



State of Rhode Island and Providence Plantations
Coastal Resources Management Council
Oliver H. Stedman Government Center
4808 Tower Hill Road, Suite 3
Wakefield, RI 02879-1900

(401) 783-3370
Fax (401) 783-2069

APPLICATION FOR STATE ASSENT

To perform work regulated by the provisions of Chapter 279 of the Public Laws of 1971 Amended.

Project Location <u>169 Riverside Drive, Tiverton</u> No. Street City/Town			File No. (CRMC USE ONLY) 2025-06-026
Owner's Name <u>Rhode Island Department Environmental Management</u>			Plat: 303 Lot(s): 126, 127, 128
Mailing Address <u>235 Promenade Street, Providence, RI 02908</u> Address City/Town, State Zip Code			Owner's Contact: Number: Email Address: david.decost.ctr@dem.ri.gov
Contractor RI Reg. # Address			Email address: Tel. No.
Designer <u>Matthew Page, GZA</u> Address <u>188 Valley Street, Suite 300 Providence, RI 02909</u>			Tel. No. 401-427-2741
Name of Waterway <u>Sakonnet River</u>			Estimated Project Cost (EPC): Application Fee: N/A
Provide Below a Description of Work As Proposed (required). Repair / replacement of existing and installation of new marina facility infrastructure. See attached narrative for more information.			

Have you or any previous owner filed an application for and/or received an assent for any activity on this property?

(If so please provide the file and/or assent numbers): 85-3-71, 88-3-29, 88-3-49, 99-4-31, 2005-2-86, 2021-02-096, 2025-01-001

Is this site within a designated historic district? ☐ YES ☒ NO

Is this application being submitted in response to a coastal violation? ☐ YES ☒ NO

If YES, you must indicate NOV or C&D Number: _____

Name/mailling addresses of adjacent property owners whose property adjoins the project site. Accurate mailing addresses will insure proper notification. DD Applicant **must** initial to certify accuracy of adjacent property owners and accuracy of mailing addresses.

James & Jeanne Smith: 39 Morgan Street, Portsmouth, RI 02871

STORMTOOLS (<http://www.beachsamp.org/resources/stormtools/>) is a planning tool to help applicants evaluate the impacts of sea level rise and storm surge on their projects. The Council encourages applicants to use STORMTOOLS to help them understand the risk that may be present at their site and make appropriate adjustments to the project design.

NOTE: The applicant acknowledges by evidence of their signature that they have reviewed the Rhode Island Coastal Resources Management Program, and have, where possible, adhered to the policies and standards of the program. Where variances or special exceptions are requested by the applicant, the applicant will be prepared to meet and present testimony on the criteria and burdens of proof for each of these relief provisions. The applicant also acknowledges by evidence of their signature that to the best of their knowledge the information contained in the application is true and valid. If the information provided to the CRMC for this review is inaccurate or did not reveal all necessary information or data, then the permit granted under this application may be found to be null and void. Applicant requires that as a condition to the granting of this assent, members of the CRMC or its staff shall have access to the applicant's property to make on-site inspections to insure compliance with the assent. This application is made under oath and subject to the penalties of perjury.

David J DeCost Jr

David J DeCost Jr

Digitally signed by David J DeCost

Date: 2025.06.03 15:43:37 -04'00'

Owner Name (PRINT)

Owner's Signature (SIGN)

PLEASE REVIEW REVERSE SIDE OF APPLICATION FORM

RECEIVED

6/5/2025

**COASTAL RESOURCES
MANAGEMENT COUNCIL**

Tiverton

(Summary Data - may not be Complete Representation of Property)

Parcel: 303-126

Location: 0 RIVERSIDE DRIVE

Owner: STATE OF RI

Account: 4867

User Acct: 38-0030-04

LUC: 80 - State

Zoning: W

Parcel Values

Total: \$260,800

Land: \$260,800

Land Area: 0.14 AC

Building: \$0

Assessed: \$260,800

Sales Information

Book and Page

1089-86

Instrument Type

N/A

Date

08/18/2006

Price

\$0

Grantor

STATE OF RI

1089-85

N/A

08/18/2006

\$0

CHASE FRANCIS H III

Building Type:

Year Built:

Grade:

Condition: AV

Heat Fuel:

Heat Type:

% Air Conditioned: 0.00

Fireplaces: 0

Exterior Wall:

Bsmnt Garage: 0

Roof Cover:

of Units: 0

of Rooms: 0

of Bedrooms: 0

Full Bath: 0

1/2 Baths: 0

Yard Item(s)

Description

Quantity

Size

Year

Condition

Quality

Value

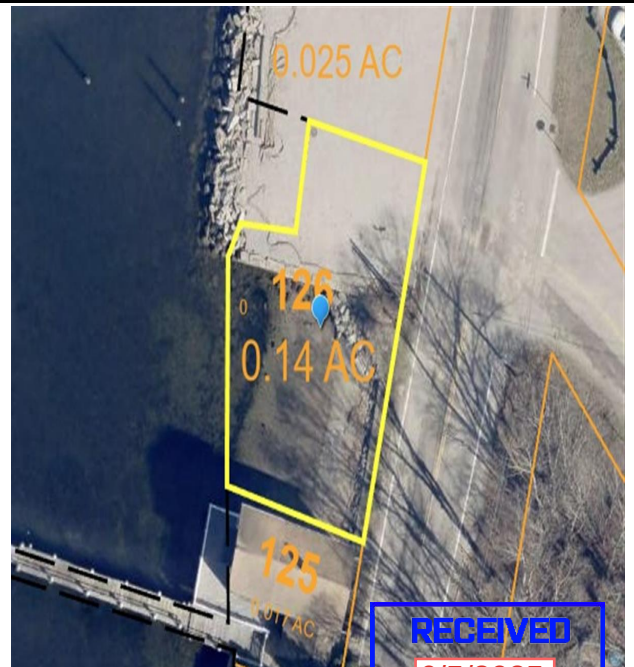
Building Areas

Area

Net Area

Finished Area

Disclaimer: This information is for tax
assessing purposes
and is not warranted



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6/5/2025

COASTAL RESOURCES
MANAGEMENT COUNCIL

Tiverton

(Summary Data - may not be Complete Representation of Property)

Parcel: 303-127

Location: 0 RIVERSIDE DRIVE

Owner: STATE OF RI

Account: 4868

User Acct: 38-0030-02

LUC: 80 - State

Zoning: W

Parcel Values

Total: \$331,300

Land: \$331,300

Land Area: 0.025 AC

Building: \$0

Assessed: \$331,300

Sales Information

Book and Page

1089-86

Instrument Type

N/A

Date

08/18/2006

Price

\$0

Grantor

STATE OF RI

1089-85

N/A

08/18/2006

\$0

CHASE FRANCIS H III

Building Type:

Year Built:

Grade:

Condition: AV

Heat Fuel:

Heat Type:

% Air Conditioned: 0.00

Fireplaces: 0

Exterior Wall:

Bsmnt Garage: 0

Roof Cover:

of Units: 0

of Rooms: 0

of Bedrooms: 0

Full Bath: 0

1/2 Baths: 0

Yard Item(s)

Description

Quantity

Size

Year

Condition

Quality

Value

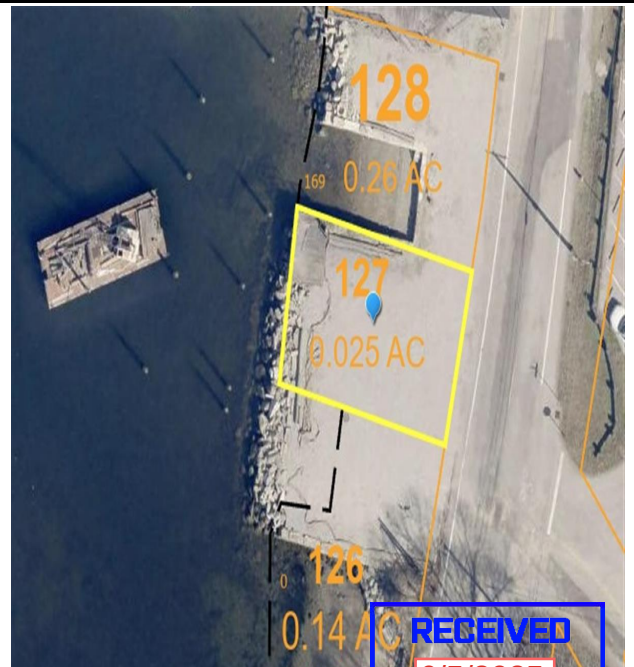
Building Areas

Area

Net Area

Finished Area

Disclaimer: This information is for tax
assessing purposes
and is not warranted



Tiverton

(Summary Data - may not be Complete Representation of Property)

Parcel: 303-128

Location: 169 RIVERSIDE DRIVE

Owner: STATE OF RI

Account: 4869

User Acct: 38-0030-03

LUC: 80 - State

Zoning: W

Parcel Values

Total: \$1,420,300

Land: \$932,800

Land Area: 0.26 AC

Building: \$487,500

Assessed: \$1,420,300

Sales Information

Book and Page

1089-86

Instrument Type

N/A

Date

08/18/2006

Price

\$0

Grantor

STATE OF RI

1089-85

N/A

08/18/2006

\$0

CHASE FRANCIS H III

Building Type: MIXUSE

Year Built: 1925

Grade: 100

Condition: GD

Heat Fuel: Gas

Heat Type: Forced Air

% Air Conditioned: 0.00

Fireplaces: 0

Exterior Wall: Clapboards

Bsmnt Garage: 0

Roof Cover: N/A

of Units: 1

of Rooms: 0

of Bedrooms: 0

Full Bath: 0

1/2 Baths: 0

Yard Item(s)

Description

Quantity

Size

Year

Condition

Quality

Value

MEZZ - FIN

1

1680

1900

GD

N/A

\$25,300.00

COMM DOCK

1

2100

1980

AV

Average

\$112,200.00

PAVING

1

1200

1980

AV

Average

\$1,300.00

SLIPS

1

1

1980

GD

Good

\$16,000.00

Building Areas

Area

Net Area

Finished Area

Finished Upper Story

1,800 SF

1,800 SF

First Floor

3,871 SF

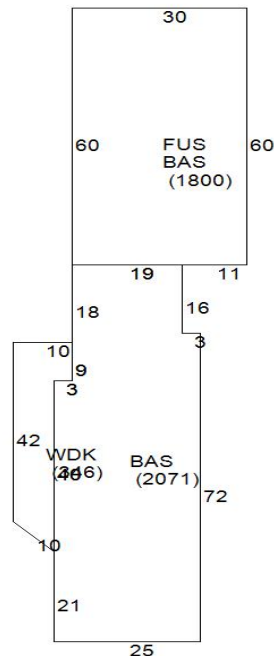
3,871 SF

Wood Deck

346 SF

0 SF

Disclaimer: This information is for tax
assessing purposes
and is not warranted



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COASTAL RESOURCES
MANAGEMENT COUNCIL

TO: **Coastal Resources Management Council**
4808 Tower Hill Road Suite 3
Wakefield, RI 02879
Phone: (401) 783-3370 / Fax: (401) 783-2069



FROM: Building Official

DATE: May 14, 2025

SUBJ: Application of: RI Dept. Environmental Management

Location: Chase Marina

Address: 169 Riverside Drive, Tiverton, RI 02878

Plat(s): 303

Lot(s): 126, 127, 128

To Construct: Shoreline protection and in-water marina facility infrastructure (pilings, docks)

I hereby certify that I have reviewed _____ foundation plan(s).

_____ plan(s) for entire structure

X site plans

Titled: Chase Marina, 169 Riverside Drive, Tiverton, Rhode Island,
Permitting, Dated April 2025

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6/5/2025

COASTAL RESOURCES
MANAGEMENT COUNCIL

Date of Plan (last revision): April 2025

_____ and find that the issuance of a local building permit is not required as in accordance with Section _____ of the Rhode Island State Building Code.

☒ and find that the issuance of a local building permit is required. I hereby certify that this permit shall be issued once the applicant demonstrates that the proposed construction/activity fully conforms to the applicable requirements of the RISBC, and all other local, state and federal regulations are met.

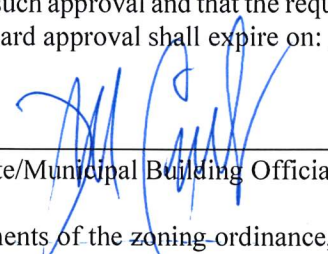
_____ and find that a Septic System Suitability Determination (SSD) must be obtained from the RI Dept. of Environmental Management.

_____ and find that a Septic System Suitability Determination (SSD) need not be obtained from the RI Dept. of Environmental Management.

_____ and find NO structural or non-structural fill is proposed in a FEMA-designated V-Zone or Coastal A-zone.

_____ and find that the proposed fill is considered non-structural fill and meets FEMA NFIP guidelines.

_____ and find that said plans conform with all elements of the zoning ordinance, and that if said plans require zoning board approval, that the applicant has secured such approval and that the requisite appeal period has passed with no appeal filed or appeal is final. The Zoning Board approval shall expire on: _____.


State/Municipal Building Official's Signature

4 JUNE 25
Date

N/A and find that said plans conform with all elements of the zoning ordinance, and that if said plans require zoning board approval, that the applicant has secured such approval and that the requisite appeal period has passed with no appeal filed or appeal is final.



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GEOTECHNICAL
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WATER
CONSTRUCTION
MANAGEMENT

188 Valley Street
Suite 300
Providence, RI 02909
T: 401.421.4140
F: 401.751.8613
www.gza.com

June 3, 2025
File No. 03.0033884.02

Ms. Lisa Turner
RI Coastal Resources Management Council
4808 Tower Hill Road; Suite 3
Wakefield, Rhode Island 02879

Mr. Kevin Kotelly
United States Army Corps of Engineers
New England District, Regulatory District
696 Virginia Road
Concord, Massachusetts 01742

Mr. Ron Gagnon
Rhode Island Department of Environmental Management
Office of Customer and Technical Assistance
235 Promenade Street
Providence, Rhode Island 02908

Re: Application for CRMC Assent, USACE Permit, and RIDEM Water Quality Certification
Chase Marina, Tiverton, Rhode Island

Dear Ms. Turner, Mr. Kotelly, Mr. Gagnon;

On behalf of our Client, the Department of Environmental Management (RIDEM), this application is being submitted by GZA GeoEnvironmental, Inc. (GZA) pursuant to the applicable requirements of the Coastal Resources Management Council (CRMC), the United States Army Corps of Engineers (USACE), and the Rhode Island Department of Environmental Management (RIDEM).

RIDEM is proposing to repair existing infrastructure and enhance the usability of the former Chase Marina facility located at 169 Riverside Drive, Tiverton, RI. The Site is located underneath the eastern end of the Sakonnet River Bridge. The Chase Marina facility, which was constructed prior to 1939, now consists of a two-story building, onsite parking, and remnants of vessel berthing infrastructure. The facility was previously used for seafood processing, sales, and as a recreational marina. It is the intention of RIDEM to redevelop the facility in support of the State's commercial fishing industry and bring back the potential for the processing and distribution of seafood.

In November 2021, GZA conducted a Level II Property Condition Assessment for the facility, in accordance with ASTM E2018-15 "Standard Guide for Property Condition Assessments". The building and surrounding infrastructure appeared to be in generally fair condition with respect to its age, use and location. At the time of the assessment, the building was vacant and unoccupied.

GZA's water-based assessment found that portions of the property seawall are in good to poor condition and certain repairs need to be made to maintain the seawall's functionality. A timber pier consisting of timber deck boards supported by timber stringers, pile caps and piles is located at the west end of the parcel. The timber pier was formerly used for offloading of vessels exhibits minimal to minor deterioration. A concrete slab on grade abutting the timber pier and building is in disrepair due to cracking and settlement of the slab. A stone rip rap slope consisting primarily of stone sizes ranging between 3' and 5' in diameter provides shoreline erosion protection to the upland area.





Necessary repairs were identified to improve the overall function of the structures, maintain or increase the load carrying capacity, prevent deterioration over time, and reduce the likelihood of the occurrence of additional advanced and severe deterioration. Proposed repairs and improvements include:

- Installation of a sheet pile bulkhead;
- Repairing voids in rip rap slopes;
- Raising grades to match existing parking area;
- Construction of timber walkways and docks; and
- Reinforcement of existing granite block walls.

The repaired and stabilized structures will allow for the revitalization of the vacant facility, resuming its function as a vital resource for the State's commercial fishing industry.

Prior Permits/Assents

There have been a number of permits/assents secured for water-based maintenance activities at the facility, including the following:

- In 1985, a CRMC assent was issued (No. 85-3-71) to replace twelve (12) existing timber pilings with twelve (12) oak pilings.
- CRMC Assent 88-3-29 was granted maintenance assents for work including but not limited to the replacement of piers, removal of an abandoned timber cradle and marine railway, dredging, and filling to create a parking lot. The facility was recognized as a Marina Facility under Assent 88-3-29.
- In 1991, CRMC Assent 88-3-49 authorized improvements and renovations to the existing building structures and installation of an individual sewage disposal system.
- CRMC Assent 99-4-31 authorized repairs to the existing loading dock along the western edge of the building.
- In 2005, USACE authorized work in accordance with CRMC Assent 2005-2-86 for the reconfiguration of the facility including the removal of an abandoned marine railway and dock infrastructure for sixteen (15) boat slips.
- In 2021, CRMC authorized the installation of an engineered cap over the property and decommissioning groundwater monitoring wells (2021-02-096).
- The removal of 11 existing piles was authorized by CRMC in 2025, under Assent 2025-01-001.

Town / Public Engagement

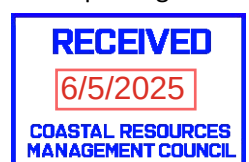
Dan Costa (State Port Manager, RIDEM) has continued to remain engaged with stakeholders and public from the Town of Tiverton including the Town Administrator and the municipal Harbor Commission. Dan presented to the Harbor Commission and public attendees on November 18, 2024, February 5, 2025, March 17, 2025, and April 23, 2025. Jim O'Connor (Superintendent of State Piers, RIDEM) co-presented at the April 23, 2025, Harbor Commission meeting.

Proposed Activities

The proposed plan separates the maintenance activities into two phases, with Phase 1 covering much of the shoreline stabilization and structural improvements with preliminary dock sections and Phase 2 covering the expansion of docks and associated infrastructure. Proposed demolition will be conducted prior to construction.

Demolition:

Many existing timber piles are severely degraded and will be removed to install replacement piles. Damaged sections of the concrete pads and timber decking will be removed and replaced. Timber wheel stops around the parking area





will be removed. As necessary, existing riprap will be removed along the shoreline in the southern portion of the site to allow for installation of the sheet pile wall. The demolition plan is included as Drawing 7 in the attached Sheet Set.

Phase 1:

The work proposed for Phase 1 is illustrated in Drawing 9 of the attached Sheet Set, with sections of each area provided in Drawing 10. In the southern portion of the Site (Sections A and B), a sand-gravel mix will be used to fill riprap slopes and the basin previously used as a marine railway, to match the existing grade and ensure a safe, usable parking area. A sheet pile bulkhead is proposed along the parking area for increased structural stability and erosion prevention.

Sections C, D, and E illustrate work proposed along the southern edge of the existing building. Timber stringers and decking will be installed to provide a landing platform to access floating docks from the building via an aluminum gangway. The timber decking is proposed to continue along the perimeter, supported by timber piles and cross bracing. At Section E, a fixed timber pier will extend south to provide access to the location of docks proposed in Phase 2. Section F includes replacing in kind the existing concrete pad in poor condition, with a concrete slab on grade over a crushed stone base. A fixed timber pier will extend west with cross bracing to provide access for the proposed seafood landing. A total of 31 timber piles (14" diameter) will support the timber pier running along the perimeter of the existing building.

In the southeastern portion of the Site, 18 steel piles (16" x 0.5" diameter) will be installed, with 5 floating docks parallel to the shoreline and 4 floating docks extending to the west. This dock system will be accessed from the aluminum gangway installed at Section C.

Phase 2:

The work proposed for Phase 2 is illustrated in Drawing 11 of the attached Sheet Set, with sections of each area provided in Drawing 12. Phase 2 proposes to expand the floating dock system to the west, with access via the aluminum gangway and timber deck landing installed during Phase 1 (refer to Section E). For Phase 2, a total of 17 floating dock sections ranging in size from 18'x6' to 20'x8', are proposed to be supported by 32 steel pipe piles (16" x 0.5" diameter).

Previous discussions with **CRMC**, including an on-site pre-application meeting held on February 27, 2025, concluded that a Category B Assent application is the proper submittal. A pre-application meeting with **USACE** and **RIDEM** on March 18, 2025, concluded that due to the components below the high tide line, an Individual Permit and Water Quality Certification (WQC) will likely be needed.

CRMC Category B Assent

The Chase Marina facility is adjacent to Type 3 waters, where "the Council's goal is to preserve, protect, and where possible enhance Type 3 areas for high intensity boating and the services that support this activity". As outlined in Redbook Section 1.2.1.D., there are limited areas suitable for marinas, especially for marinas of comparable size to Chase Marina. The redevelopment of the existing facility serves to meet the CRMC solution of utilizing available facilities more efficiently and to recycle already altered sites in the upper Bay, meeting the demands of moorings, dockage, and storage space in an innovative way.

For aid in Category B review, the following Redbook sections will be addressed:

- SECTION 1.1.4 Alterations and Activities that Require an Assent from the Coastal Resources Management Council;
- SECTION 1.1.10 Climate Change and Sea Level Rise;
- SECTION 1.2.1(D) Type 3 Waters – High Intensity Boating;
- SECTION 1.2.2(F) Manmade Shorelines;
- SECTION 1.3.1(A) Category B Requirements;





- SECTION 1.3.1(B) Filling, Removing, or Grading of Shoreline Features;
- SECTION 1.3.1(C) Residential, Commercial, Industrial, and Recreational Structures;
- SECTION 1.3.1(D) Recreational Boating Facilities;
- SECTION 1.3.1(G) Shoreline Protection;
- SECTION 1.3.6 Protection and Enhancement of Public Access to the Shore; and
- SECTION 1.5 Public and Governmental Participation

Section 1.1.4 Alterations and Activities That Require an Assent from the Coastal Resources Management Council

A CRMC Assent is required for any alteration or activity proposed for tidal waters within the territorial seas, shoreline features, and areas contiguous to shoreline features. This narrative has been prepared to address CRMC requirements necessary to secure an Assent in accordance with 650-RICR-20-00-1 *Red Book*.

