  
DEPARTMENT OF TRANSPORTATION

DESIGNED BY:	SCALE:					
CHECKED BY:						
DATE:	REVISIONS			REVISED		
SHEET:	33	NO.	DATE	BY	NO.	DATE
OF:	38					

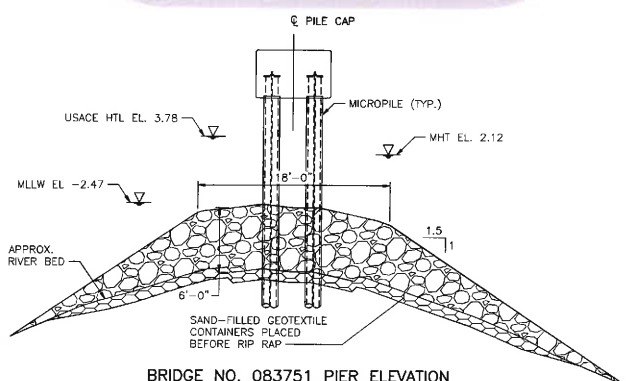




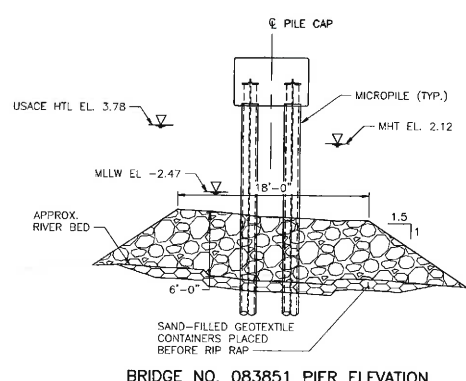
BRIDGE NO. 083751 PIER  
SCALE: 3/16"=1'-0"



BRIDGE NO. 083851 PIER  
SCALE: 3/16"=1'-0"



BRIDGE NO. 083751 PIER ELEVATION  
SCALE: 3/16"=1'-0"



BRIDGE NO. 083851 PIER ELEVATION  
SCALE: 3/16"=1'-0"

LEGEND:

DUMPED RIP RAP

NOTES:

1. BASED ON 2014 EAST BAY BIKE PALMER RIVER BRIDGE, THE CALC. BRIDGE NO. 083751 AND 5.0 FEET PREVENT LOCAL SCOUR AT THE PIER.
2. THE GEOTEXTILE CONTAINERS SHA FILTRATION AND STRENGTH, IN BA ENOUGH OVERLAP TO ENSURE FULL ARE INSTALLED THERE SHALL BE FILTER GEOTEXTILE. THE CONTRA A PREFABRICATED PANEL. THE RE ARE AS FOLLOWS:

MASS PER UNIT AREA	AO:
278 g/m <sup>2</sup>	0.18n

3. CONTRACTOR SHALL AVOID DAMAGE

**AETNA**  
Aetna Bridge Company

**vhb**  
1 Cedar Street  
Suite 400  
Providence, RI 02903  
401.272.8100

**RI**  
**DOT**

RHODE ISLAND  
DEPARTMENT OF TRANSPORTATION

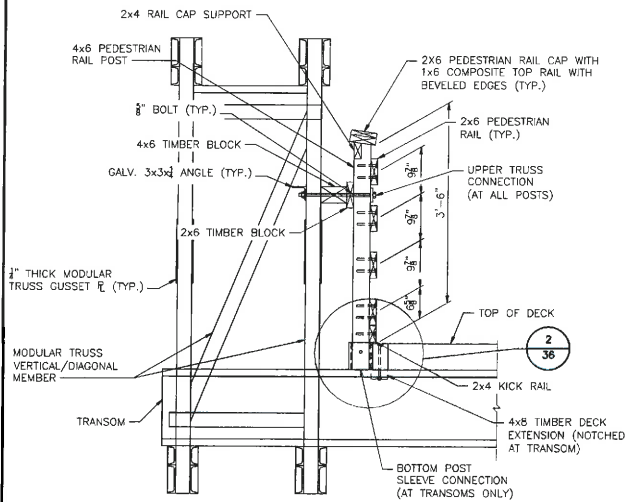
PROJECT  
MODIFICATIONS SINCE  
OCTOBER 24, 2023  
COMB. FULL CONSIDER  
PUBLIC HEARING