Section 1.1.10 Climate Change And Sea Level Rise

Climate change and resultant sea level rise are recognized by CRMC as ongoing and various scenarios are integrated into its programs to prepare Rhode Island for these new and evolving circumstances. The FEMA 100-year Flood Insurance Map (panel 44005C0039J) shows that the facility is located within a VE Zone with BFE 16. The proposed floating dock systems have been designed for: 1) top of pile located at elevation 20 feet approximately 4 feet above BFE 16; and 2) support the mooring loads that will be transferred to the pile foundations during the 100-year flood recurrence interval. The proposed development of the facility as a commercial, working port includes measures to accommodate current NOAA projections of sea level rise. The design is intended to accommodate commercial marina operations for existing conditions while allowing for the structures to be modified in the future and adapt to changes in sea level rise. The steel bulkhead, timber pier, and timber landing have been designed to allow for changes in elevation of the structures with minimal modifications to the foundation components (e.g. steel sheet piling and timber piling).

Section 1.2.1(D) Type 3 Waters – High Intensity Boating

As noted earlier, the facility is located adjacent to Type 3 Waters. The proposed activities will agree with CRMC's highest priority uses for Type 3 waters intends to support the Council goal of utilizing the available facility more efficiently and encourage boating uses in the area.

Section 1.2.2(F) Manmade Shorelines

The marina facility, which pre-dates CRMC with evidence supporting constriction prior to 1939, features a shoreline characterized by concentrations of shoreline protection structures and other alterations, to the extent that natural features are no longer dominant.

The proposed activities promote the Council's goal to "encourage maintenance of structures that effectively mitigate erosion and/or sustain landforms adjacent to the water". The strengthened and supported shoreline structures will mitigate erosion and create a shoreline that will be more resilient to future flood, wave action and sea level rise, extending usability of the facility. In removing the existing hazards, the scenic qualities and functionality will be restored.

Section 1.3.1(A) Category B Requirements

The proposed reconstruction activities satisfy the basic requirements for a Category B Assent, specifically:

- a) Proposed Repair Need: As evidenced by the 2021 report and subsequent inspections, the facility is in need of structural and operational improvements. Under existing conditions, the facility is not safe and use is not feasible. The restored facility would provide berthing for existing commercial fishermen and job opportunities to new individuals interested in the fishing industry, one so vital to the State's economy.





- b) Code Satisfaction: All applicable zoning, building, and design codes will be met. In addition to a CRMC Assent, authorization will be obtained from RIDEM and USACE prior to commencement of construction activities. The Rhode Island Building Code does not regulate shoreline protection or dock structures. Land based activities at the interface of the slope may require temporary soil disturbance, for which the contractor will deploy erosion controls to prevent sediment from leaving the immediate work area within the upland area. The project team will coordinate with the US Coast Guard as needed.
- c) Coastal Waters Affected: Boundaries of the proposed project are identified throughout the attached sheet set. The length of work is approximately 700 linear feet along the shoreline, wrapping around the perimeter of the existing building and encompassing existing structures. The proposed work is adjacent to the Sakonnet River.
- d) Minimal Erosion/Deposition: The construction activities are expected to protect the existing shoreline structures from damage associated with erosion, therefore not exacerbating erosion or deposition processes along the shore or in tidal waters. Minimal soil disturbance is anticipated at the landward connection area, where sand-gravel fill is proposed. During the construction phases, composite silt filter socks (filtrexx soxx) will be utilized as perimeter controls at the limit of work to minimize soil erosion and deposition. A turbidity curtain will be deployed around the perimeter of all in-water work to minimize disturbance of soils within the tidal zone. Construction entrances will be established to prevent sediment tracking. Sediment bag inlet protection will be used within Site catch basins. Temporary controls are illustrated in Drawing 5 with details included in Drawing 6.
- e) Minimal Impacts to Plants/Animals: Installation of the proposed repairs will not detrimentally impact existing plant and animal life. There is no eel grass, salt marsh, or submerged aquatic vegetation (SAV) identified in the area waters.
- f) Impacts on Public Access: Under current conditions, public access to the facility is prohibited. Upon project completion, the repairs will allow for safe public use in line with RIDEM's proposed operating plan including use of the shore and tidal waters.
- g) Minimal Impacts on Water Circulation: The proposed repairs will not cause significant impacts to water circulation, flushing, turbidity, or sedimentation. A temporary turbidity curtain will be deployed during construction to limit disturbance and will be removed upon project stabilization.
- h) Minimal Water Quality Deterioration: No deterioration of water quality in the immediate vicinity of the proposed repairs is anticipated and a RIDEM Water Quality Certificate will be obtained prior to construction.
- i) No Historic and Archaeological Impacts: A review of available information has concluded that the property is not identified as historic with the National, State, or Local registers.
- j) No Impacts on Water Dependent Uses: The property has historically operated for recreational and commercial use. Proposed work will allow these uses to return to the property. No impact is anticipated on the various water dependent uses in the Sakonnet River.
- k) No Adverse Scenic Impacts: The aesthetics of the repairs are designed to restore the waterfront value of the property and will improve upon the existing dilapidated condition.

Section 1.3.1(B) Filling, Removing, or Grading of Shoreline Features

The proposed activities will require filling and grading of the shoreline features. The primary purpose of filling along the shoreline is to enhance safety and usability. Currently, there is a basin within the footprint of the former marine railway system. The basin is approximately 20'x40' and creates a significant hazard for safety and structural stability. The basin and adjacent riprap slopes will be filled with a sand-gravel mix to match the existing grade. This created area will likely serve as an improved parking area, in order to meet CRMC requirements for vehicle parking at a marine facility within the limited available space. Applicable sections of the most recent version of the Rhode Island Soil Erosion and Sediment Control Handbook will be followed to properly manage activities. Existing slopes will be reduced, fill material shall be clean and free of materials that may cause pollution of tidal waters, and there shall be no application, generation, or migration of toxic substances.

Section 1.3.1(C) Residential, Commercial, Industrial, and Recreational Structures





The proposed work aims to prevent, minimize or mitigate the risks of storm damage to the property and coastal resources, consistent with policies of the State of Rhode Island. In accordance with 1.3.1(C)7., ASTM standards will be followed as applicable and bracing between piles will be installed. The intention of the designs is to allow the facility's use to maintain a commercial marina.

SECTION 1.3.1(D) Recreational Boating Facilities

The proposed activities are intended to develop the facility as a working port for commercial fishermen. The marina has historically been a commercial marina and not a recreational boating facility, as reflected in the Marina Perimeter Limit.

Section 1.3.1(G) Shoreline Protection

The existing conditions require that any work on the property include structural shoreline protection methods; there are no reasonable or practical alternatives. The proposed bulkhead seaward of the existing riprap slope will service the water dependent uses of the marina, as addressed in Sections 1.3.1(G)1(C) and 3(E). The structure will enhance access to the shoreline and aid in the installation of suitable parking and dock access. In considering alternatives for Site development, the bulkhead proved more practical and reasonable. The structures will not increase erosion and will not adversely affect the stability of the shoreline on either side of the project. All shoreline protection methods will be designed and certified by a RI Professional Engineer.

Section 1.3.6 Protection and Enhancement of Public Access to the Shore

Under current conditions, public access to the facility is prohibited. Upon project completion, the repairs will allow for safe public use in line with RIDEM's proposed operating plan including use of the shore and tidal waters. The restored facility would provide berthing locations and opportunities for Rhode Island commercial fishermen.

Section 1.5 Public and Governmental Participation

Throughout the conceptual and design processes, RIDEM has engaged with the general public and targeted stakeholders. The Department sought input through a Public Information Request in 2022 on the development and operation of the Chase Marina facility. RIDEM has engaged with multiple stakeholders at the municipal level.

USACE Individual Permit

Informed by on-going discussions, including a March 18, 2025, pre-application meeting with USACE, it is GZA's understanding that an Individual Permit is necessary due to the inclusion of new fill below the high tide line. The current estimate is that approximately 2,025 square feet (SF) of permanent impacts will occur in Rhode Island tidal waters, below the high tide line.

As noted above, the proposed maintenance activities will occur along approximately 700 linear feet of shoreline. There is no evidence of eel grass, salt marsh, or SAV in the area waters. Fill below the high tide line primarily consists of a sand-gravel mix and steel pipe piles.

This project is not anticipated to impact aquatic life movements or water flows in Narragansett Bay, nor create any discharges of pollutants. This project does not require any in-water excavating or dredging. Temporary impacts will be minimized to the extent possible, including the deployment of a turbidity curtain prior to construction. Any impacts related to erosion and sedimentation in upland areas, including sand-gravel fill landward of the bulkhead, will be mitigated utilizing a perimeter compost tube (See Drawings 5 & 6). The repairs will not create new impervious surfaces along the shoreline; therefore, treatment of stormwater runoff will not be required. The proposed repairs represent a single and complete project and do not depend on any upland activities. This project is not located on a property owned by the USACE nor will it impact property or easements owned by the USACE. There is no Federally maintained channel in the area and there will be no impact on navigation. The project team will coordinate with the United States Coast Guard, as needed, to address aids to navigation that may be required. The overall project impacts are as follows.





- Total of 2,025 square feet (186 cubic yards) of fill below high tide line
- Total of 1,678 square feet (121 cubic yards) of fill below mean high water

Once the project has obtained authorization from the regulatory agencies, copies of the permits/approvals will be kept onsite during construction. A completed Individual Permit form (ENG Form 4345) is included in this application package.

RIDEM Water Quality Certification

RIDEM classifies the coastal waters in this area (RI0010031E-01C) as an SB water body. SB waterbodies are designated for primary and secondary contact recreational activities; shellfish harvesting for controlled relay and depuration; and fish and wildlife habitat. They shall be suitable for aquacultural uses (other than shellfish for direct human consumption), navigation, and industrial cooling. These waters shall have good aesthetic value. There are no vegetated wetlands adjacent to the water body's edge in the area of proposed construction.

- Total of 2,025 square feet (186 cubic yards) of fill below high tide line
- Total of 1,678 square feet (121 cubic yards) of fill below mean high water

The proposed limit of work is less than 1-acre, with grading and permanent changes within the area. Infiltration of precipitation will not be altered. There will be no storage of uncovered materials that may contaminate runoff. The project will not create any additional impervious surface requiring stormwater treatment. Sediment disturbance on land and in-water will be minimized to the extent possible using perimeter controls (compost filter tubes and turbidity curtain). We believe this project will not adversely impact the water quality of Narragansett Bay.

We are hopeful that this combined permit application package provides the necessary information and serves to keep each environmental permitting agency informed with one another. If you have any questions or need additional information, please contact Megan Elwell at megan.elwell@gza.com or 401-256-3991.

Very truly yours,

GZA GEOENVIRONMENTAL, INC.

Megan Elwell
Assistant Project Manager

Russell J. Morgan, P.E.
Consultant/Reviewer

Matthew Page, P.E.
Associate Principal

Attachments: 12 Sheet Plan Set
Application Form
Site Photographs
Proof of Ownership
Building Official Form





Photo No.1: View looking north of existing building, timber pier, and pilings.



Photo No.2: Previously decommissioned marine railway basin to be filled.

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COASTAL RESOURCES
MANAGEMENT COUNCIL

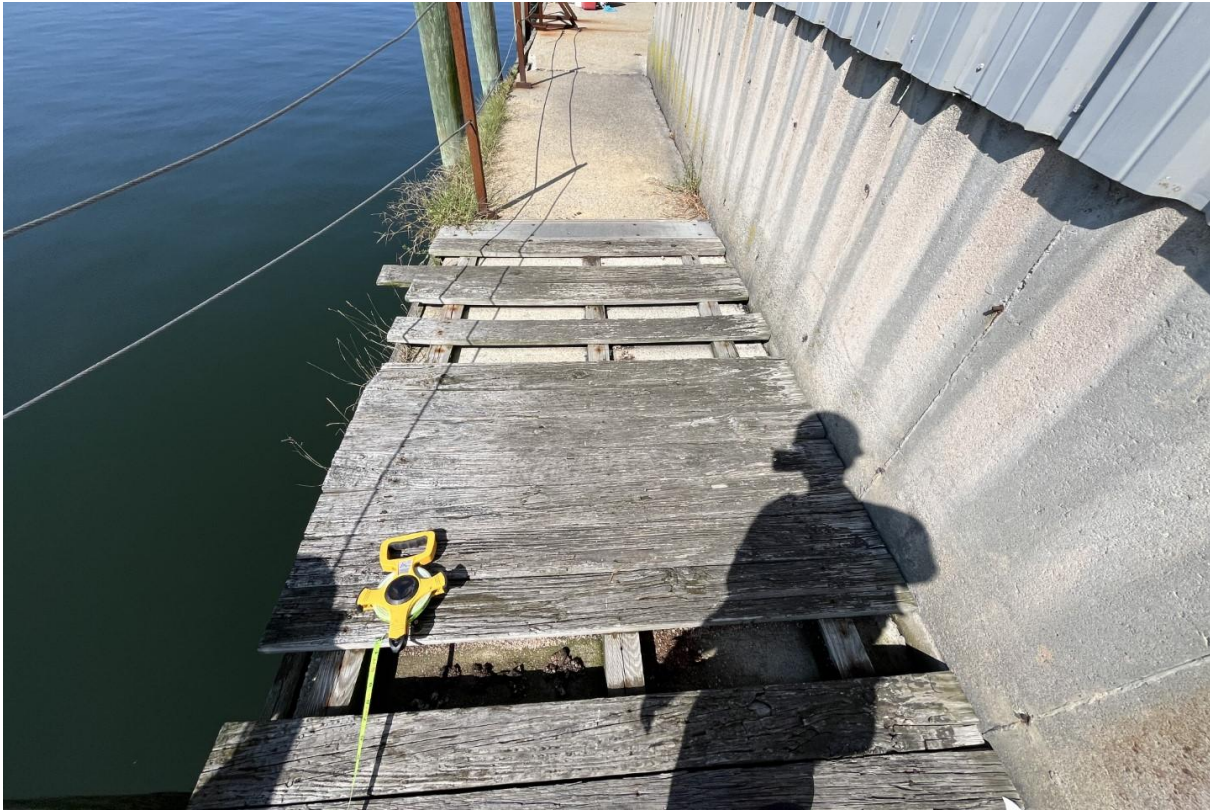


Photo No.3: View of existing timber and concrete walkway to be replaced, adjacent to building.

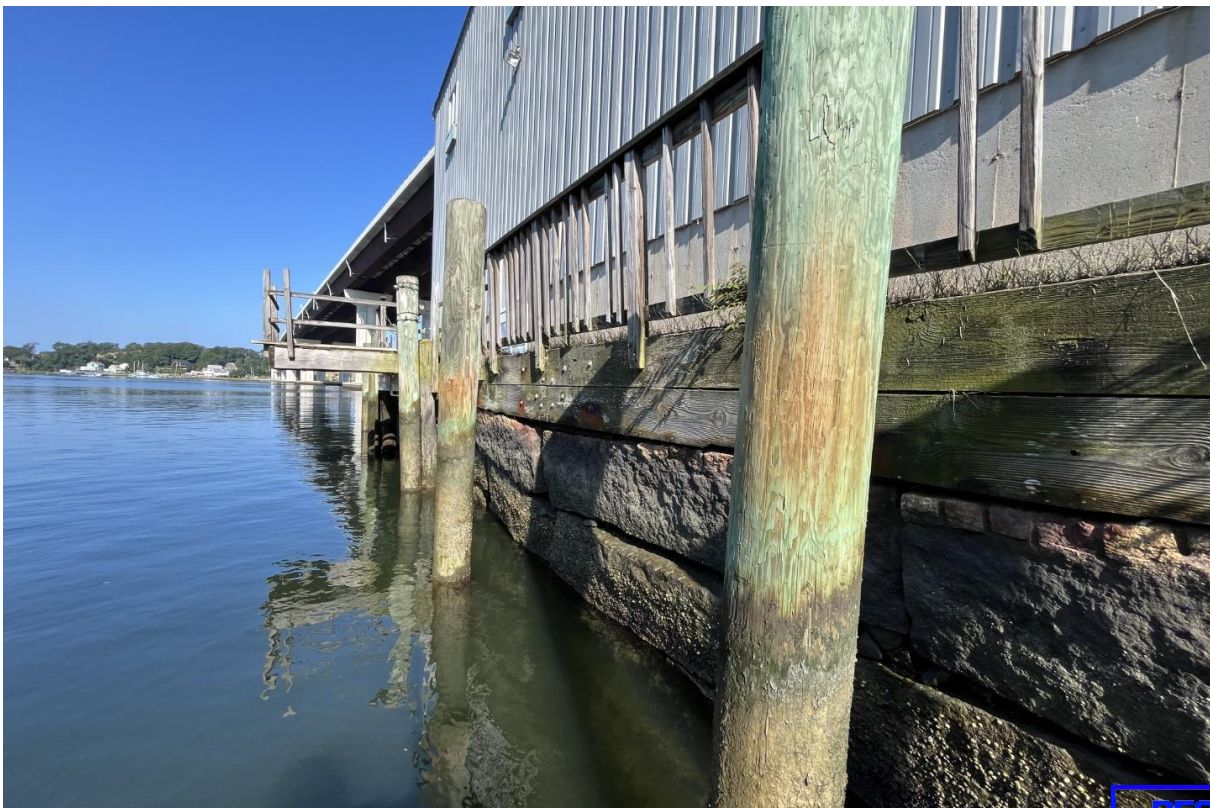


Photo No.4: View of existing timber and concrete walkway to be replaced, adjacent to building.

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**COASTAL RESOURCES
MANAGEMENT COUNCIL**



Photo No.5: View of existing timber pier and concrete pad to be replaced.



Photo No.6: View of existing riprap slope and timber pad adjacent to parking area.

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COASTAL RESOURCES
MANAGEMENT COUNCIL



Photo No.7: View of existing riprap slope, parking area, and timber walkway along the building.



Photo No.8: View looking south, of existing parking area with previous railroad basin.

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COASTAL RESOURCES
MANAGEMENT COUNCIL

CHASE MARINA

169 RIVERSIDE DRIVE
TIVERTON, RHODE ISLAND

PERMITTING
APRIL, 2025

PREPARED FOR:



RHODE ISLAND DEPARTMENT
OF ENVIRONMENTAL MANAGEMENT
301 GREAT ISLAND ROAD
NARRAGANSETT, RHODE ISLAND

DESIGNED BY:



GZA GEOENVIRONMENTAL, INC.
188 VALLEY ST. SUITE 300
PROVIDENCE RHODE ISLAND, 02909

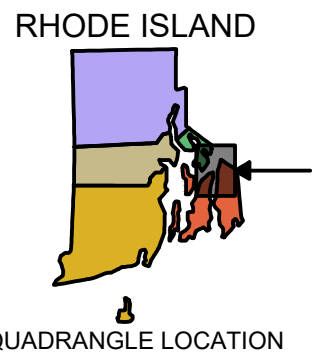


PROJECT LOCUS MAP

0 1000' 2000' 4000' 6000'
SCALE IN FEET 1" = 2000'

SOURCE: USGSSTORE.GOV
BASE MAP FROM THE FOLLOWING USGS QUADRANGLE MAP:
TIVERTON, RHODE ISLAND (2021), FALL RIVER, MASSACHUSETTS (2021)
DIGITAL TOPOGRAPHIC MAPS PROVIDED BY USGSSTORE.GOV.

CONTOUR ELEVATIONS REFERENCE NAVD 88.
CONTOURS ARE SHOWN IN FEET AT 10 FOOT INTERVALS



QUADRANGLE LOCATION



PROJECT VICINITY MAP

0 1000' 2000' 4000' 6000'
SCALE IN FEET 1" = 2000'

BASE MAP DEVELOPED FROM RIGIS AERIAL IMAGERY
PUBLISHED IN APRIL 2019.

INDEX OF DRAWINGS	
SHEET No.	SHEET TITLE
1	COVER SHEET AND INDEX OF DRAWINGS
2	GENERAL NOTES
3	EXISTING CONDITIONS PLAN
4	EXISTING CONDITIONS SECTIONS
5	TEMPORARY CONTROLS PLAN - PHASE I
6	TEMPORARY CONTROLS DETAILS
7	DEMOLITION PLAN
8	DEMOLITION SECTIONS
9	PROPOSED CONDITIONS PLAN - PHASE I
10	PROPOSED CONDITIONS SECTIONS - PHASE I
11	PROPOSED CONDITIONS PLAN - PHASE II
12	PROPOSED CONDITIONS SECTIONS - PHASE II



UNLESS SPECIFICALLY STATED BY WRITTEN AGREEMENT, THIS DRAWING IS THE SOLE PROPERTY OF GZA
GEOENVIRONMENTAL, INC. (GZA). THE INFORMATION SHOWN ON THE DRAWING IS SOLELY FOR USE BY
GZA'S CLIENT OR THE CLIENT'S DESIGNATED REPRESENTATIVE FOR THE SPECIFIC PROJECT AND LOCATION
IDENTIFIED ON THE DRAWING. THE DRAWING SHALL NOT BE TRANSFERRED, REUSED, COPIED, OR ALTERED
IN ANY MANNER FOR USE AT ANY OTHER LOCATION OR FOR ANY OTHER PURPOSE WITHOUT THE PRIOR
WRITTEN CONSENT OF GZA. ANY TRANSFER, REUSE, OR MODIFICATION TO THE DRAWING BY THE CLIENT
OR OTHERS, WITHOUT THE PRIOR WRITTEN EXPRESS CONSENT OF GZA, WILL BE AT THE USER'S SOLE
RISK AND WITHOUT ANY RISK OR LIABILITY TO GZA.

REFERENCE NOTES

EXISTING CONDITIONS PLAN BASE MAP DEVELOPED FROM THE FOLLOWING:

1. BASE MAP DEVELOPED FROM ELECTRONIC DRAWING FILE 8630A TOPO SHIPPED 2024-09-16, PREPARED BY DOUCET SURVEY AND TRANSMITTED TO GZA ON SEPTEMBER 16, 2024..
2. BATHYMETRIC SURVEY PERFORMED BY GZA IN SEPTEMBER, 2024..
3. LOCATION OF OVERHEAD BRIDGE APPROXIMATED USING GOOGLE EARTH IMAGERY.

GENERAL NOTES

1. LOCATIONS OF UNDERGROUND UTILITIES ARE APPROXIMATE ONLY, AND ARE NOT WARRANTED TO BE CORRECT. ADDITIONAL UTILITIES MAY EXIST WHICH ARE NOT INDICATED ON THESE PLANS. ALL EXISTING UTILITIES SHALL BE VERIFIED FOR SERVICE, SIZE, INVERT ELEVATION, LOCATION, ETC. PRIOR TO ANY CONSTRUCTION WORK IN THE VICINITY THEREOF. THE CONTRACTOR MUST NOTIFY DIGSAFE (PHONE NUMBER: 811) AT LEAST 3 FULL WORKING DAYS PRIOR TO ANY CONSTRUCTION. APPROPRIATE MUNICIPAL DEPARTMENTS MUST ALSO BE NOTIFIED. NOTIFY OWNER AND ENGINEER IN WRITING OF ANY AND ALL DISCREPANCIES PRIOR TO COMMENCING WORK.
2. THE CONTRACTOR SHALL BE RESPONSIBLE FOR FINDING, VERIFYING, AND RE-SETTING (IF NECESSARY) CONTROL POINTS AND BENCHMARKS FOR THE WORK OF THE CONTRACT OR OTHERWISE ESTABLISHING THEIR OWN.
3. THE RESPONSIBILITY FOR SAFETY IN, ON, OR ABOUT THE JOBSITE SHALL BE THAT OF THE CONSTRUCTION CONTRACTOR. THESE DRAWINGS DO NOT INCLUDE COMPONENTS WHICH MAY BE NECESSARY FOR CONSTRUCTION SAFETY.
4. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE MEANS AND METHODS OF CONSTRUCTION, EXCEPT WHERE SPECIFICALLY DETAILED IN THE PLANS. LIKEWISE, THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE SEQUENCE OF THE WORK, EXCEPT WHERE SPECIFICALLY DETAILED IN THE PLANS.
5. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL SURFACE AND GROUNDWATER CONTROL DURING THE WORK OF THE CONTRACT. TEMPORARY WATER CONTROL MEASURES SHALL BE, AT MINIMUM, AS REQUIRED BY THE PROJECT PLANS, SPECIFICATIONS, AND PERMIT CONDITIONS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL ADDITIONAL MEASURES NECESSARY FOR WATER CONTROL NECESSARY TO EXECUTE THE WORK OF THE CONTRACT "IN THE DRY". WATER CONTROL MEASURES ARE SUBJECT TO SPECIFIC LIMITS AND CONDITIONS AS MAY BE SHOWN ON THE PLANS.
6. ACCESS TO THE SITE SHALL BE FROM RIVERSIDE DRIVE. THE CONTRACTOR SHALL RESTORE ALL AREAS DISTURBED BY CONSTRUCTION AS PER THE PLANS AND SPECIFICATIONS. WHERE NO SPECIFIC INSTRUCTION IS GIVEN, RESTORATION SHALL BE TO THE ORIGINAL CONDITION OR BETTER AND AT NO ADDITIONAL COST TO THE OWNER. THE CONTRACTOR IS SPECIFICALLY INFORMED THAT THE RESTORATION REQUIREMENT APPLIES TO ALL AREAS DISTURBED AS A RESULT OF THE PROJECT, INCLUDING ACCESS ROADS.
7. SPECIFIC AREAS HAVE BEEN DESIGNATED AND DELINEATED ON THE PLANS AS A CONTRACTOR STAGING AREAS. THE CONTRACTOR SHALL USE THESE AREAS AND THESE AREAS ONLY FOR ON-SITE PARKING, OFFICE TRAILERS (IF NECESSARY), EQUIPMENT AND MATERIAL STORAGE, ETC. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY NECESSARY SIGNAGE, FENCING, SAFETY, SEDIMENT/EROSION CONTROL, IMPROVEMENTS, RESTORATIONS, ETC. IN THESE AREAS. AREAS WITHIN THE LIMITS OF THE WORK MAY BE USED FOR TEMPORARY STORAGE, HAUL ROADS, PARKING, ETC.; HOWEVER, NO ADDITIONAL CONSIDERATIONS OR PAYMENT WILL BE MADE FOR WORK NECESSARY TO RE-GRADE AND RESTORE SUCH AREAS TO PRE-CONSTRUCTION CONDITIONS OR RELOCATE ANY MATERIALS OR EQUIPMENT TEMPORARILY STORED WITHIN THE LIMITS OF THE WORK. IF THE CONTRACTOR REQUIRES AND IDENTIFIES ADDITIONAL STAGING AREAS ON THE OWNER'S PROPERTY, THE CONTRACTOR SHALL MAKE A WRITTEN REQUEST TO THE OWNER AND ENGINEER DESCRIBING THE NEED AND LOCATION OF THE PROPOSED AREA. NO GUARANTEE IS MADE THAT ADDITIONAL STAGING AREAS WILL BE MADE AVAILABLE.
8. PRIOR TO THE START OF WORK, THE CONTRACTOR SHALL DEVELOP, SUBMIT, AND MAINTAIN AN EMERGENCY CONTACT LIST WITH NAMES AND PHONE NUMBERS (DAY AND NIGHT) OF ALL KEY PERSONNEL INVOLVED WITH THE PROJECT. THE LIST SHALL SPECIFICALLY INCLUDE THE PERSON FROM THE CONTRACTOR WHO SHALL BE RESPONSIBLE FOR ENVIRONMENTAL COMPLIANCE. THE LIST SHALL BE PROVIDED TO THE OWNER, ENGINEER, AND RIDEM AND UPDATED AS NEEDED.
9. THE CONTRACTOR IS RESPONSIBLE FOR ALL PENALTIES AND DELAYS DUE TO NON-COMPLIANCE WITH PERMIT CONDITIONS.
10. THE CONTRACTOR SHALL VERIFY ALL DIMENSIONS IN THE FIELD.

EROSION AND SEDIMENTATION CONTROL

1. TEMPORARY CONTROL OF EROSION AND SEDIMENT DISCHARGE IS REQUIRED THROUGHOUT THE DURATION OF THE PROJECT AND UNTIL PROPOSED STABILIZATION IS ACHIEVED. IT IS THE CONTRACTOR'S RESPONSIBILITY TO PROTECT THE AREAS WITHIN THE LIMITS OF WORK AND BEYOND FROM SEDIMENT AND/OR POLLUTANTS ORIGINATING FROM ANY WORK DONE ON OR IN SUPPORT OF THE PROJECT, INCLUDING SEDIMENT DUE TO EROSION FROM STORMWATER RUNOFF.
2. THE CONTRACTOR SHALL BE RESPONSIBLE FOR IMPLEMENTING ALL TEMPORARY SEDIMENT AND EROSION CONTROL MEASURES NECESSARY TO EXECUTE AND COMPLETE THE WORK OF THE CONTRACT, IN COMPLIANCE WITH THE TERMS AND CONDITIONS CONTAINED IN THE CONTRACT, PROJECT PERMITS AND ALL STATE AND LOCAL ORDINANCES THAT APPLY. CONTROLS SHOWN ON THE CONTRACT DRAWINGS SHALL BE CONSIDERED MINIMUM REQUIREMENTS. THE CONTRACTOR SHALL EMPLOY WHATEVER SUPPLEMENTARY MEASURES NECESSARY TO PROTECT WATER, AND ADJACENT AREAS FROM DISTURBANCE OR DISCHARGE OF SEDIMENTS.
3. ALL NECESSARY PRECAUTIONS SHALL BE TAKEN TO PREVENT MIGRATION INTO WATER BY SILT, SEDIMENT, FUELS, SOLVENTS, LUBRICANTS, CONCRETE, OR ANY OTHER POLLUTANTS ASSOCIATED WITH CONSTRUCTION PROCEDURES.
4. ACTUAL LOCATION OF EROSION CONTROLS AND BEST MANAGEMENT PRACTICES (BMP) MAY VARY DUE TO FIELD CHANGES, ONGOING CONSTRUCTION, ACCESS NEEDS, WEATHER, ETC. THE CONTRACTOR SHALL BE RESPONSIBLE FOR IDENTIFYING THESE CHANGES AND ADJUSTING EROSION CONTROLS AND BMP LOCATIONS ACCORDINGLY. IN PARTICULAR, THE CONTRACTOR SHALL COORDINATE THE INSTALLATION AND RELOCATION OF BMPS WITH PROJECT PHASING, AS NECESSARY.
5. ALL EROSION CONTROLS AND BMPS SHALL REMAIN IN PLACE, EXCEPT AS OTHERWISE NECESSARY, UNTIL CONSTRUCTION IS COMPLETED AND PROPOSED STABILIZATION IS ACHIEVED. ALL CONTROLS AND BMPS SHALL BE SUBJECT TO INSPECTION BY THE OWNER AND THEIR REPRESENTATIVE AT ANYTIME THEREAFTER.
6. ADDITIONAL EROSION CONTROL BARRIERS SHALL BE INSTALLED AT THE DIRECTION OF THE ENGINEER TO MINIMIZE THE THREAT OF ADVERSE IMPACT DURING THE CONSTRUCTION PROCESS. AN ADEQUATE SUPPLY OF REPLACEMENT EROSION CONTROL BARRIERS WILL BE AVAILABLE ON-SITE FOR EMERGENCY PURPOSES.
7. PERIODIC INSPECTION, MAINTENANCE, AND CLEANING OF TEMPORARY EROSION AND SEDIMENT CONTROL MEASURES AND BMPS ARE REQUIRED. ALL CONTROLS AND BMPS SHALL BE INSPECTED EVERY 7 DAYS AND WITHIN 24 HOURS OF RAINFALL EVENTS OF 0.5 INCHES OR GREATER. ROUTINE INSPECTION AND MAINTENANCE WILL REDUCE THE CHANCE OF POLLUTING STORMWATER BY FINDING AND CORRECTING PROBLEMS BEFORE THE NEXT RAIN EVENT. THE CONTRACTOR WILL BE REQUIRED TO KEEP A WRITTEN, UPDATED SITE MAINTENANCE LOG DOCUMENTING INSPECTION AND MAINTENANCE ACTIVITY.
8. CONTRACTOR SHALL PROVIDE AND MAINTAIN TURBIDITY CURTAINS AND IS RESPONSIBLE FOR DETERMINING LAYOUT AND INSTALLATION PROCEDURES IN ACCORDANCE WITH CONTRACT DOCUMENTS.
9. TURBIDITY CURTAINS SHALL BE INSTALLED TO SURROUND THE LIMIT OF WORK, AS NEEDED, INCLUDING BUT NOT LIMITED TO DEMOLITION AND WALL CONSTRUCTION.

WATER CONTROL NOTES

1. TEMPORARY WATER CONTROL BY THE CONTRACTOR SHALL BE PERFORMED AS SPECIFIED IN THE CONTRACT DOCUMENTS.
2. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL TEMPORARY WATER CONTROL, SURFACE WATER AND GROUNDWATER CONTROL, NECESSARY TO EXECUTE AND COMPLETE THE WORK OF THE CONTRACT SUBJECT TO THE RESTRICTIONS CONTAINED IN THE CONTRACT AND PROJECT PERMITS. CONTROLS SHOWN IN THE CONTRACT DRAWINGS SHALL BE CONSIDERED MINIMUM REQUIREMENTS. THE CONTRACTOR SHALL EMPLOY WHATEVER SUPPLEMENTARY MEASURES NECESSARY TO PROTECT THE SITE AND THE WORK.
3. ALL TEMPORARY WATER CONTROL MEASURES SHALL BE IMPLEMENTED IN CONJUNCTION WITH APPROPRIATE SEDIMENT AND EROSION CONTROL MEASURES SO AS TO MINIMIZE TO THE GREATEST EXTENT POSSIBLE RELEASE OF SEDIMENT INTO WATER BODIES AND POTENTIAL EROSION OF SOIL.
4. ANY TEMPORARY PUMPS USED AT THE SITE MUST BE PROPERLY BAFFLED AGAINST EXCESSIVE NOISE. PUMPS OR GENERATORS WHICH USE LIQUID FUEL MUST BE PLACED WITHIN AN IMPERMEABLE SECONDARY CONTAINMENT AREA WITH SUFFICIENT CAPACITY TO CONTAIN THE FULL VOLUME OF THE FUEL TANK.
5. WATER PUMPED FROM THE EXCAVATIONS MUST BE PASSED THROUGH A PUMPED WATER FILTER BAG OR OTHER SUCH BEST MANAGEMENT PRACTICE FEATURE PRIOR TO BEING DISCHARGED BACK TO A WATER BODY. DISCHARGE WATER SHALL MEET APPROPRIATE WATER QUALITY STANDARDS.
6. PUMPED WATER DISCHARGE AREAS MUST BE PROPERLY PROTECTED TO PREVENT EROSION BY HIGH VELOCITY FLOW.

CONSTRUCTION NOTES:

1. THE CONTRACTOR AND/OR SUBCONTRACTORS MUST SHOW EVIDENCE OF PRIOR EXPERIENCE AND SUCCESSFUL COMPLETION OF THE TYPE OF REPAIR WORK INDICATED.

LAYOUT NOTES:

1. ALL LINES ARE PERPENDICULAR OR PARALLEL TO THE LINES FROM WHICH THEY ARE MEASURED UNLESS OTHERWISE INDICATED.
2. WALL LAYOUTS SHALL BE FIELD ADJUSTED TO PROVIDE THE MINIMUM WALKWAY OR GROUND FEATURE DIMENSIONS AS SHOWN ON THE PLANS.

LOAM AND SEED NOTES:

1. CONTRACTOR SHALL APPLY FERTILIZER AND LOAM TO PREPARE TOPSOIL SURFACE IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS.
2. CONTRACTOR SHALL SEED ENTIRE DISTURBED AREA AS INDICATED IN THE PROJECT DOCUMENTS.
3. CONTRACTOR SHALL MULCH ENTIRE SEEDED AREA WITH STRAW MULCH TO HELP DEVELOP AN ACCEPTABLE STAND OF GRASS.
4. CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING THE SEEDED AREA THROUGH ONE GROWING SEASON, MAINTENANCE INCLUDES MOWING, WATERING, AND RE-SEEDING AS REQUIRED TO DEVELOP A SATISFACTORY STAND OF HEALTHY GRASS.

DEMOLITION NOTES:

1. EXCAVATED MATERIALS SUSPECTED OF CONTAMINATION SHALL BE SEPARATED, AND WRAPPED IN POLYTHENE SHEETING STOCKPILED ON-SITE FOR EVALUATION BY THE ENGINEER.
2. CONTRACTOR SHALL EXERCISE EXTREME CAUTION TO AVOID DAMAGE TO ANY EXISTING UTILITIES AND STRUCTURES TO REMAIN IN PLACE DURING CONSTRUCTION AND/OR AFTER CONSTRUCTION IS COMPLETE.
3. ALL MATERIALS REMOVED AND NOT SPECIFIED TO BE SALVAGED OR REUSED SHALL BECOME THE PROPERTY OF THE CONTRACTOR AND LEGALLY DISPOSED OF OFF-SITE.

MATERIAL NOTES:

1. MATERIAL AND PRODUCT REQUIREMENTS, WHERE REFERENCED AS RIDOT, THE REQUIREMENT REFERS TO THE RHODE ISLAND DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE DESIGN "BLUE BOOK", LATEST EDITION.

GRAVEL BORROW: M.01.02.1

CRUSHED STONE: M.01.06

COMMON FILL: M.01.01

LOAM: L.01.01.1 / M.18.01

FILTER FABRIC: NON-WOVEN FABRIC, INERT TO UV LIGHT, EQUAL IN PROPERTIES TO MIRAFI 180N OR APPROVED EQUAL

2. REINFORCING SHALL HAVE A MINIMUM OF 2 INCHES OF COVER WHEN CONCRETE IS CAST AGAINST FORMWORK. FOR CONCRETE CAST AGAINST EARTH, THE COVER SHALL BE 3 INCHES MINIMUM.

CONCRETE NOTES:

CONCRETE - 28-DAY COMPRESSIVE STRENGTH OF 5,000 PSI MINIMUM
3/4 INCH MAXIMUM AGGREGATE SIZE
AIR CONTENT OF 5.0% - 7.0%
COLD WEATHER OR HOT WEATHER REQUIREMENTS AS APPLICABLE AND AS DETERMINED BY THE ENGINEER

CEMENT - PORTLAND CEMENT, ASTM C 150, TYPE II

REINFORCING STEEL NOTES:

REINFORCING BAR: ASTM A 615, GRADE 60, GALVANIZED ASTM A 123, ASTM A 153

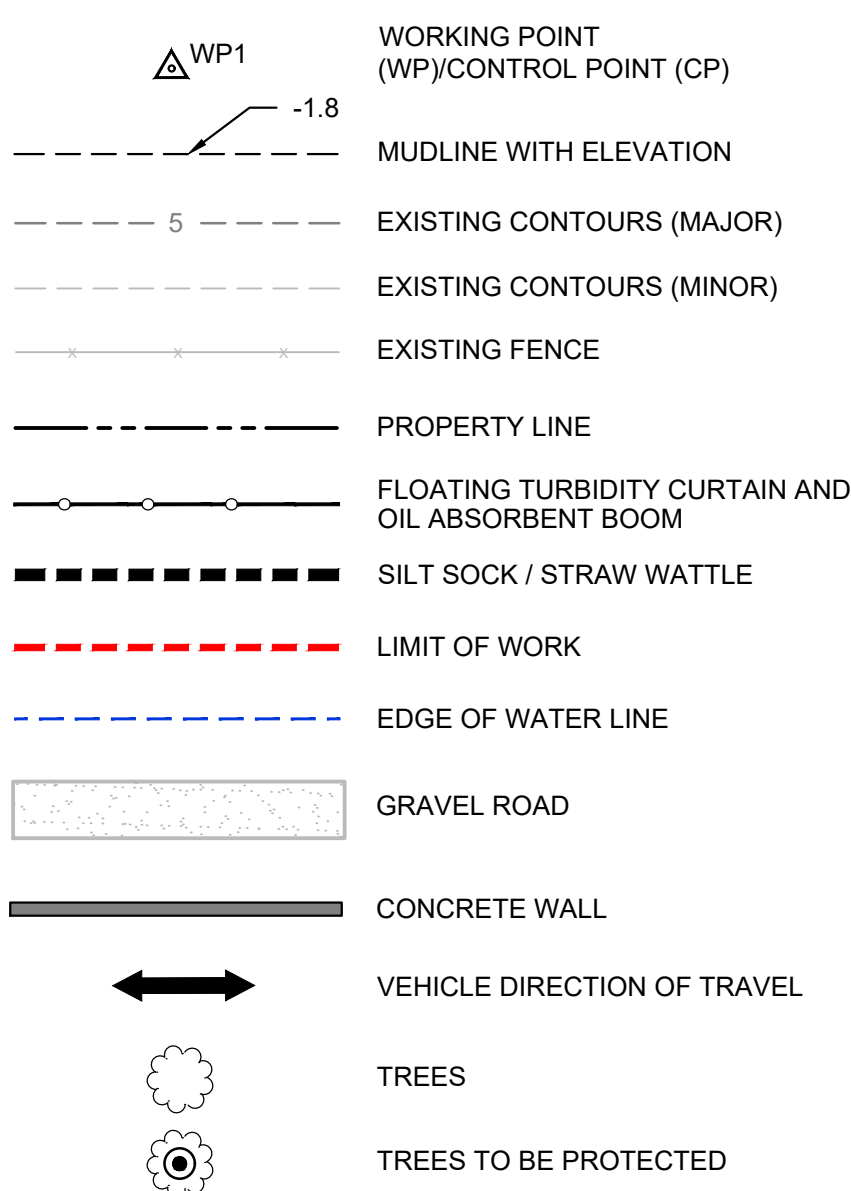
DOWELS: ASTM A 615, GRADE 60, GALVANIZED ASTM A 123, ASTM A 153

GROUT: TWO-COMPONENT TYPE EPOXY ADHESIVE SIKADUR 32, HI MOD OR APPROVED EQUAL

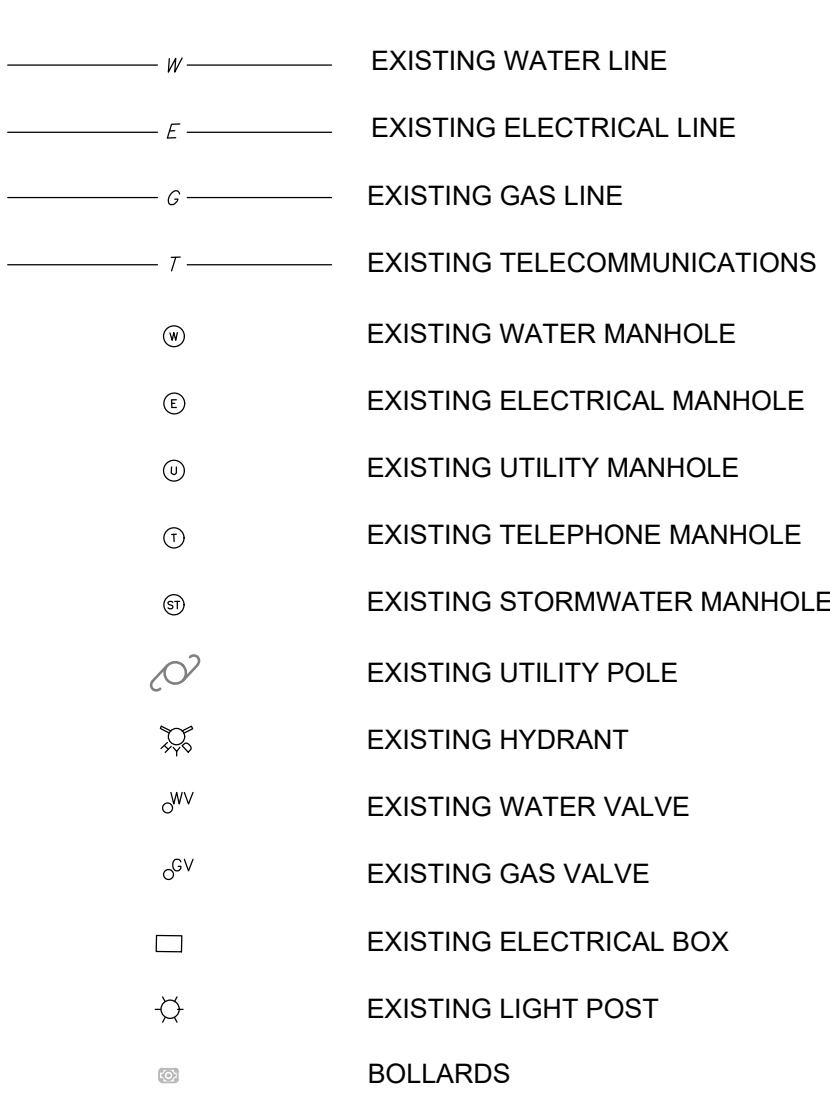
DESIGN CRITERIA:

- A. AMERICAN CONCRETE INSTITUTE, ACI 318, (LATEST EDITION)
- B. STEEL CONSTRUCTION MANUAL, AMERICAN INSTITUTE OF STEEL CONSTRUCTION (LATEST EDITION)
- C. WELDING OF STRUCTURAL AND MISCELLANEOUS STEEL - STRUCTURAL WELDING CODE (LATEST EDITION)
- D. RHODE ISLAND BUILDING CODE

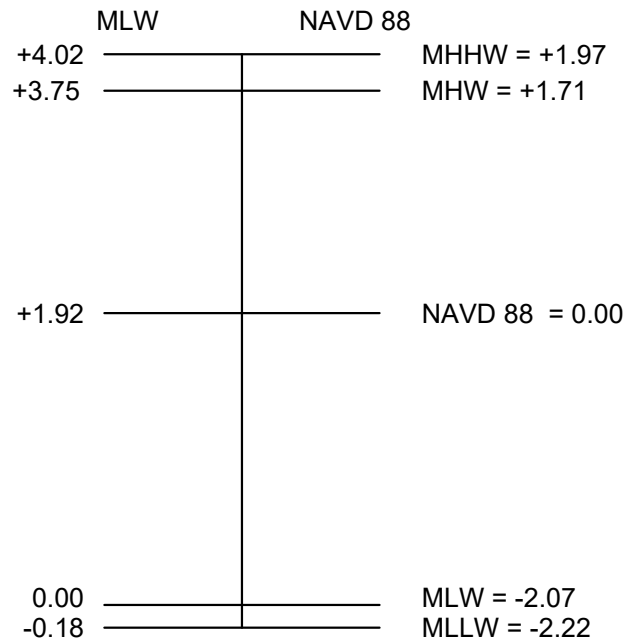
LEGEND



UTILITIES LEGEND:



TIDAL ELEVATIONS



NOTE:
TIDAL ELEVATIONS ARE REFERENCED TO NOAA STATION 8450948, ANTHONY POINT, TIVERTON, RI.