DESIGNED BY:  
CHECKED BY:  
DATE:  
SHEET: 34  
OF: 38

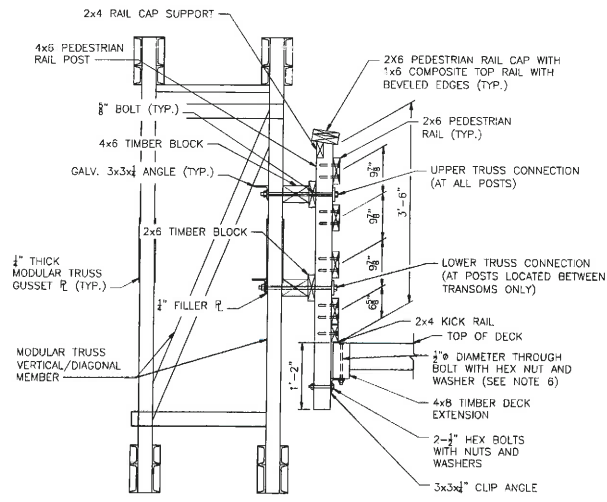
SCALE:

NO.	DATE	BY	NO.	DA
1	3/2/24	VMS		

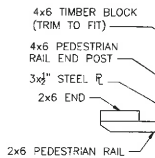
RECEIVED  
4/18/2024  
CONTRACT ADMINISTRATION  
MANAGEMENT OFFICE



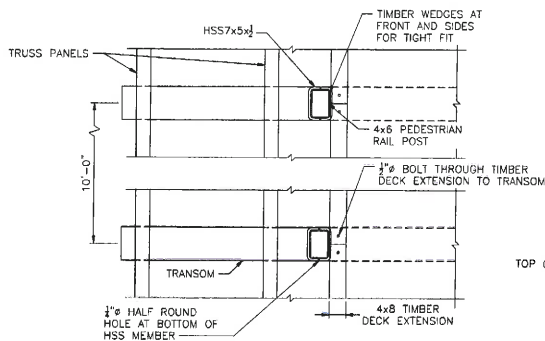
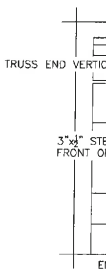
**PEDESTRIAN BRIDGE RAIL SECTION  
AT TRANSOM**  
SCALE: 1"=1'-0"