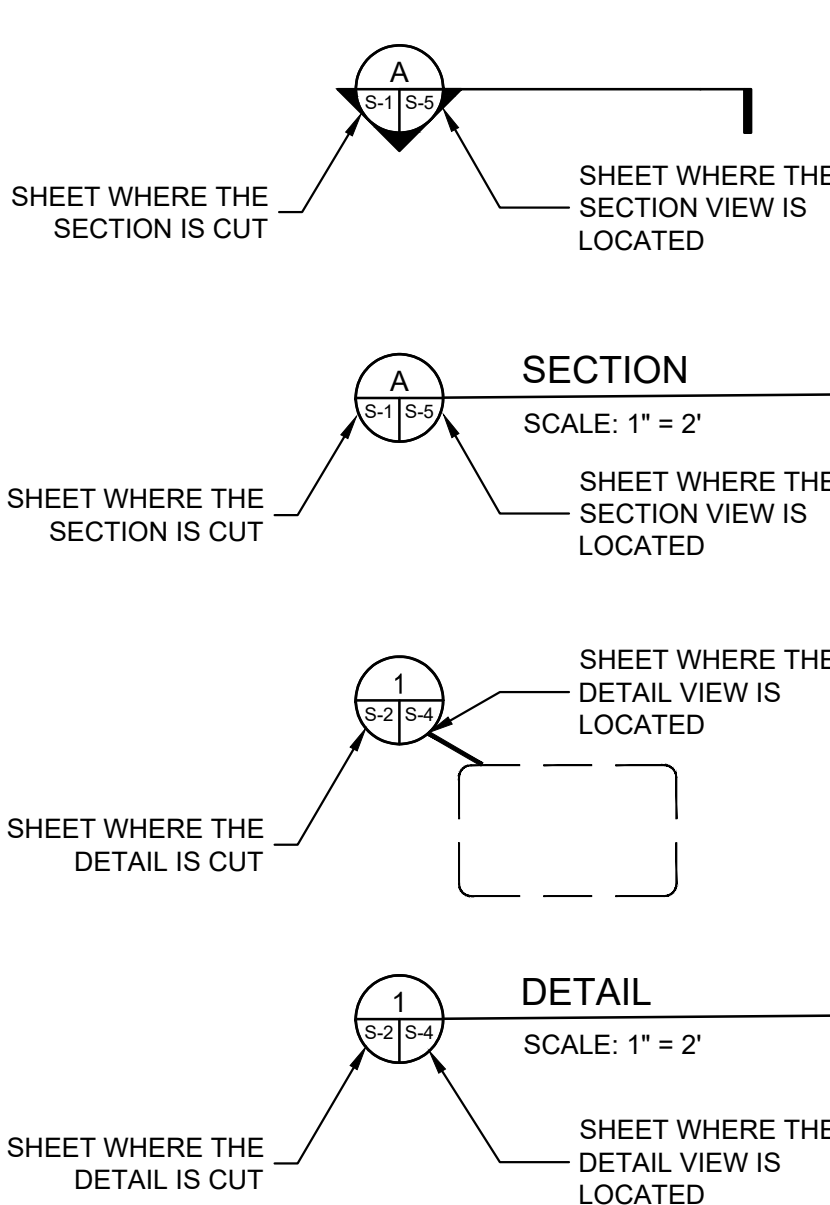
SURVEY AND PROJECT DATUM:

1. ALL ELEVATIONS SHOWN ON PLANS ARE IN U.S. FEET AND REFERENCE THE NORTH AMERICAN VERTICAL DATUM OF 1988 (NAVD88).
2. COORDINATES ARE BASED ON NORTH AMERICAN DATUM OF 1983 (NAD83), RHODE ISLAND STATE PLANE, EASTERN ZONE, U.S. FEET (RI83-EF).
3. CONTRACTOR SHALL MAINTAIN ADEQUATE SURVEY CONTROL AT ALL TIMES TO ESTABLISH AND MAINTAIN ALL LINES AND ELEVATIONS.

ABBREVIATIONS AND ACRONYMS

CON	CITY OF NEWPORT
DIA.	DIAMETER
D.I.	DUCTILE IRON
EL.	ELEVATION
HASP	HEALTH AND SAFETY PLAN
HDPE	HIGH-DENSITY POLYETHYLENE
INV.	INVERT
MHHW	MEAN HIGH, HIGH WATER
MHW	MEAN HIGH WATER
MIN.	MINIMUM
MLLW	MEAN LOW, LOW WATER
MLW	MEAN LOW WATER
NOAA	NATIONAL OCEANIC AND ATMOSPHERIC ADMINISTRATION
OHW	OVERHEAD WIRES
PCP	PRE-CAST PIPE
STA.	STATION
TYP.	TYPICAL
N / F	NON OR FORMERLY

ANNOTATIONS AND LABELS



PERMITTING ONLY
NOT FOR CONSTRUCTION

REV	DESCRIPTION	DATE	BY

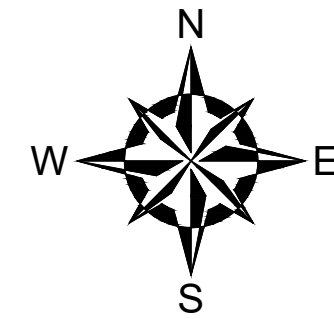
PROJECT NO:	35429.00
DATE:	APRIL, 2025
PROJECT MGR:	TWS
DESIGNED BY:	JTF
DRAWN BY:	GRB
CHECKED BY:	TWS
REVIEWED BY:	MJP
SCALE:	AS NOTED
REVISION NO.	

CHASE MARINA
169 RIVERSIDE DRIVE
TIVERTON, RHODE ISLAND

GENERAL NOTES

DRAWING
2
SHEET NO. 2 OF 12





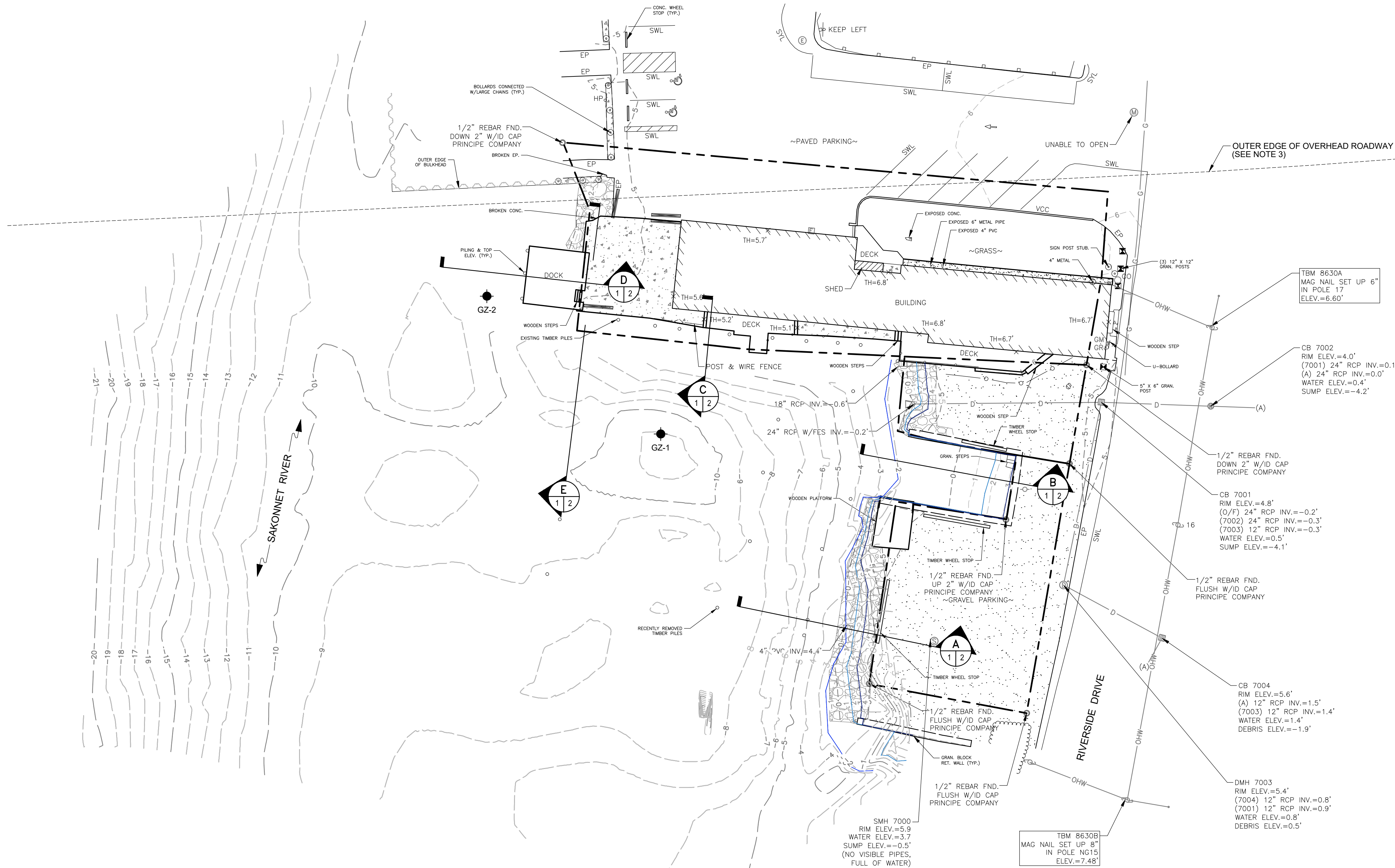
REFERENCE NOTES:

1. BASE MAP DEVELOPED FROM ELECTRONIC DRAWING FILE 8630A TOPO SHIPPED 2024-09-16 PREPARED BY DOUCET SURVEY TRANSMITTED TO GZA ON SEPTEMBER 16, 2024.
2. BATHYMETRIC SURVEY PERFORMED BY GZA IN SEPTEMBER, 2024.
3. OVERHEAD BRIDGE NOT SHOWN FOR CLARITY.

LEGEND

---	APPROXIMATE LOT LINE PER REF. PLAN
- - -	MAJOR CONTOUR LINE
- - -	MINOR CONTOUR LINE
—	RETAINING WALL
—	CHAIN LINK FENCE
—	HANDRAIL
—	GUARDRAIL
—	OVERHEAD WIRE
—	DRAIN LINE
—	GAS LINE
—	PROPERTY BOUNDARY
—	SHRUB LINE
—	CONCRETE
—	RIP RAP
—	CRUSHED STONE
—	SPOT GRADE
—	PIPE/ROD FOUND
—	UTILITY POLE
—	UTILITY POLE & GUY WIRE
—	UTILITY POLE STUB
—	DRAIN MANHOLE
—	CATCH BASIN
—	FLARED END SECTION
—	SEWER MANHOLE
—	CLEANOUT
—	GAS METER
—	GAS REGULATOR VALVE
—	ELECTRIC MANHOLE
—	MANHOLE
—	SIGN
—	SQUARE POST
—	POST
—	WOODEN PILING & TOP ELEVATION
—	ACCESSIBLE PARKING SPACE
—	FLOW DIRECTION
—	JERSEY BARRIER
—	CONCRETE
—	EDGE OF PAVEMENT
—	GRANITE
—	POLYVINYL CHLORIDE PIPE
—	REINFORCED CONCRETE PIPE
—	RETAINING WALL
—	SINGLE WHITE LINE
—	SINGLE YELLOW LINE
—	THRESHOLD ELEVATION
—	TYPICAL
—	VERTICAL GRANITE CURB
—	INVERT I.D. CONNECTION UNKNOWN

TEST BORINGS PERFORMED BY REAGAN MARINE CONSTRUCTION ON FEBRUARY 25 AND 26, 2025 AND OBSERVED BY GZA.



APPROVED:



PREPARED FOR:



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PERMITTING ONLY
NOT FOR CONSTRUCTION

REV DESCRIPTION DATE BY

PROJECT NO: 35429.00

DATE: APRIL, 2025

PROJECT MGR: TWS

DESIGNED BY: JTF

DRAWN BY: GRB

CHECKED BY: TWS

REVIEWED BY: MJP

SCALE: AS NOTED

REVISION NO.

CHASE MARINA
169 RIVERSIDE DRIVE
TIVERTON, RHODE ISLAND

EXISTING CONDITIONS PLAN

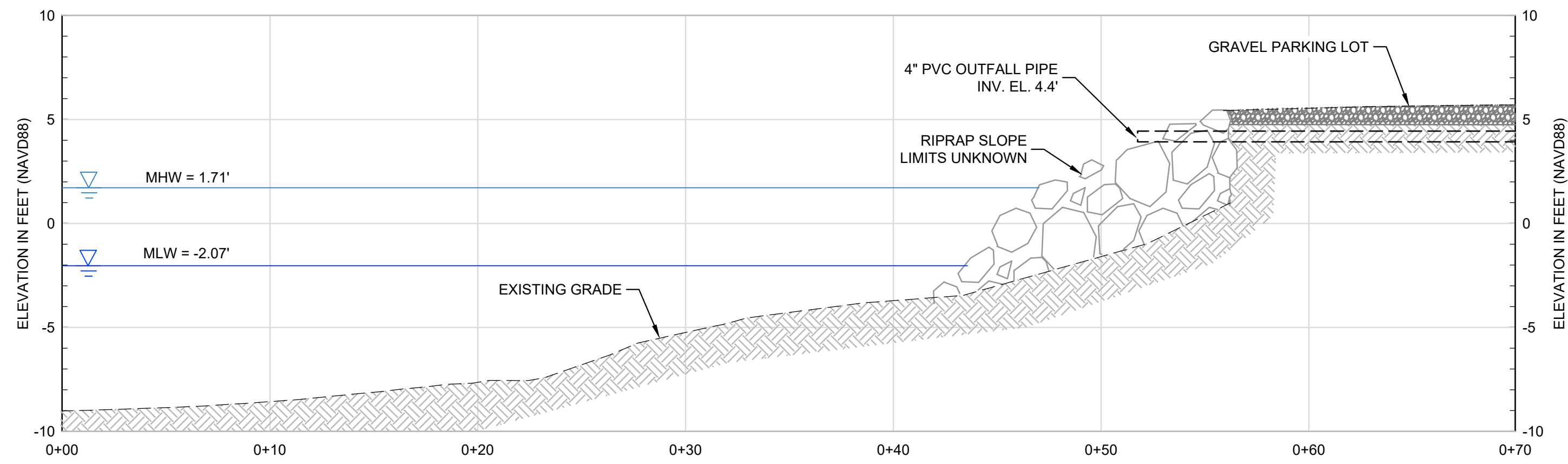
DRAWING

3

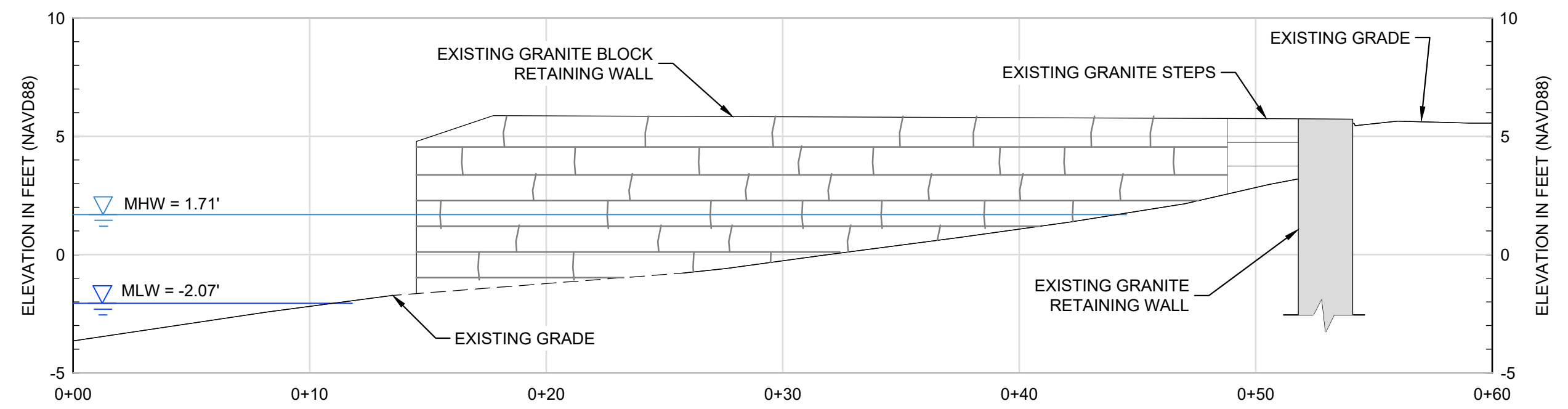
SHEET NO. 3 OF 12



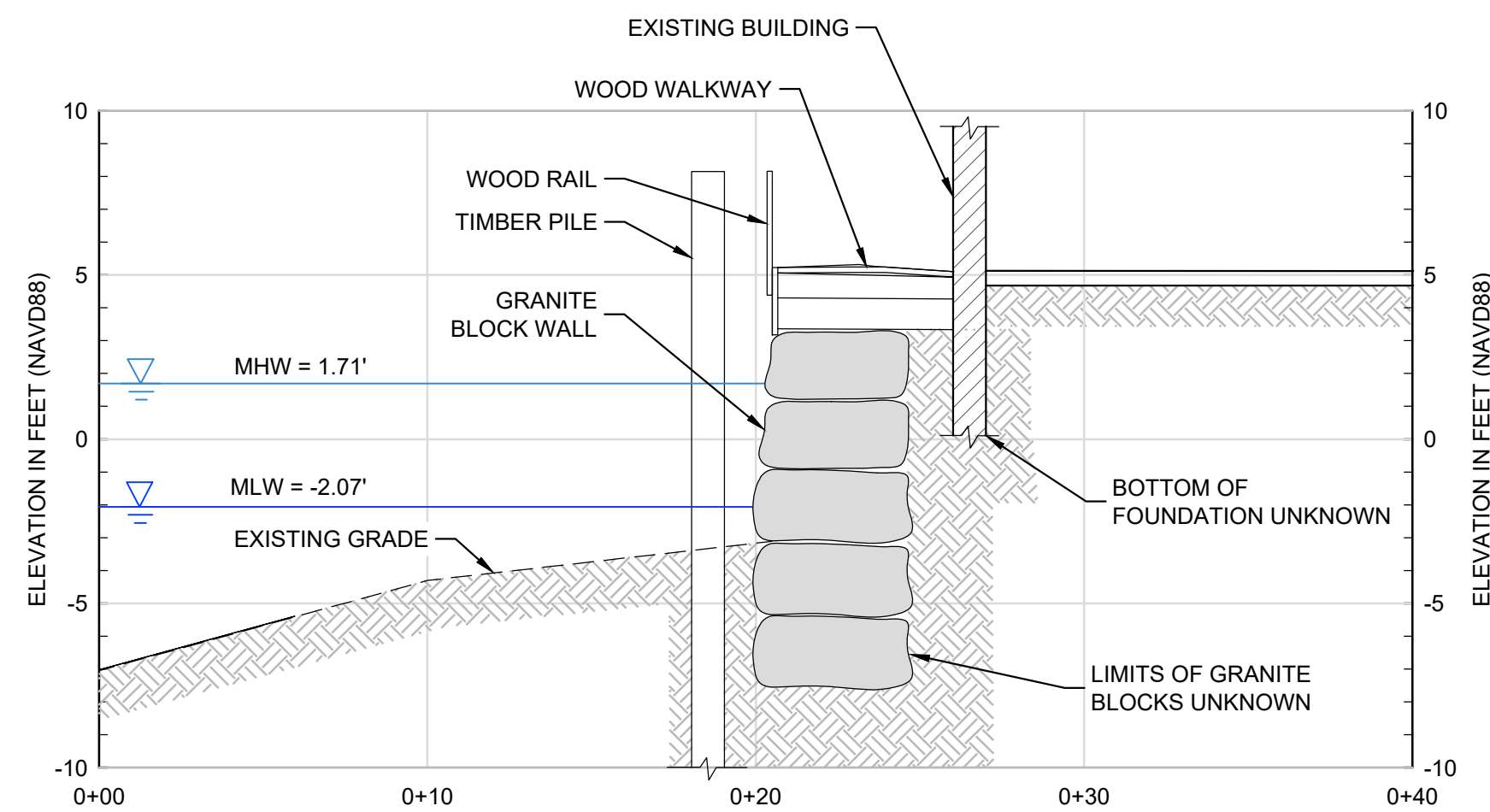
© 2025 - GZA GeoEnvironmental, Inc. GZA-J\GEO\35429.TWS\FIGURES\CAD\DWGS\CURRENT\PLANS\PERMIT SET\35429.00_4_EXISTING COND SECTIONS.DWG 4 April 29, 2025 GARY BASTIEN



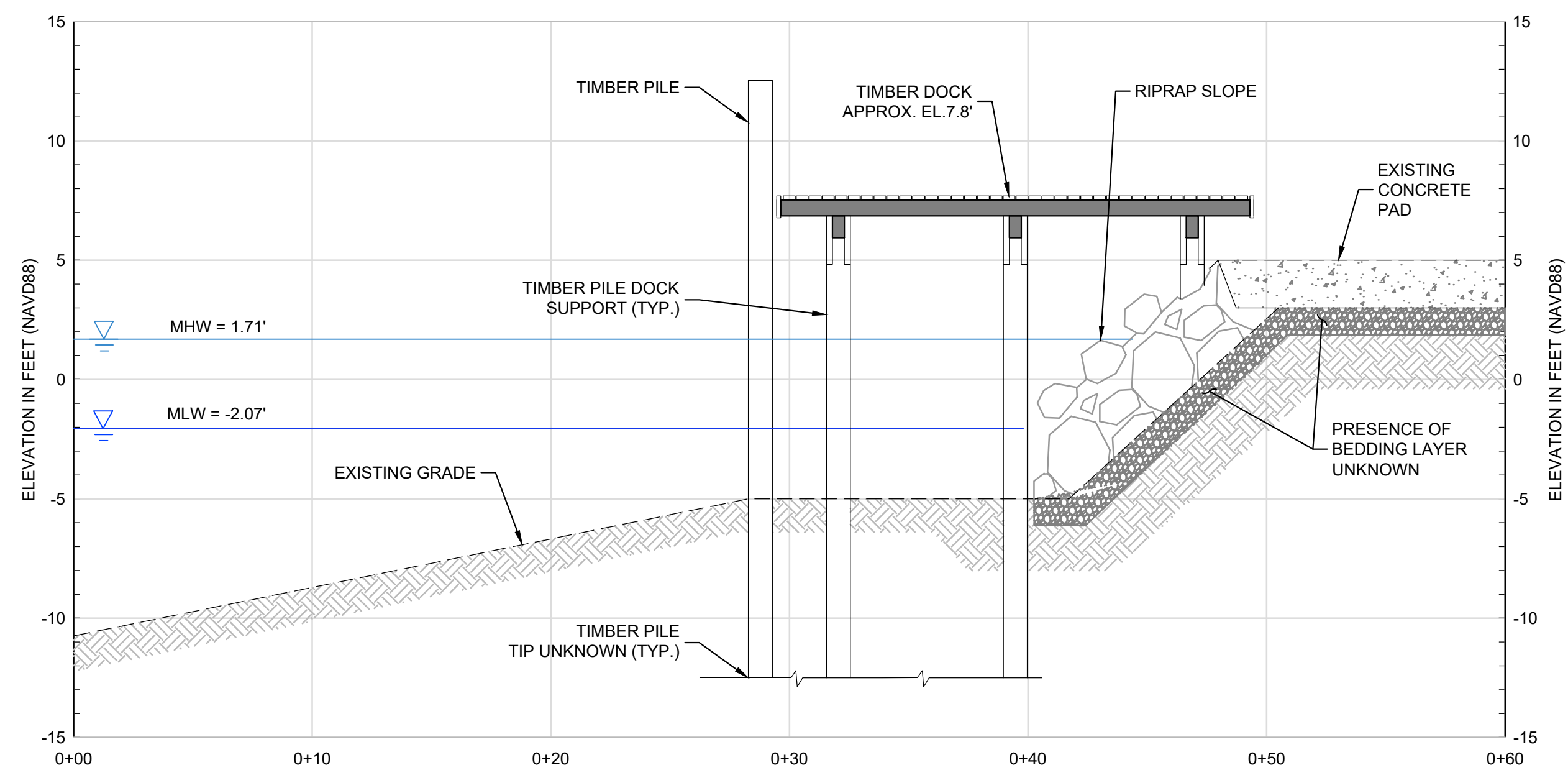
A RIPRAP SLOPE / PARKING LOT - SECTION
SCALE: 1"=5'



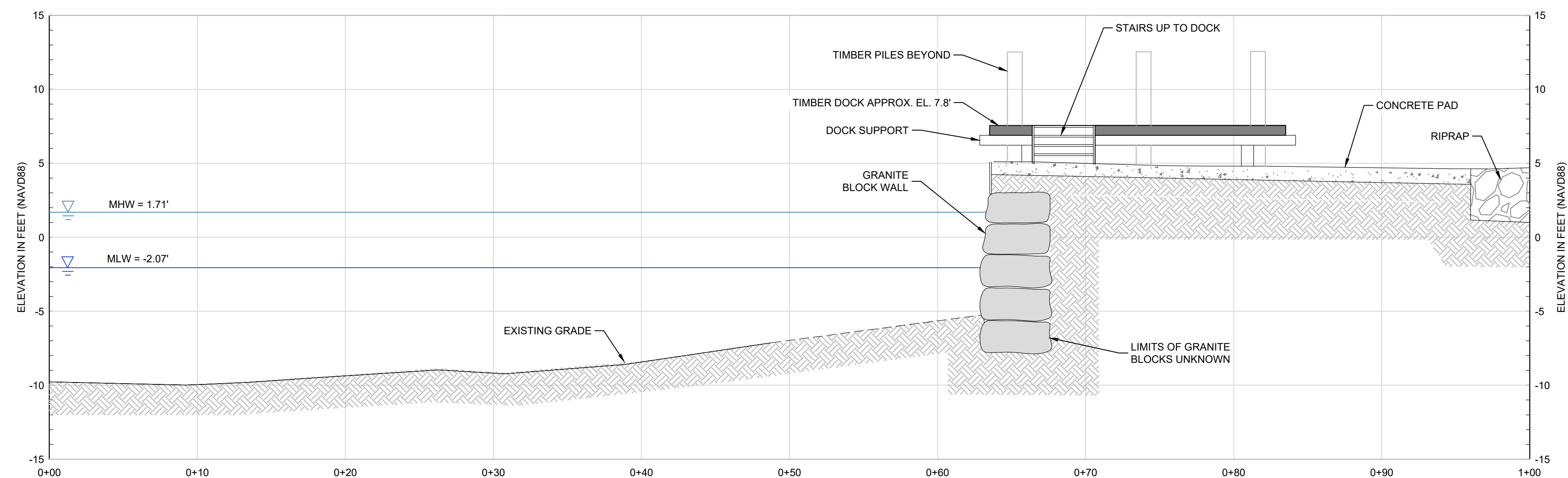
B GRANITE BLOCK WALL - SECTION
SCALE: 1"=5'



C GRANITE BLOCK WALL - SECTION
SCALE: 1"=5'



D DOCK PLATFORM - SECTION
SCALE: 1"=5'



E DOCK PLATFORM - SECTION
SCALE: 1"=5'



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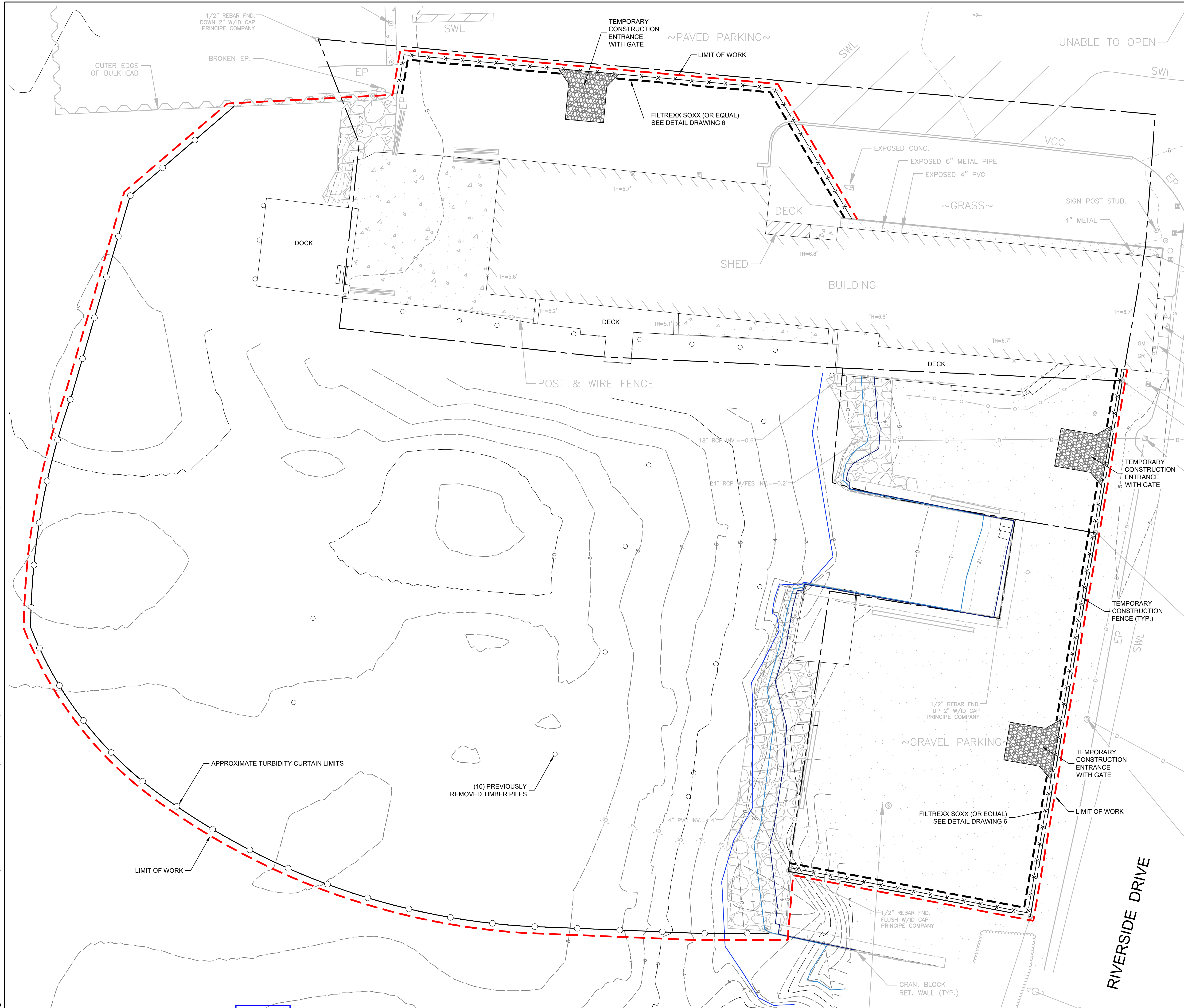
PROJECT NO: 35429.00
DATE: APRIL, 2025
PROJECT MGR: TWS
DESIGNED BY: JTF
DRAWN BY: GRB
CHECKED BY: TWS
REVIEWED BY: MJP
SCALE: AS NOTED
REVISION NO.

CHASE MARINA
169 RIVERSIDE DRIVE
TIVERTON, RHODE ISLAND

EXISTING CONDITIONS SECTIONS

DRAWING
4
SHEET NO. 4 OF 12

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REFERENCE NOTES:

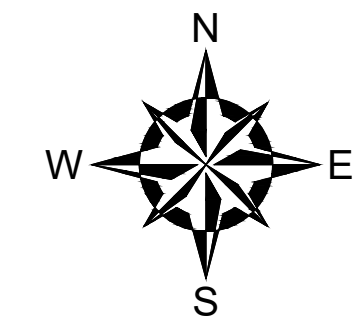
1. BASE MAP DEVELOPED FROM ELECTRONIC DRAWING FILE 8630A TOPO SHIPPED 2024-09-16 PREPARED BY DORCET SURVEY TRANSMITTED TO GZA ON SEPTEMBER 16, 2024.
2. BATHYMETRIC SURVEY PERFORMED BY GZA IN SEPTEMBER, 2024.

TEMPORARY CONTROL NOTES:

1. CONTRACTOR SHALL BE RESPONSIBLE FOR ESTABLISHING AND MAINTAINING ALL TEMPORARY EROSION AND SEDIMENT CONTROLS IN ACCORDANCE WITH PERMITS AND CONTRACT DOCUMENTS.
2. INSTALL EROSION AND SEDIMENT CONTROLS BEFORE DISTURBING ANY SOILS, SEDIMENT OR VEGETATION AT THE SITE.
3. ADJUST, RELOCATE OR PROVIDE ADDITIONAL EROSION AND SEDIMENT CONTROLS AS WORK PROGRESSES INTO PREVIOUSLY UNDISTURBED AREAS OF THE SITE.
4. MAINTAIN EROSION AND SEDIMENT CONTROLS UNTIL THE SITE IS RESTORED AND SITE IMPROVEMENTS INCLUDING LANDSCAPING, IF ANY, ARE COMPLETE WITH UNDERLYING SOILS PERMANENTLY STABILIZED.
5. CARE SHALL BE TAKEN NOT TO PLACE REMOVED SEDIMENTS WITHIN THE PATH OF EXISTING, NEWLY CREATED OR PROPOSED AREAS THAT ARE SUBJECT TO STORMWATER FLOW.
6. CONTRACTOR SHALL PROVIDE AND MAINTAIN TURBIDITY CURTAINS AND IS RESPONSIBLE FOR DETERMINING LAYOUT AND INSTALLATION PROCEDURES IN ACCORDANCE WITH CONTRACT DOCUMENTS.
7. TURBIDITY CURTAINS SHALL BE INSTALLED TO SURROUND THE LIMIT OF WORK, AS NEEDED, INCLUDING BUT NOT LIMITED TO DEMOLITION AND WALL CONSTRUCTION.
8. DEWATER LANDWARD OF BULKHEAD USING A PUMP WITH A FILTER BAG. PUMP WATER INTO NEWPORT HARBOR.
9. PROTECTING DEVICES SHALL BE USED AROUND TREES AND TRANSFORMERS ON SITE.

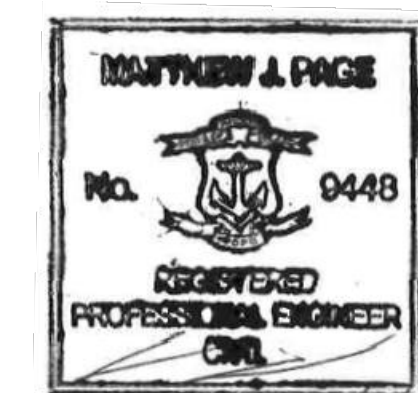
LEGEND - EROSION CONTROL

- APPROXIMATE PROPERTY BOUNDARY
- LIMIT OF WORK
- FILTREXX (R) SILT SOXX (OR EQUAL)
- x-x-x- TEMPORARY FENCE LINE
- o FLOATING TURBIDITY CURTAIN



0 5' 10' 20' 30'
SCALE IN FEET 1" = 10'

APPROVED:



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REV DESCRIPTION DATE BY

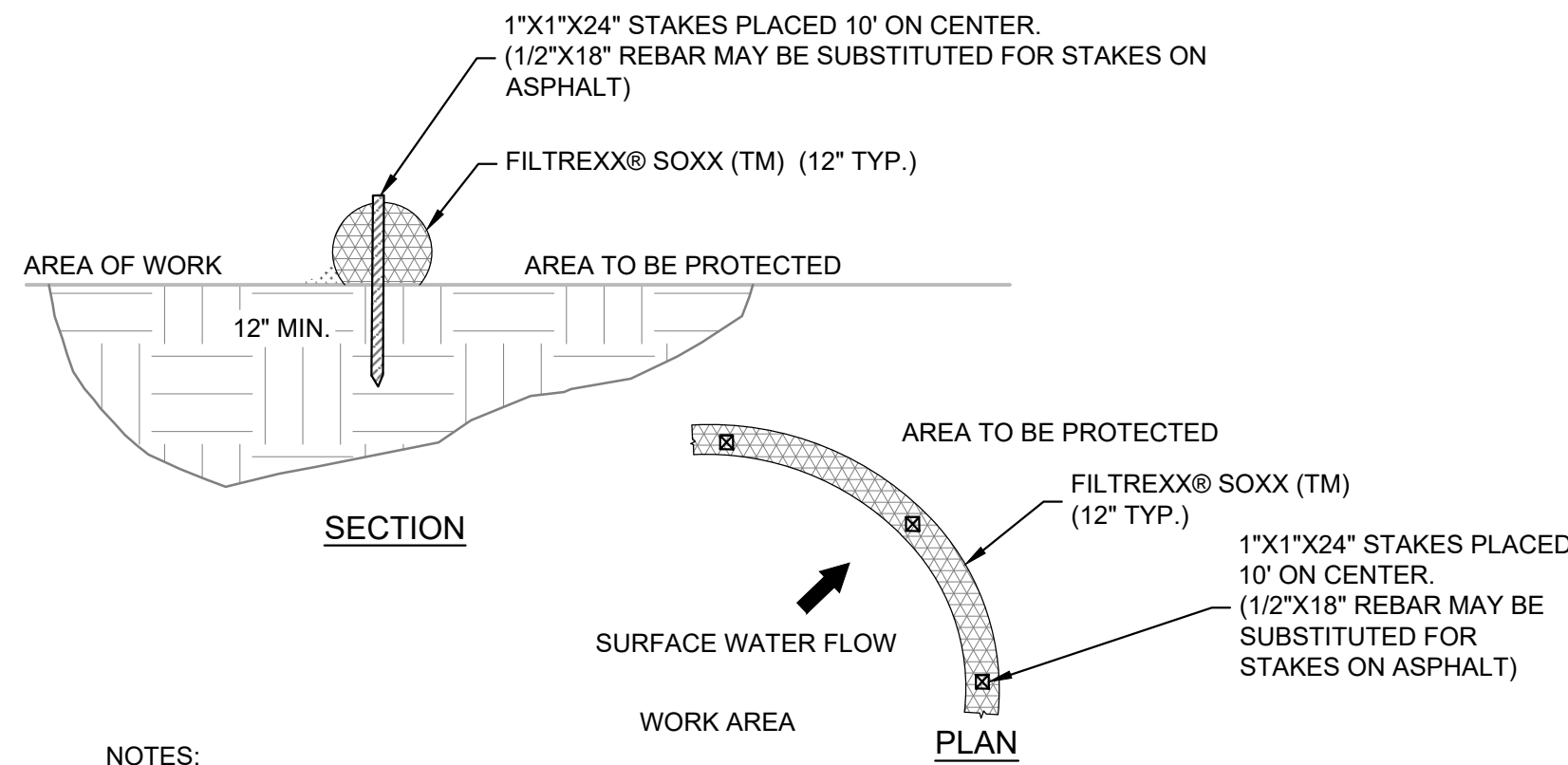
PROJECT NO: 35429.00
DATE: APRIL, 2025
PROJECT MGR: TWS
DESIGNED BY: JTF
DRAWN BY: GRB
CHECKED BY: TWS
REVIEWED BY: MJP
SCALE: AS NOTED
REVISION NO.

CHASE MARINA
169 RIVERSIDE DRIVE
TIVERTON, RHODE ISLAND

TEMPORARY CONTROLS PLAN
PHASE I

DRAWING
5
SHEET NO. 5 OF 12

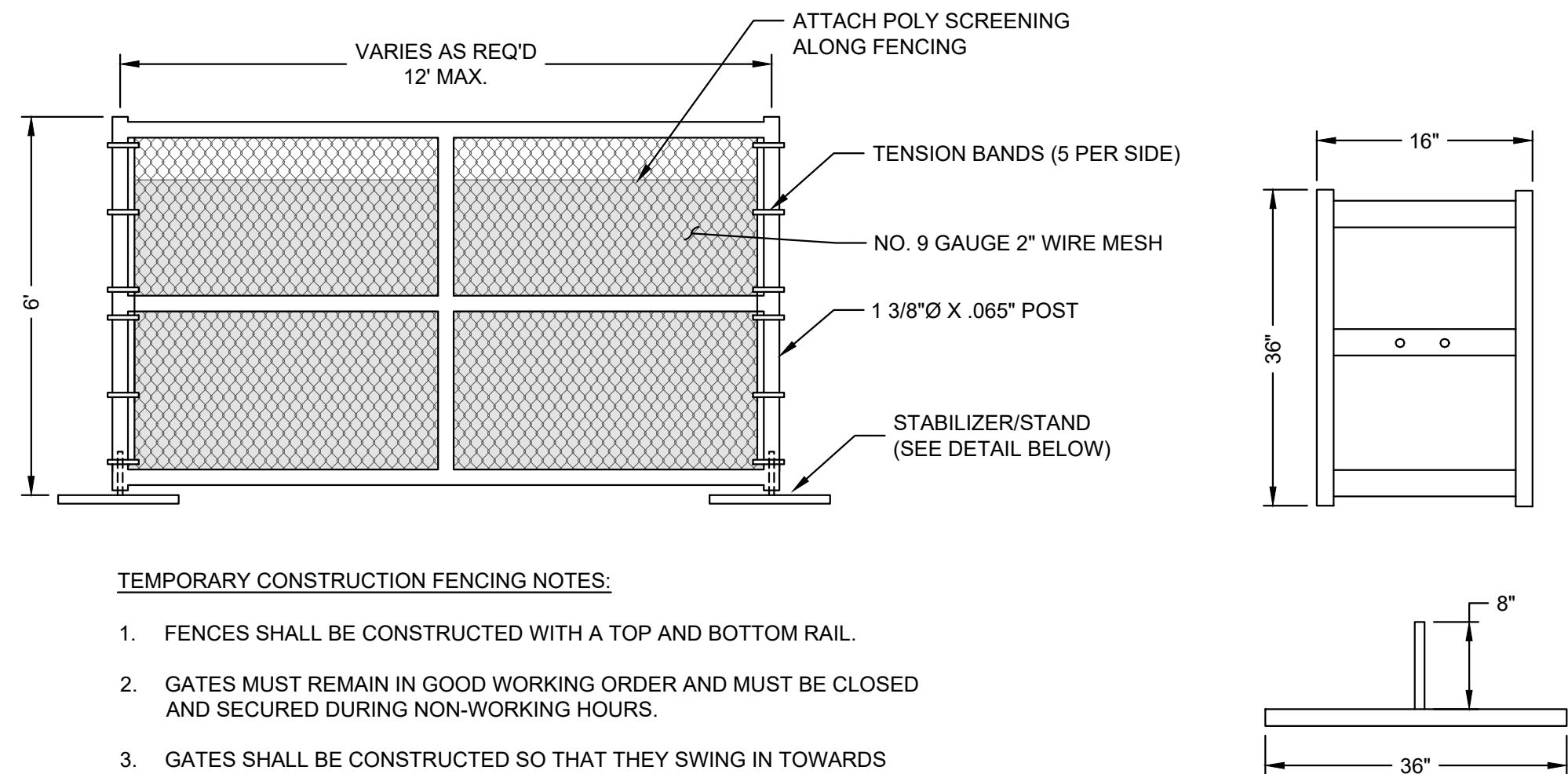




NOTES:

1. ALL MATERIAL TO MEET FILTREXX® SPECIFICATIONS.
2. STAKES/REBAR PINS SHALL HAVE PROTECTIVE CAPS INSTALLED TO PREVENT FALL INJURY.