**PEDESTRIAN BRIDGE RAIL SECTION  
AT MID SPAN BETWEEN TRANSOMS**  
SCALE: 1"=1'-0"



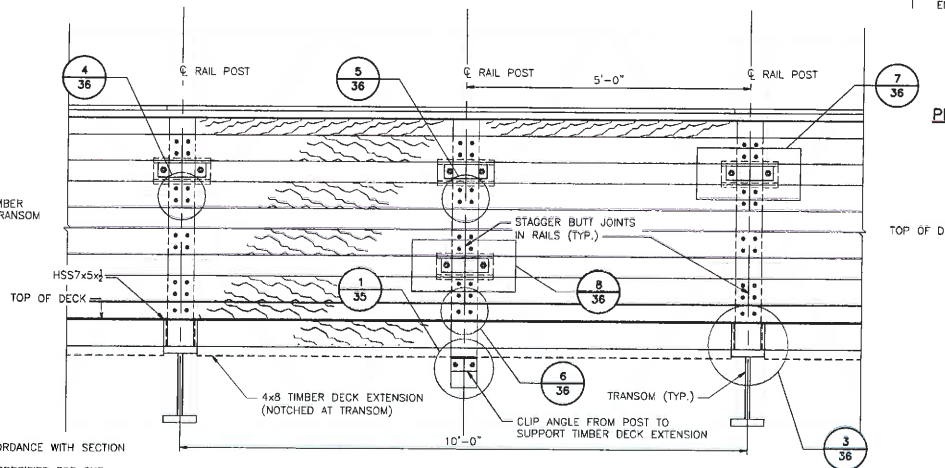
**PEDESTRIAN**  
A



**PLAN AT BOTTOM POST SLEEVE**  
SCALE: 1"=1'-0"

**NOTES:**

1. ALL TIMBER, HARDWARE AND CONSTRUCTION FOR THE BRIDGE RAIL SHALL BE IN ACCORDANCE WITH SECTION 806 OF THE RIDOT STANDARD SPECIFICATIONS.
2. SURFACED FOUR SIDES LUMBER IS LABELED WITH NOMINAL DIMENSIONS. ALL TIMBER SPECIFIED FOR THE RAILING SHALL BE SOUTHERN PINE NO. 1.
3. ALL BOLTS, NUTS, WASHERS AND STRUCTURAL STEEL ARE TO BE GALVANIZED PER SHEET 5 NOTE 6.
4. PROVIDE OPEN JOINT IN COMPOSITE TOP RAIL AT PANELS ADJACENT TO EXPANSION JOINTS.
5. BEVEL 1/4" TOP FACE AND EDGES AT END CUT (ENDS OF BRIDGE).
6. TOP OF 4x8 TIMBER DECK EXTENSION AND BOLT HEAD TO BE FLUSH WITH WEARING SURFACE. FILL BOLT COUNTERBORE RECESS WITH SILICONE SEALANT AFTER TIMBER DECK EXTENSION BOLTS ARE INSTALLED AND TIGHTENED.



**TYPICAL ELEVATION OF PEDESTRIAN BRIDGE RAIL**  
SCALE: 1"=1'-0"

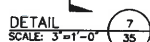
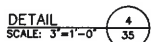
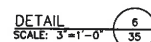
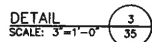
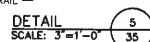
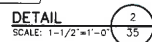


**RHODE ISLAND  
DEPARTMENT OF TRANSPORTATION**

DESIGNED BY:		SCALE:	
CHECKED BY:			
DATE:		REVISIONS	REVIS
SHEET:	35	NO.	DATE
OF:	38	BY	NO.
			DA

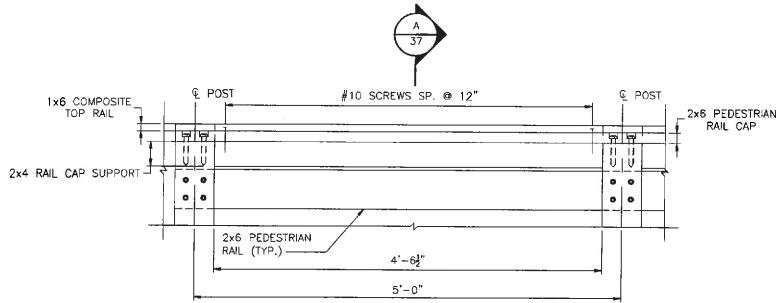




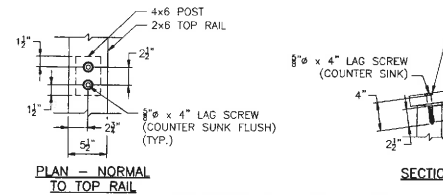


DESIGNED BY:	SCALE:					
CHECKED BY:						
DATE:	REVISIONS			REVISIONS		
SHEET:	NO.	DATE	BY	NO.	DATE	BY
OF:						

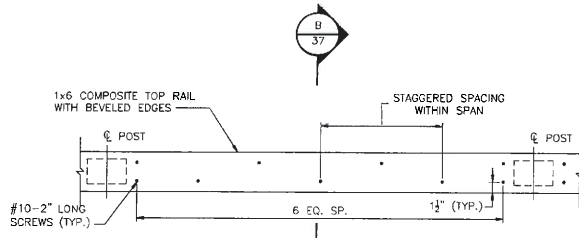




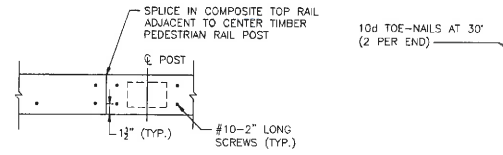
**TYPICAL RAIL CAP SUPPORT TO PEDESTRIAN RAIL CAP CONNECTION**  
SCALE: 1-1/2"=1'-0"



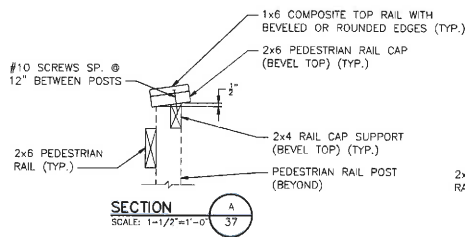
**RAIL CAP CONNECTION AT RAILING POST**  
SCALE: 1-1/2"=1'-0"



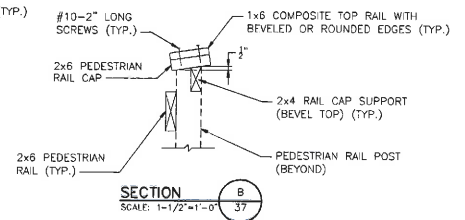
**TYPICAL COMPOSITE TOP RAIL TO PEDESTRIAN RAIL CAP CONNECTION**  
SCALE: 1-1/2"=1'-0"