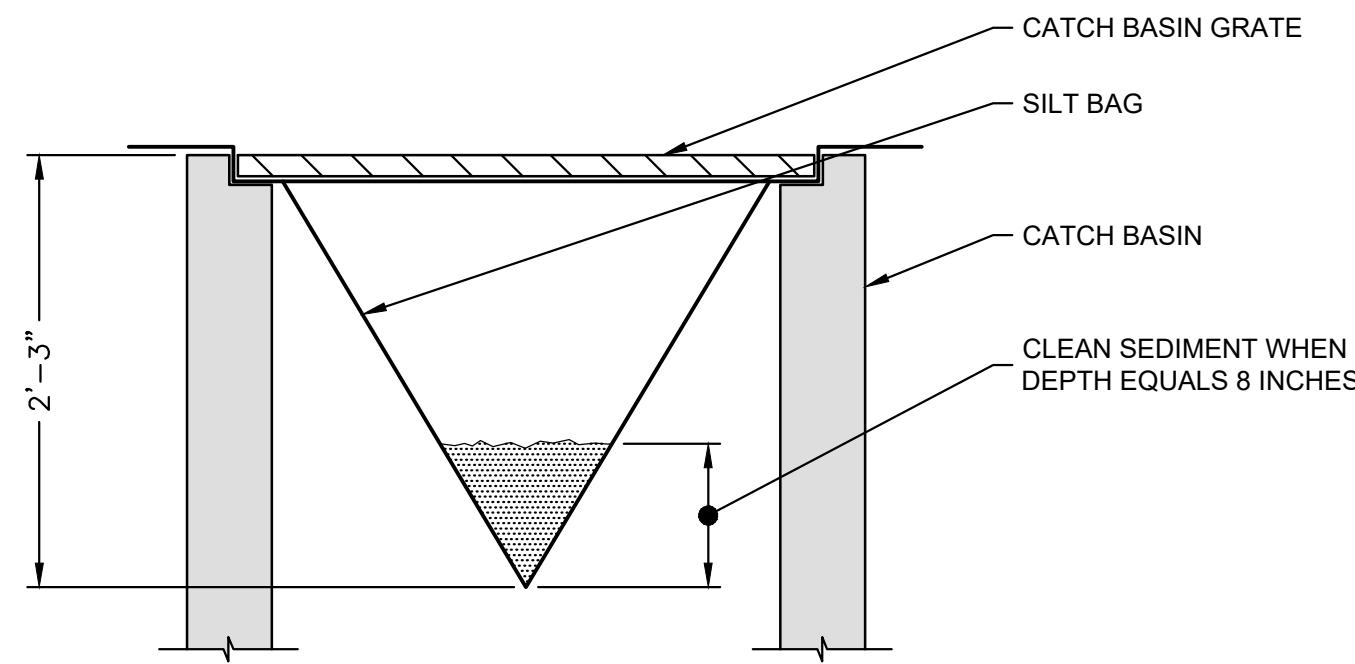
1 Filtrex®SOXX SEDIMENT CONTROL DETAIL
5 6 NOT TO SCALE



TEMPORARY CONSTRUCTION FENCING NOTES:

1. FENCES SHALL BE CONSTRUCTED WITH A TOP AND BOTTOM RAIL.
2. GATES MUST REMAIN IN GOOD WORKING ORDER AND MUST BE CLOSED AND SECURED DURING NON-WORKING HOURS.
3. GATES SHALL BE CONSTRUCTED SO THAT THEY SWING IN TOWARDS THE CONSTRUCTION SITE.
4. GATES MUST BE CONSTRUCTED WITH THE SAME DESIGN CHARACTERISTICS AS THE TEMPORARY CONSTRUCTION FENCE.
5. FABRIC SHALL BE ANCHORED IN EACH GROMMET WITH ALUMINUM TIES.

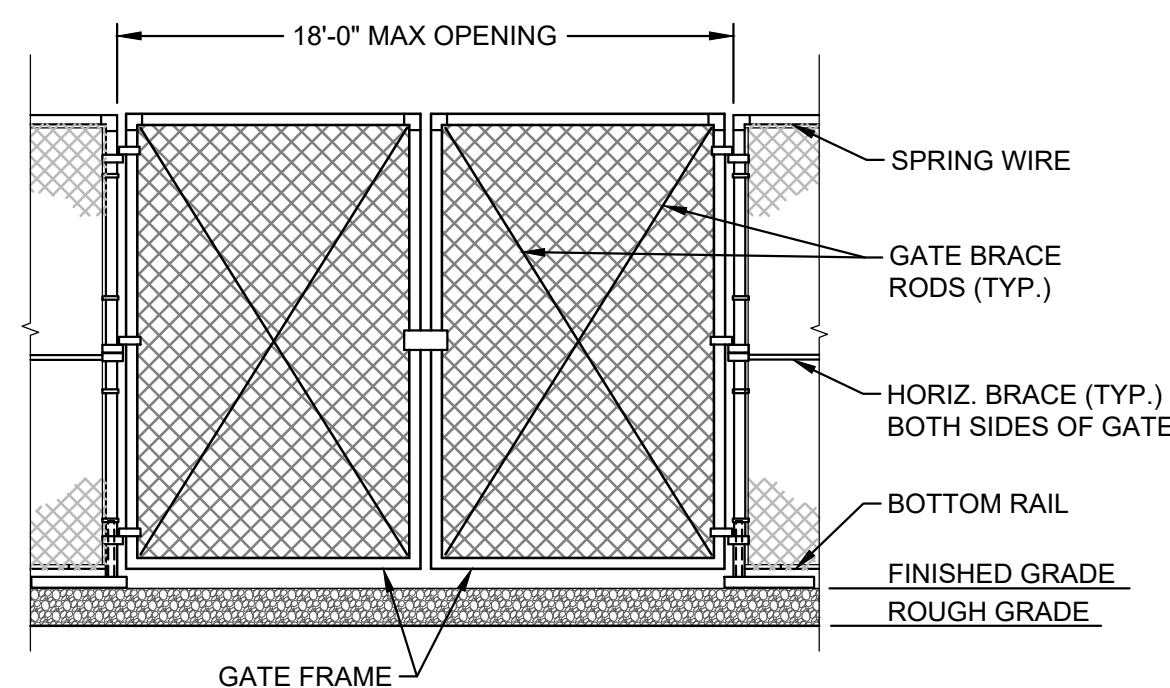
4 TYPICAL TEMPORARY CONSTRUCTION FENCE PANEL
5 6 NOT TO SCALE



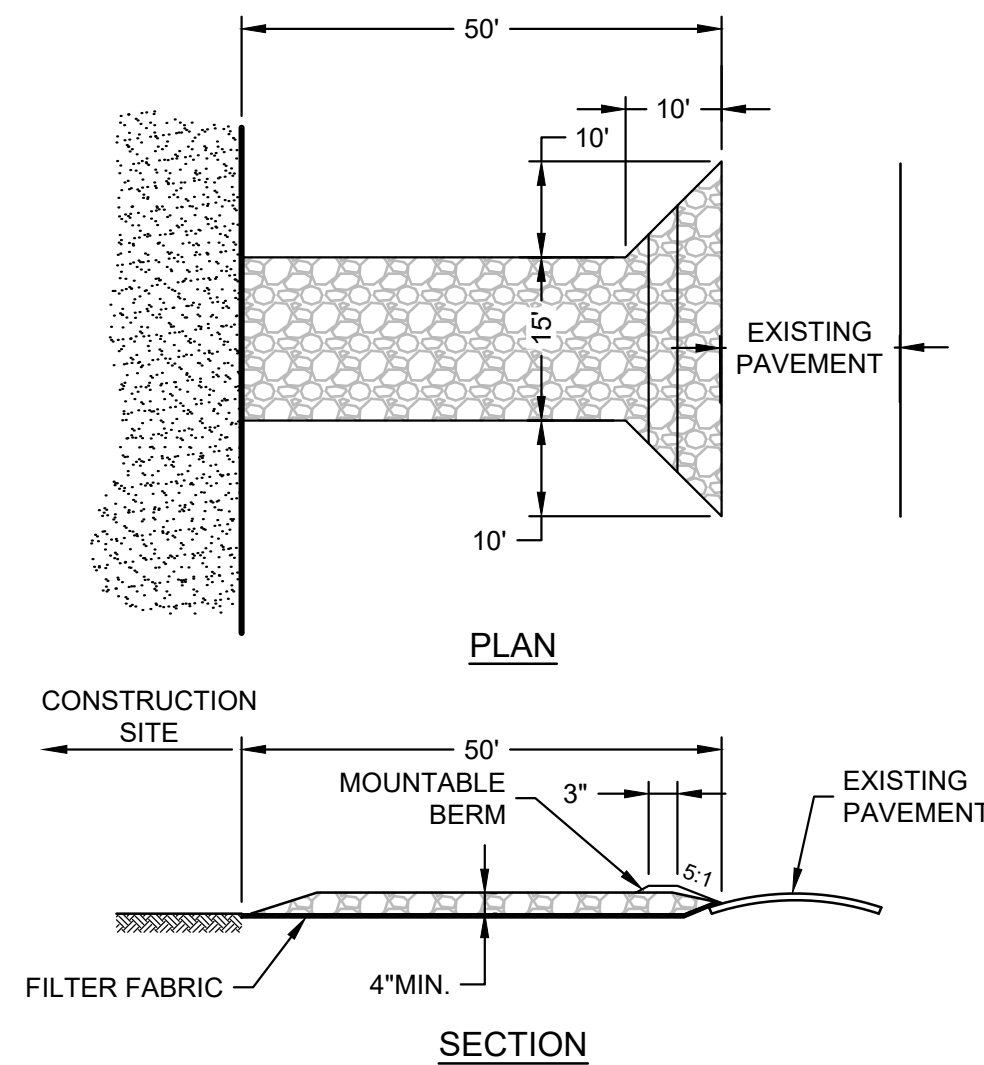
NOTES:

1. SEDIMENT BAG INLET PROTECTION TO BE SILT SACK MANUFACTURED BY ATLANTIC CONSTRUCTION FABRICS INC. RICHMOND, VA OR APPROVED EQUAL.
2. STORM WATER CATCH BASINS OR DRAINS SHALL BE PROTECTED FROM MATERIALS RUN-OFF. CONTRACTOR SHALL INSTALL SILT SACKS WITHIN EACH CATCH BASIN IN THE VICINITY OF ANY WORK AREAS IN ACCORDANCE WITH MANUFACTURER'S RECOMMENDATIONS. CONTRACTOR SHALL USE ALL BMP'S NECESSARY TO PROTECT THESE INLETS FROM SEDIMENT AND DEBRIS.

2 SILT SACK DETAIL
5 6 NOT TO SCALE



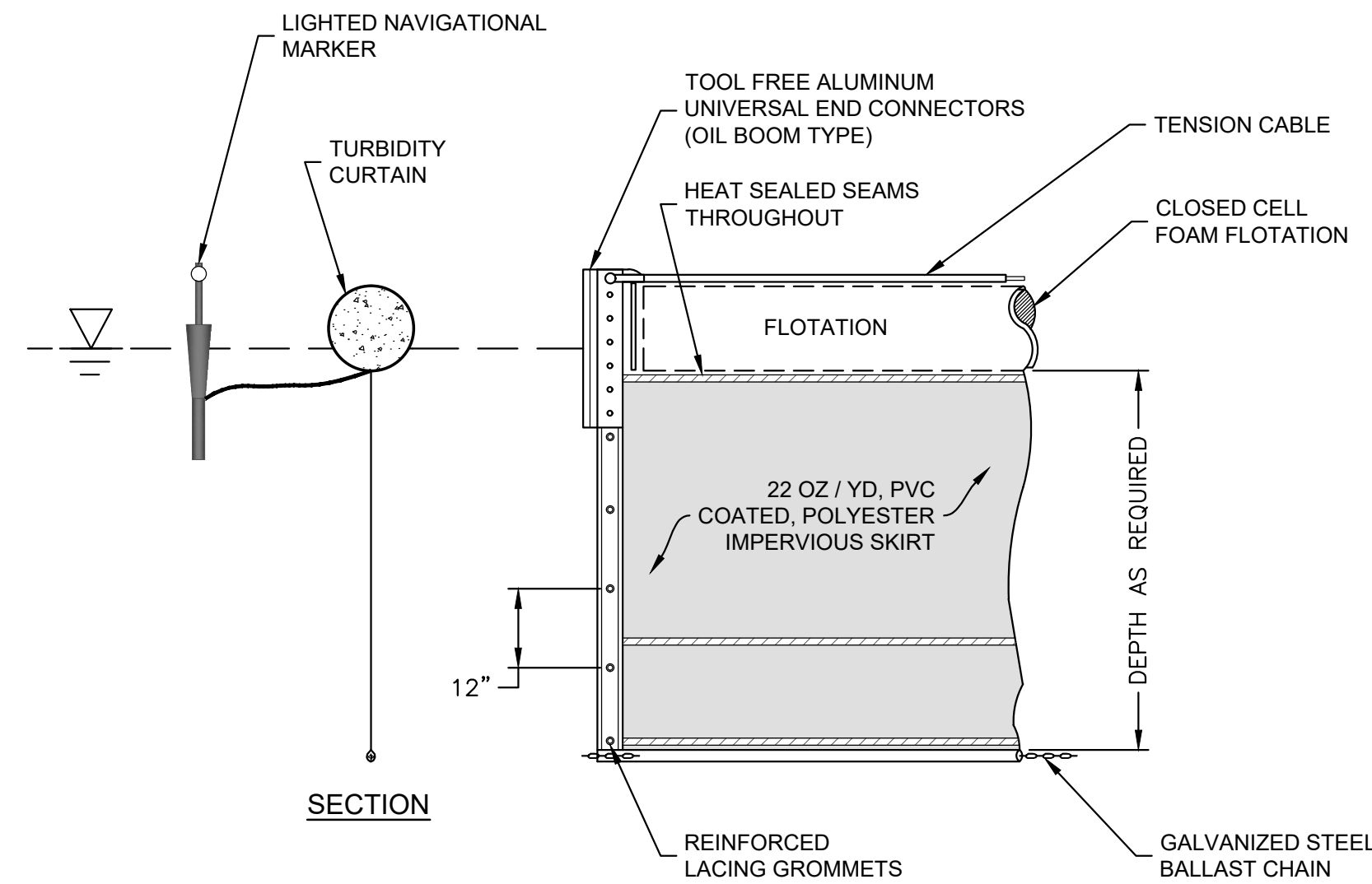
5 TYPICAL TEMPORARY CONSTRUCTION DOUBLE SWING GATE
5 6 NOT TO SCALE



6 TEMPORARY CONSTRUCTION ENTRANCE / EXIT
5 6 NOT TO SCALE

NOTES:

1. STONE SIZE - USE 2" STONE (MINIMUM) TO 6" STONE (MAXIMUM).
2. LENGTH - GREATER THAN OR EQUAL TO 50 FEET WITH THICKNESS OF 4".
3. WIDTH - FIFTEEN (15) FOOT TYP., BUT NOT LESS THAN FULL WIDTH AT POINTS WHERE INGRESS OR EGRESS OCCURS.
4. SURFACE WATER - ALL SURFACE WATER FLOWING OR DIVERTED TOWARD CONSTRUCTION ENTRANCES SHALL BE PIPED ACROSS ENTRANCE. IF PIPING IS IMPRACTICAL, MOUNTABLE BERM SHALL BE PERMITTED.
5. MAINTENANCE - THE ENTRANCE SHALL BE MAINTAINED IN A CONDITION WHICH SHALL PREVENT TRACKING OR FLOWING OF SEDIMENT ONTO PUBLIC RIGHTS-OF-WAY. THIS MAY REQUIRE PERIODIC TOP DRESSING AND ADDITIONAL STONE AS CONDITIONS DEMAND AND REPAIR OR CLEANOUT OF ANY MEASURES USED TO TRAP SEDIMENT. ALL SEDIMENT SPILLED, DROPPED, WASHED OR TRACKED ONTO PUBLIC RIGHTS-OF-WAY MUST BE REMOVED IMMEDIATELY.
6. PERIODIC INSPECTION AND NEEDED MAINTENANCE SHALL BE PROVIDED.
7. THE CLEAN STONE SHOULD BE INSTALLED OVER A GEOTEXTILE FABRIC. GEOTEXTILE FABRIC MAY BE OMITTED FOR PERMANENT CONSTRUCTION ENTRANCES-EXITS ON A CASE-BY-CASE BASIS WITH THE APPROVAL OF THE NATIONAL GRID ENVIRONMENTAL.
8. FOLLOWING CONSTRUCTION, THE CONSTRUCTION ENTRANCE / EXIT SHALL BE REMOVED AND THE AREA GRADED, SEEDED, AND MULCHED AS NEEDED. ENTRANCE / EXITS MAY REMAIN DEPENDING UPON FUTURE ACCESS NEEDS AND / OR PROJECT-SPECIFIC APPROVALS BUT REQUIRES APPROVALS FROM THE NATIONAL GRID ENVIRONMENTAL AND PROPERTY LEGAL.



NOTES:

1. REFER TO SECTION 01 57 00 - TEMPORARY CONTROLS FOR MINIMUM REQUIREMENTS OF TURBIDITY CURTAINS.
2. END OF CURTAIN SHALL BE ANCHORED SECURELY AT THE SHORELINE ABOVE MEAN HIGH WATER ELEVATION IN ACCORDANCE WITH MANUFACTURERS SPECIFICATIONS.
3. TURBIDITY CURTAINS SHALL BE INSPECTED REGULARLY TO DETERMINE IF ALL COMPONENTS ARE FUNCTIONING PROPERLY.

3 TURBIDITY CURTAIN
5 6 NOT TO SCALE



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PROJECT NO:	35429.00
DATE:	APRIL, 2025
PROJECT MGR:	TWS
DESIGNED BY:	JTF
DRAWN BY:	GRB
CHECKED BY:	TWS
REVIEWED BY:	MJP
SCALE:	AS NOTED
REVISION NO.	

CHASE MARINA
169 RIVERSIDE DRIVE
TIVERTON, RHODE ISLAND

TEMPORARY CONTROLS DETAILS

APPROVED:



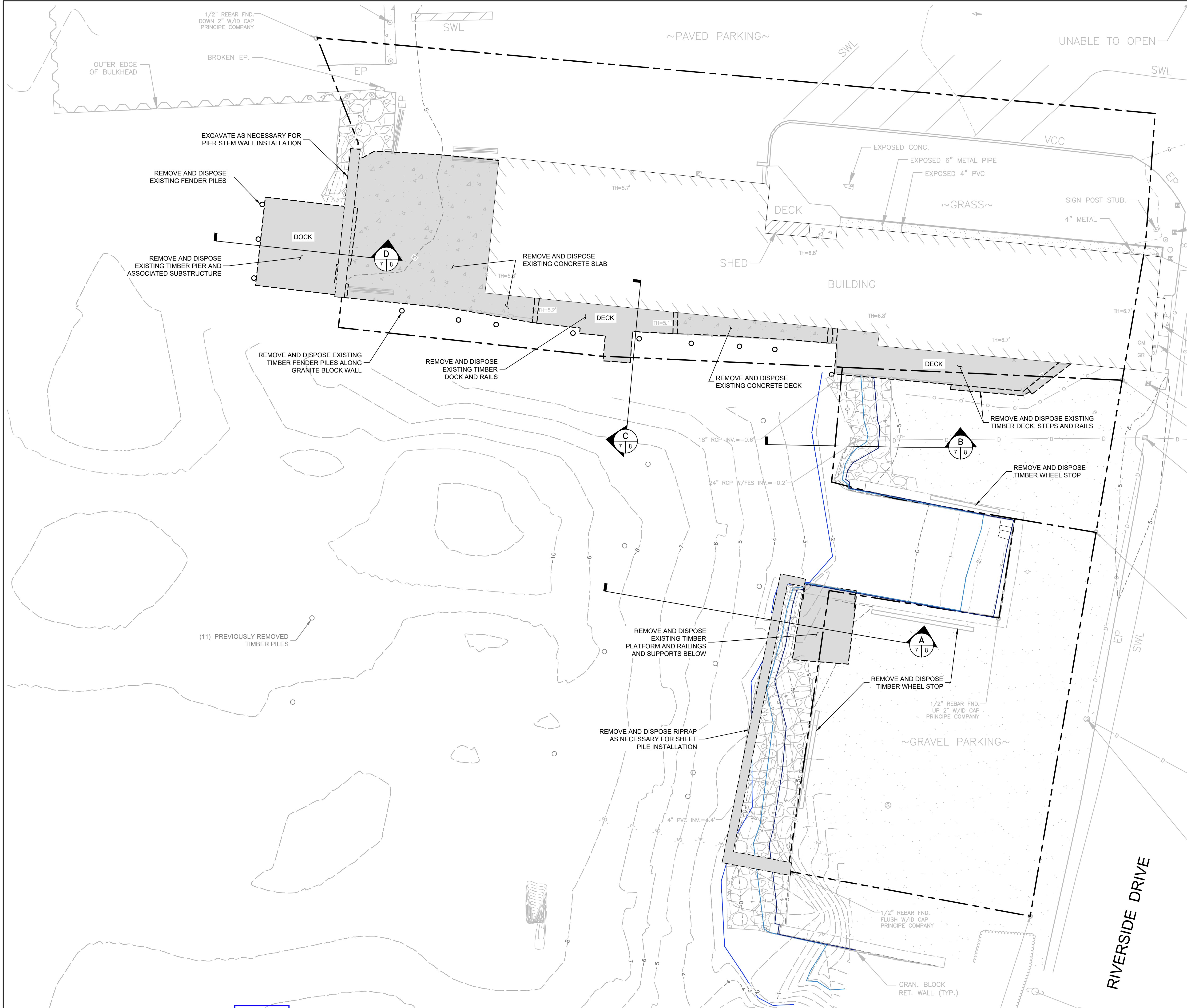
DRAWING

6

SHEET NO. 6 OF 12



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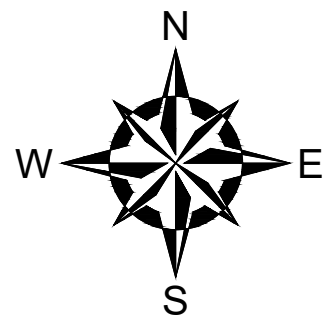


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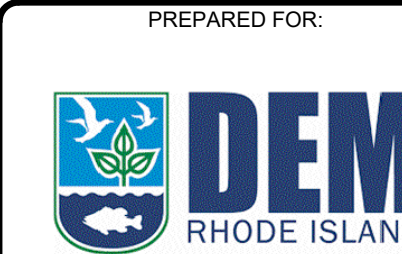
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2. BATHYMETRIC SURVEY OF SITE CREATED BY GZA - AMESBURY.