**TYPICAL COMPOSITE TOP RAIL TO PEDESTRIAN RAIL CAP CONNECTION AT SPLICE LOCATION**  
SCALE: 1-1/2"=1'-0"



**SECTION**  
SCALE: 1-1/2"=1'-0"



**SECTION**  
SCALE: 1-1/2"=1'-0"

**AETNA**  
Aetna Bridge Company

**vhb**  
1 Cedar Street  
Suite 400  
Providence, RI 02903  
401.272.8100

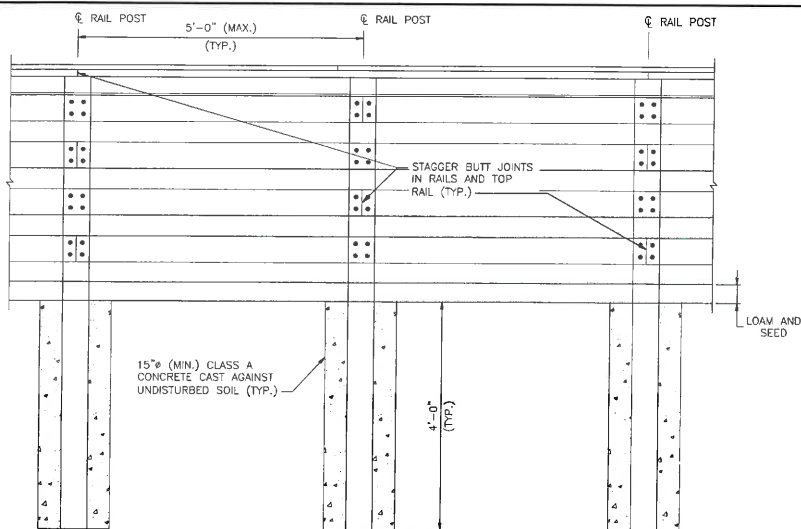
**RI DOT**

RHODE ISLAND  
DEPARTMENT OF TRANSPORTATION

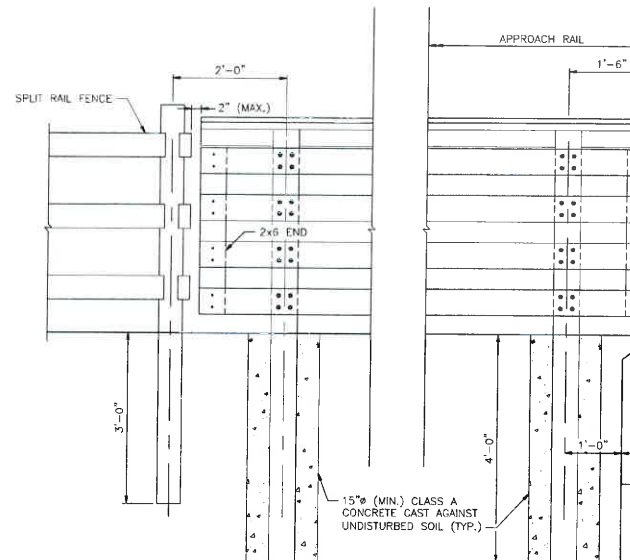
DESIGNED BY:	SCALE:
CHECKED BY:	
DATE:	
SHEET:	
OF:	

NO.	DATE	BY	NO.	DATE
37				
38				

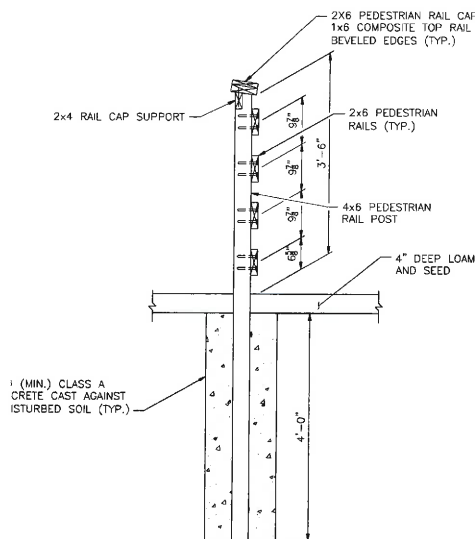
RECEIVED  
4/18/2024  
Rhode Island Department of Transportation



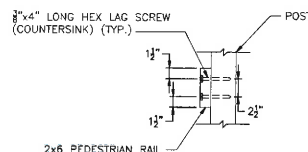
**PEDESTRIAN BRIDGE APPROACH RAIL (PBAR) ELEVATION**  
SCALE: 1"=1'-0"



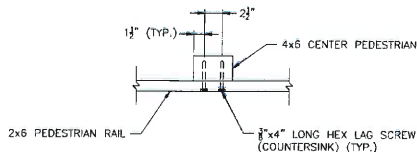
**PEDESTRIAN RAIL TREATMENT AT SPLIT RAIL FENCE DETAIL**  
SCALE: 1"=1'-0"



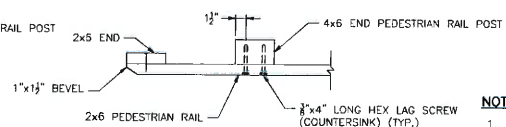
**PEDESTRIAN BRIDGE APPROACH RAIL (PBAR) DETAIL**  
SCALE: 1"=1'-0"



**SECTION AT RAILING POST CONNECTION**



**PLAN AT RAILING POST CONNECTION**



**PLAN AT END OF RAILING CONNECTION**

**TYPICAL RAIL CONNECTION DETAILS**

SCALE: 1-1/2" = 1'-0"

**NOTES:**

1. WHEN SHAL BACK SHAL
2. SEE

**AETNA**  
Aetna Bridge Company

**vhb**  
1 Cedar Street  
Suite 400  
Providence, RI 02903  
401.272.8100

**RI**  
**DOT**

RHODE ISLAND  
DEPARTMENT OF TRANSPORTATION

DESIGNED BY:  
CHECKED BY:  
DATE:  
SHEET:  
OF:

REVISIONS					REVISION
NO.	DATE	BY	NO.	DA	
38					
39					

RECEIVED  
4/18/2024  
CONTRACT ADMINISTRATION