LEGEND

- APPROX. LOT LINE PER REF. PLAN
- MAJOR CONTOUR LINE
- MINOR CONTOUR LINE
- RETAINING WALL
- CHAIN LINK FENCE
- HANDRAIL
- GUARDRAIL
- OVERHEAD WIRE
- DRAIN LINE
- GAS LINE
- SHRUB LINE
- CONCRETE
- RIP RAP
- CRUSHED STONE
- SPOT GRADE
- PIPE/ROD FOUND
- UTILITY POLE
- UTILITY POLE & GUY WIRE
- UTILITY POLE STUB
- DRAIN MANHOLE
- CATCH BASIN
- FLARED END SECTION
- SEWER MANHOLE
- CLEANOUT
- GAS METER
- GAS REGULATOR VALVE
- ELECTRIC MANHOLE
- MANHOLE
- SIGN
- SQUARE POST
- POST
- WOODEN PILING & TOP ELEVATION
- ACCESSIBLE PARKING SPACE
- FLOW DIRECTION
- JERSEY BARRIER
- CONCRETE
- EDGE OF PAVEMENT
- GRANITE
- POLYVINYL CHLORIDE PIPE
- REINFORCED CONCRETE PIPE
- RETAINING WALL
- SINGLE WHITE LINE
- SINGLE YELLOW LINE
- THRESHOLD ELEVATION
- TYPICAL
- VERTICAL GRANITE CURB
- INVERT I.D. CONNECTION UNKNOWN
- REMOVE AND DISPOSE EXISTING GUIDE PILES AND FENDER PILES
- REMOVE AND DISPOSE EXISTING TIMBER DECK AND PLATFORMS / CONCRETE DECK



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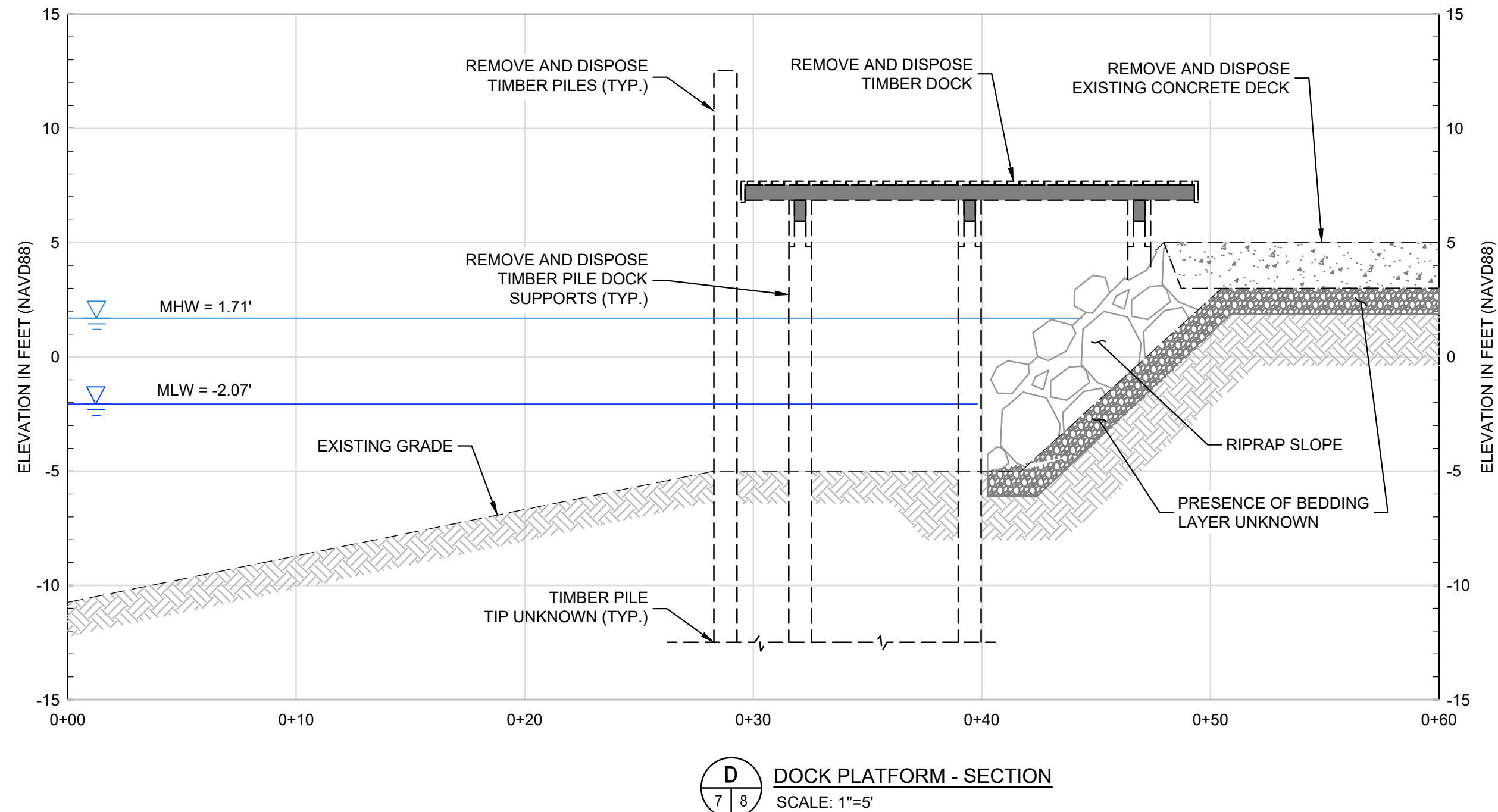
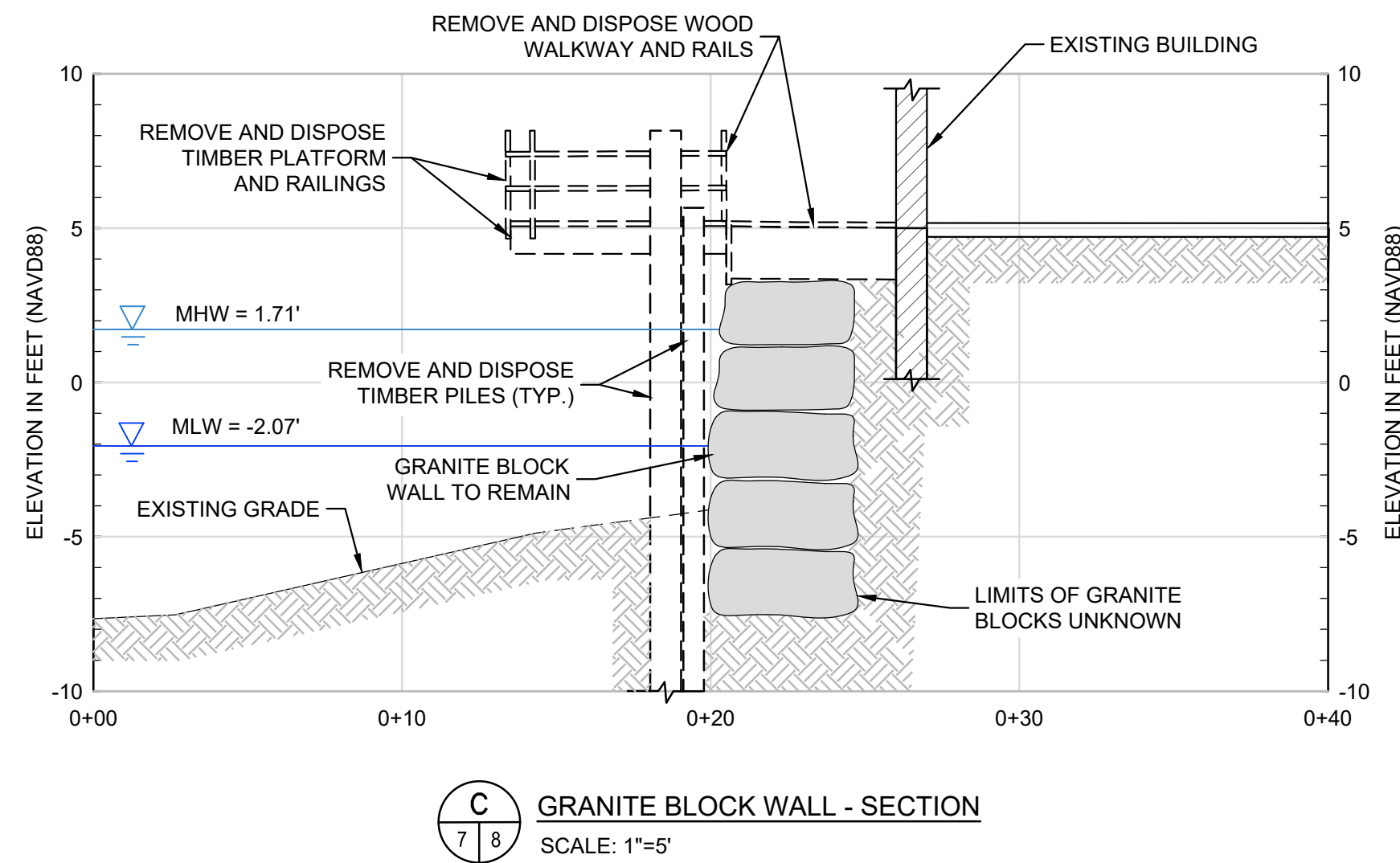
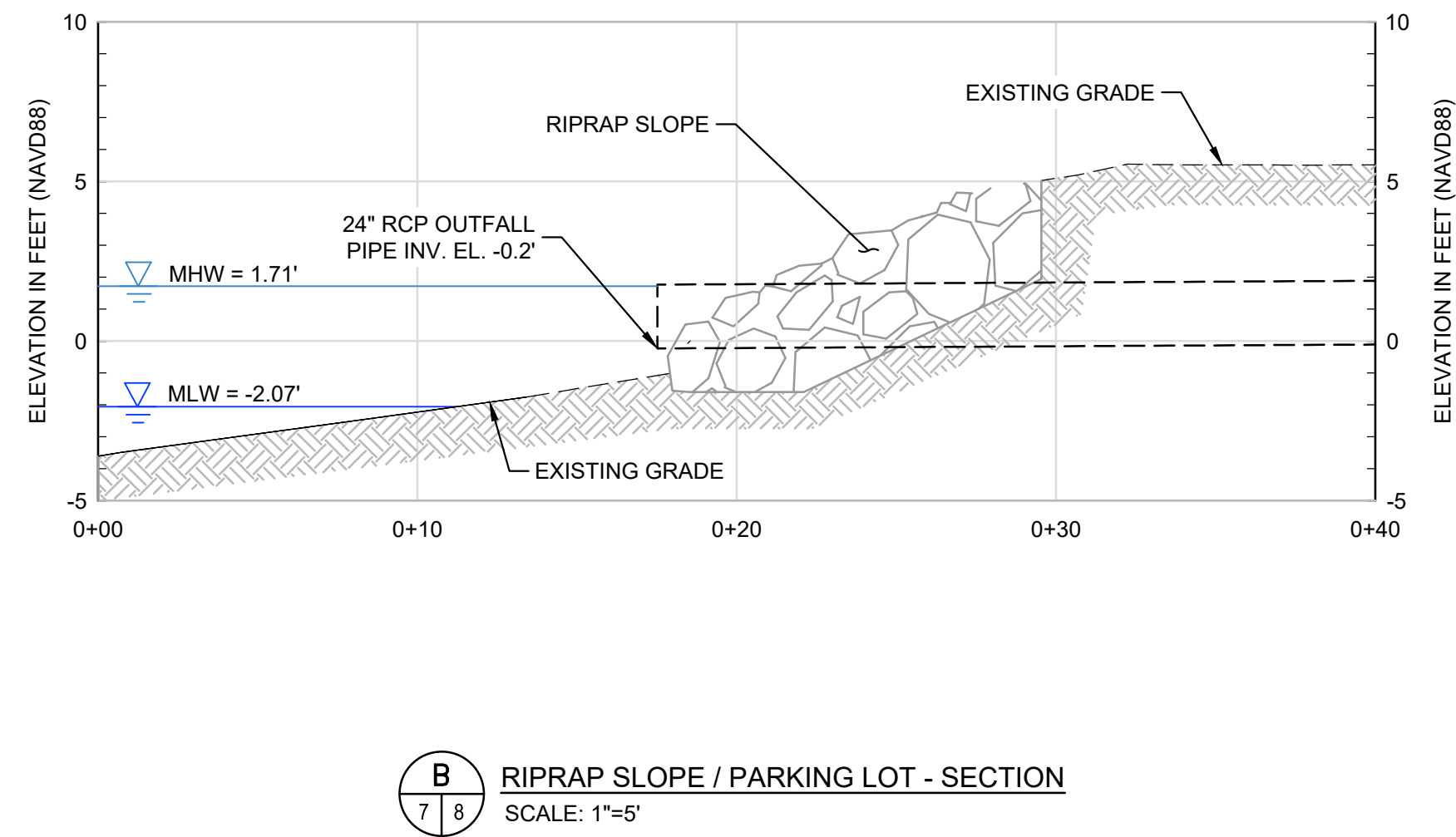
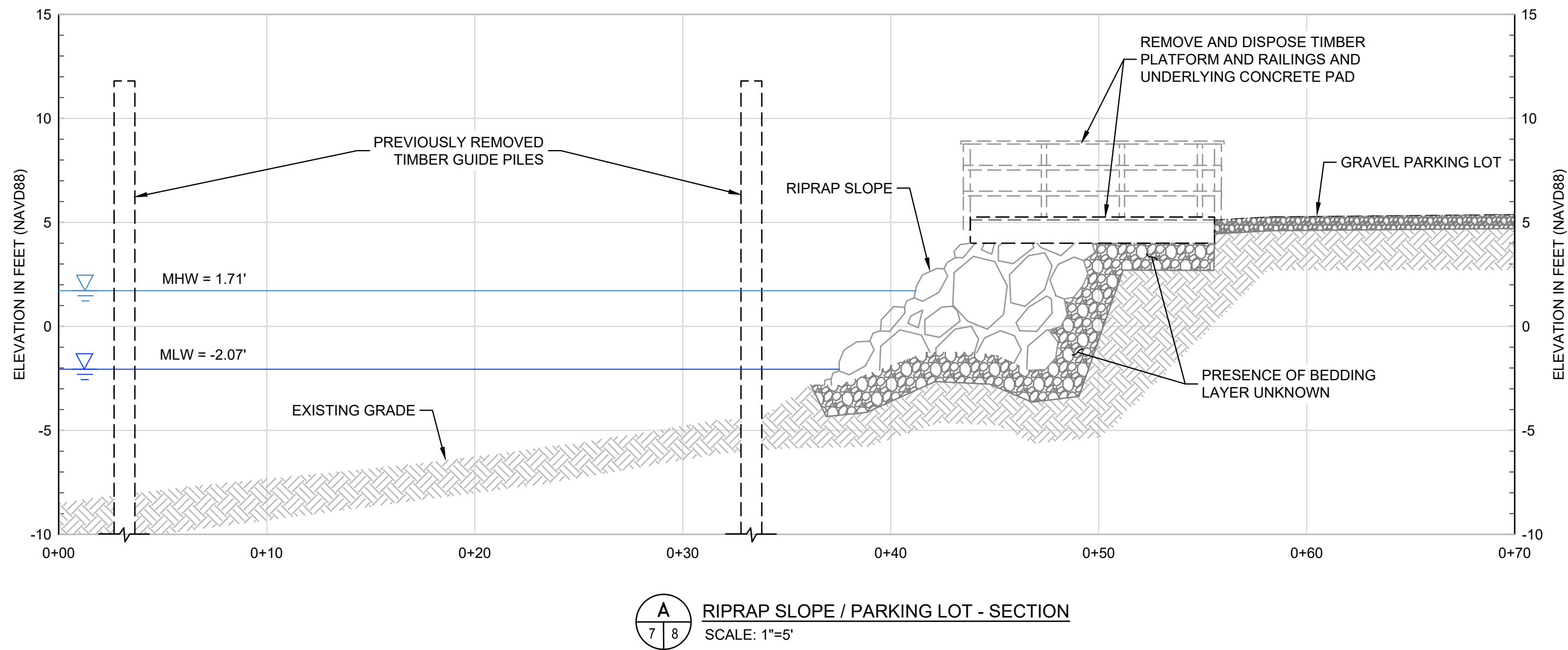
PROJECT NO:	35429.00
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PROJECT MGR:	TWS
DESIGNED BY:	JTF
DRAWN BY:	GRB
CHECKED BY:	TWS
REVIEWED BY:	MJP
SCALE:	AS NOTED
REVISION NO.	

CHASE MARINA
169 RIVERSIDE DRIVE
TIVERTON, RHODE ISLAND

DEMOLITION PLAN

DRAWING
7
SHEET NO. 7 OF 12





NOTES:

1. LIMITS AND EXTENT OF BELOW GRADE RIPRAP, BEDDING LAYERS, BUILDING FOUNDATIONS, AND STONE WALLS ARE UNKNOWN.



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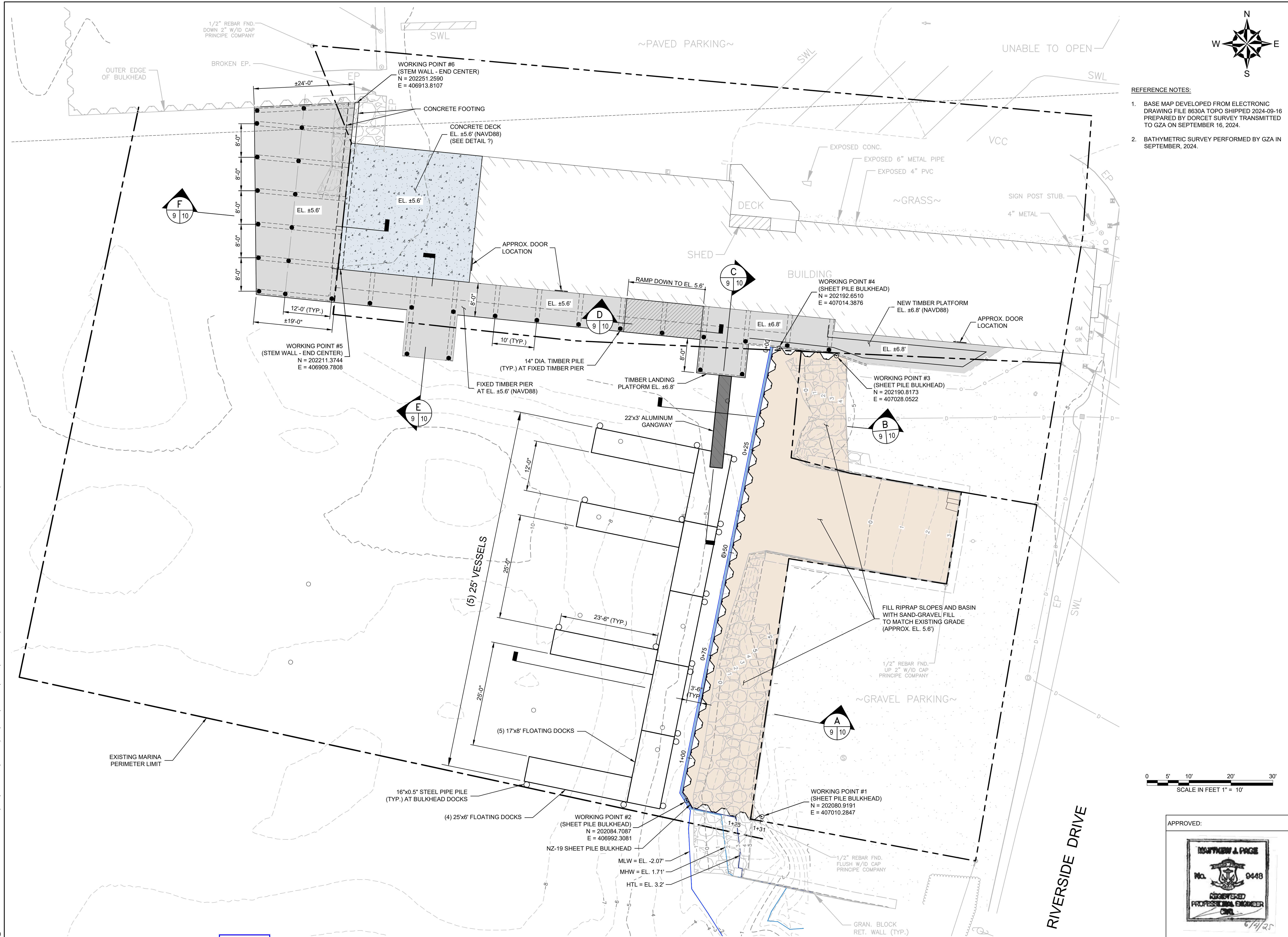
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DRAWING

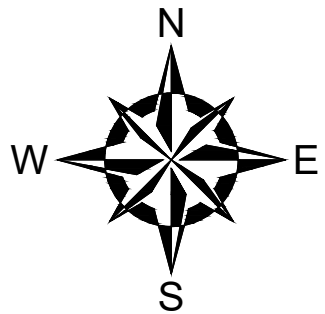
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SHEET NO. 8 OF 12





- REFERENCE NOTES:
1. BASE MAP DEVELOPED FROM ELECTRONIC DRAWING FILE 8630A TOPO SHIPPED 2024-09-16 PREPARED BY DORCET SURVEY TRANSMITTED TO GZA ON SEPTEMBER 16, 2024.
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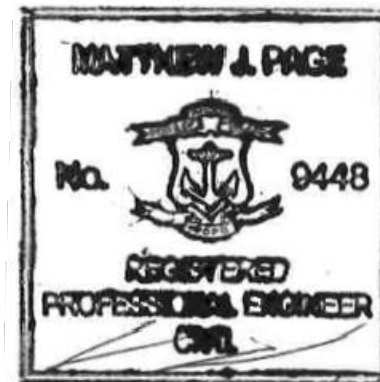
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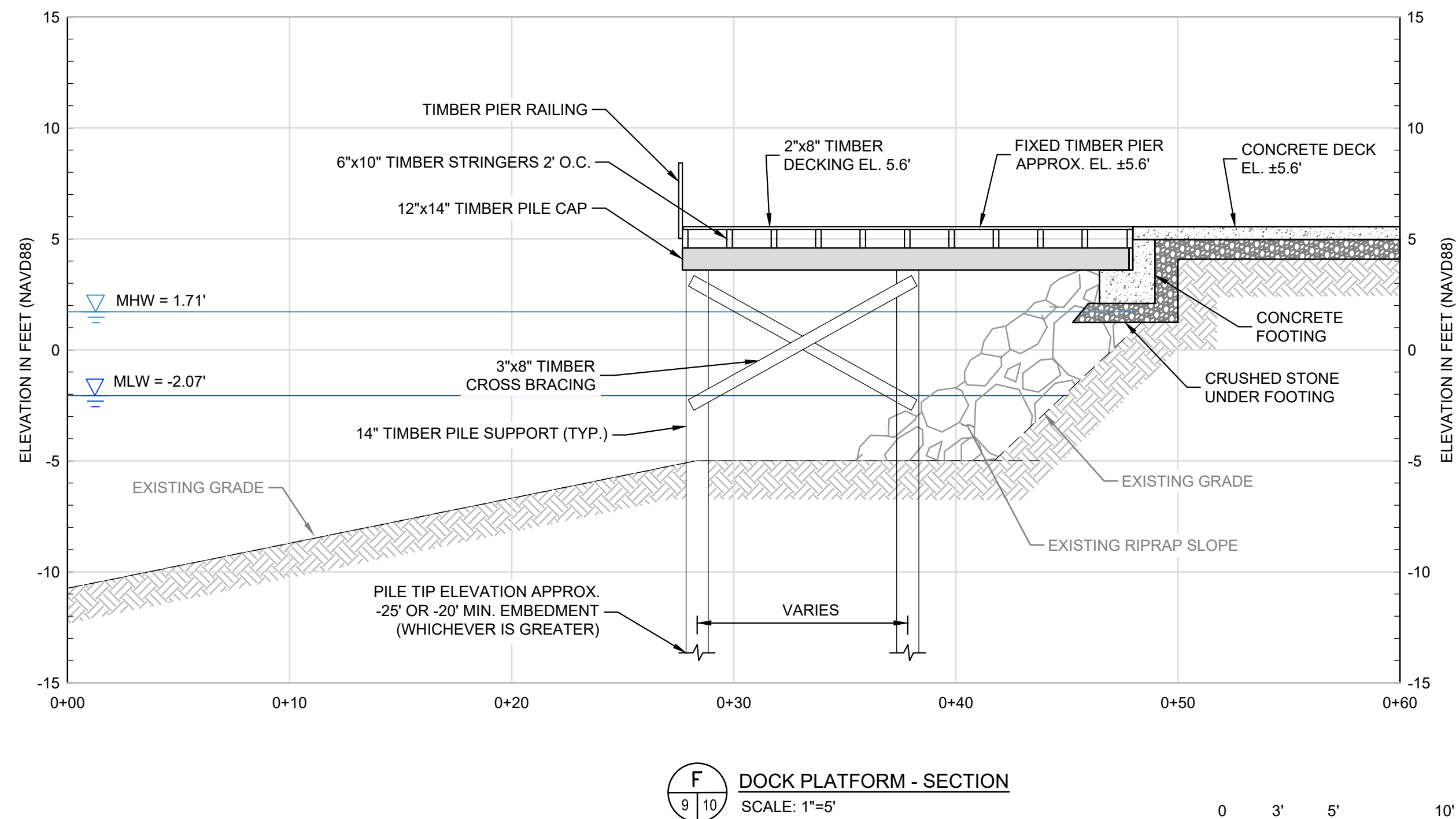
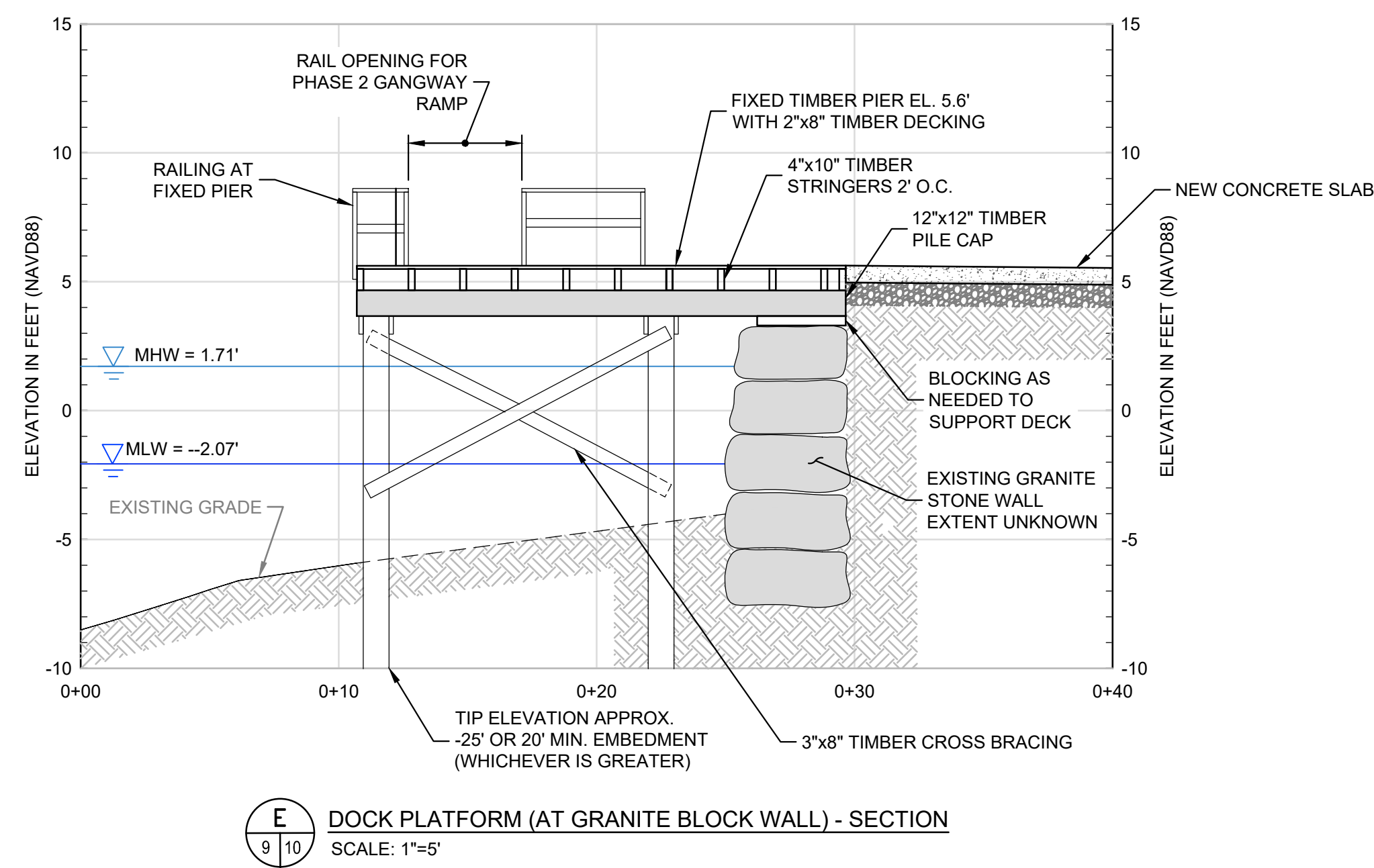
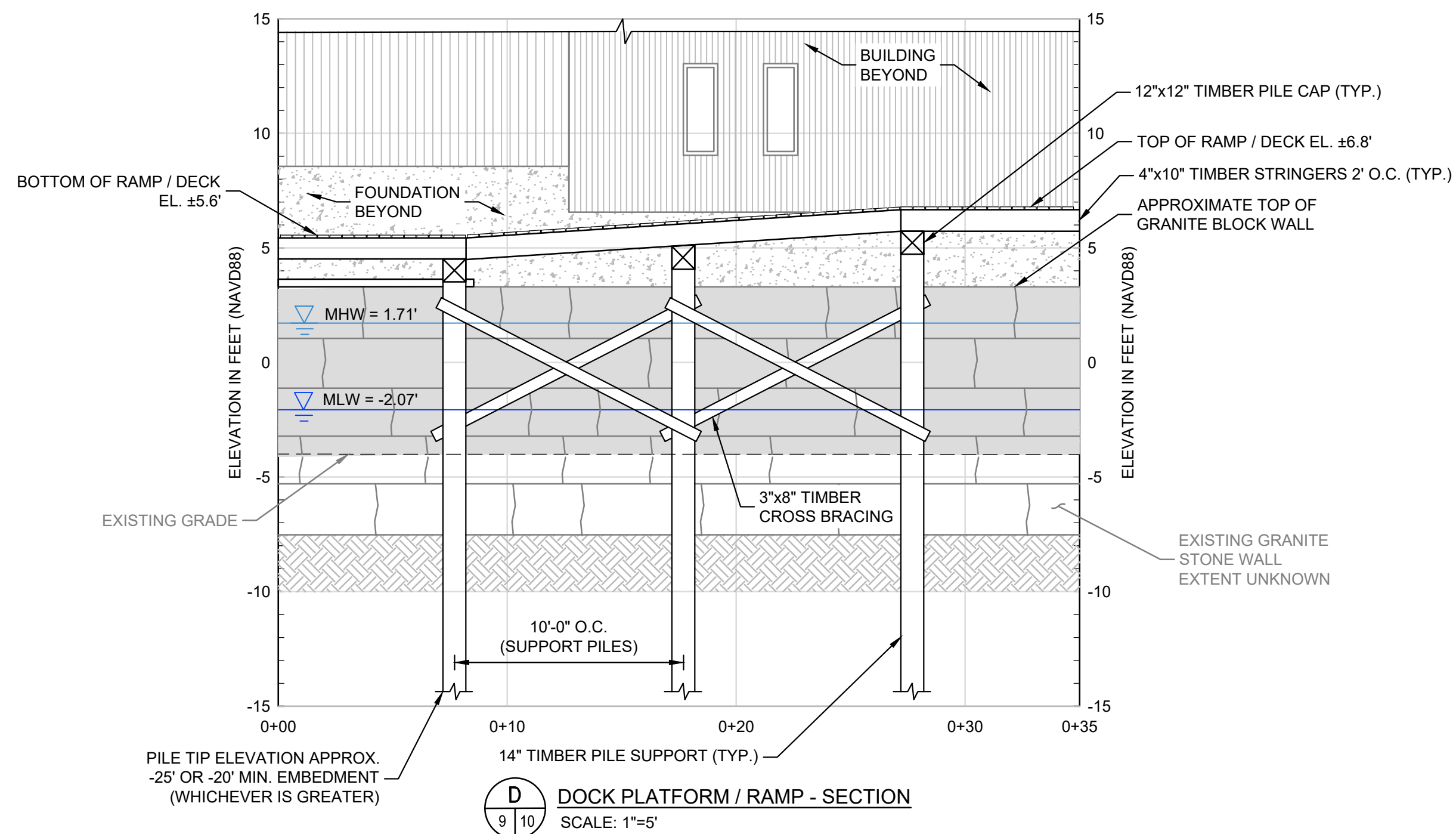
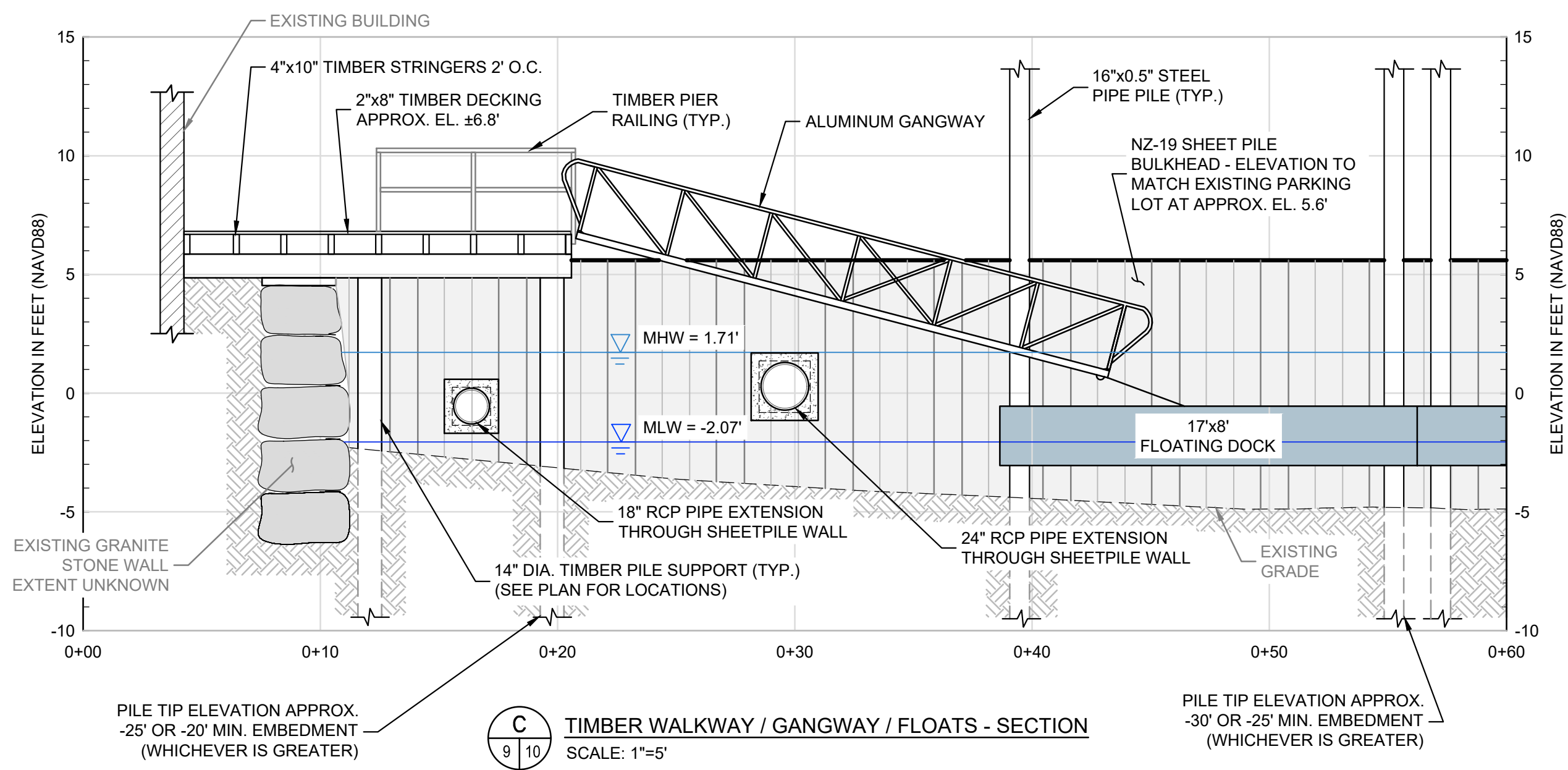
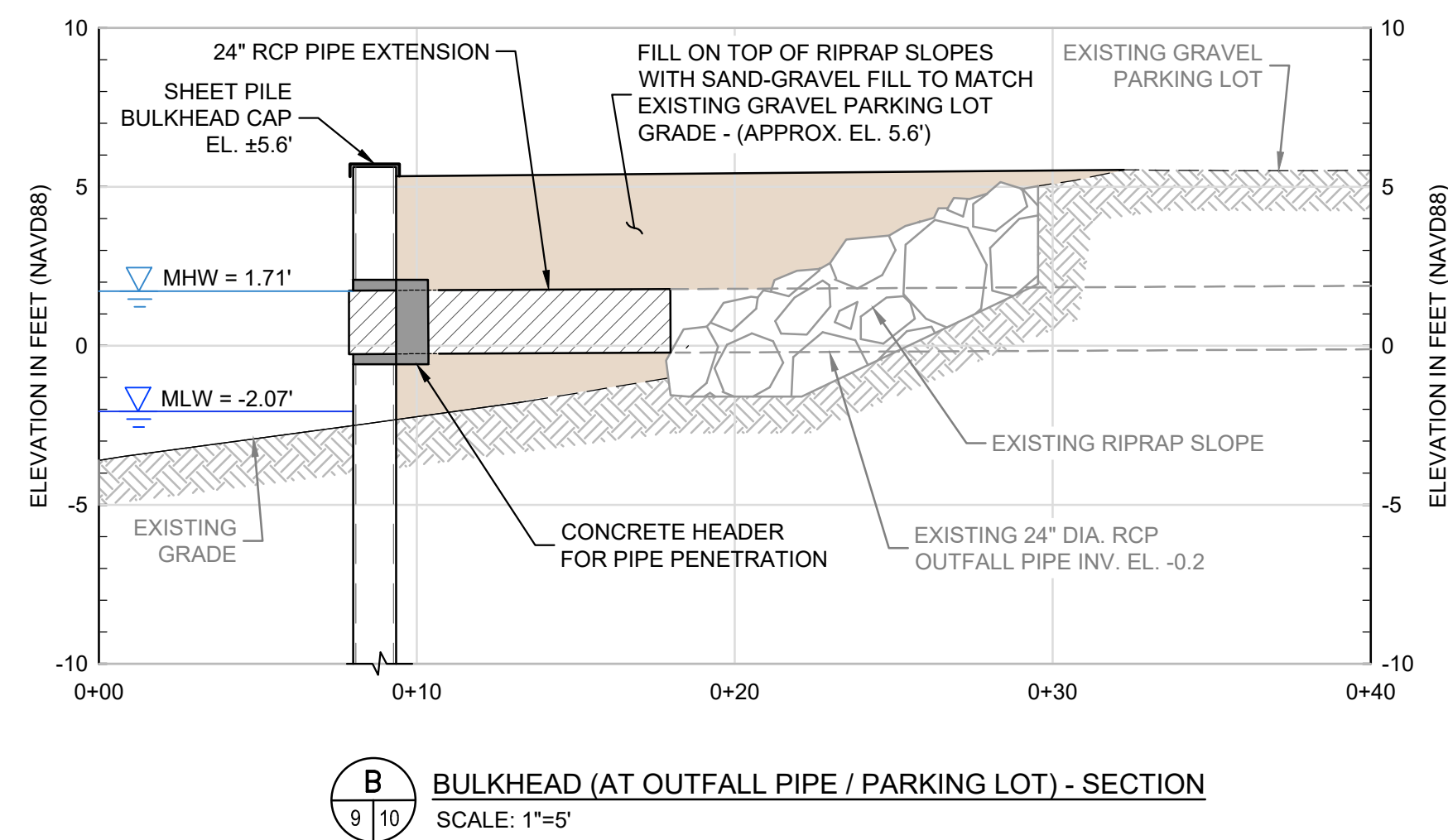
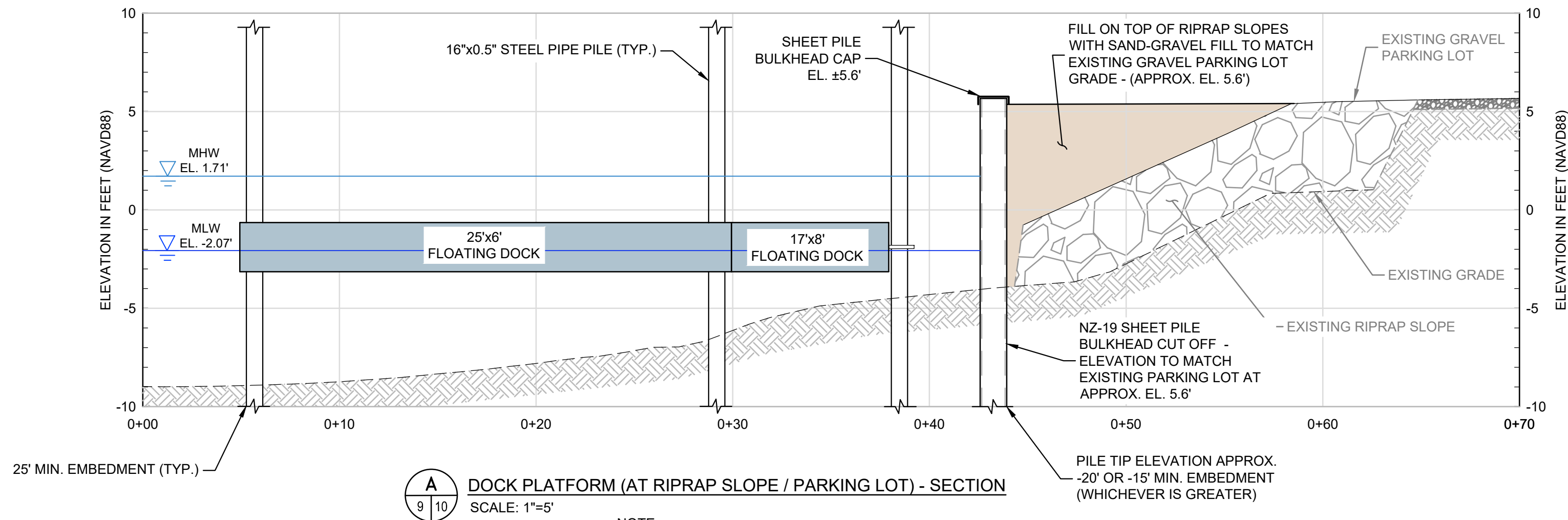
PROPOSED CONDITIONS PLAN
PHASE 1

DRAWING
9
SHEET NO. 9 OF 12

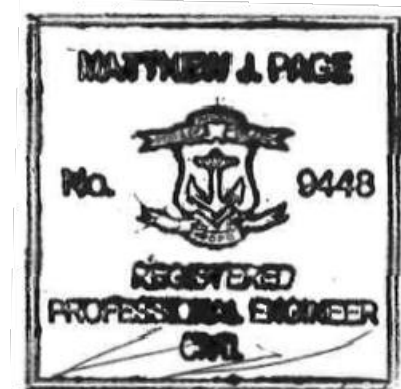


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DRAWN BY: GRB
CHECKED BY: TWS
REVIEWED BY: MJP
SCALE: AS NOTED
REVISION NO.

CHASE MARINA
169 RIVERSIDE DRIVE
TIVERTON, RHODE ISLAND

PROPOSED CONDITIONS SECTIONS
PHASE 1

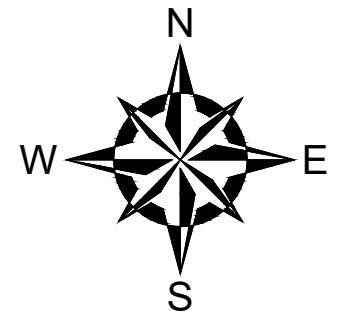
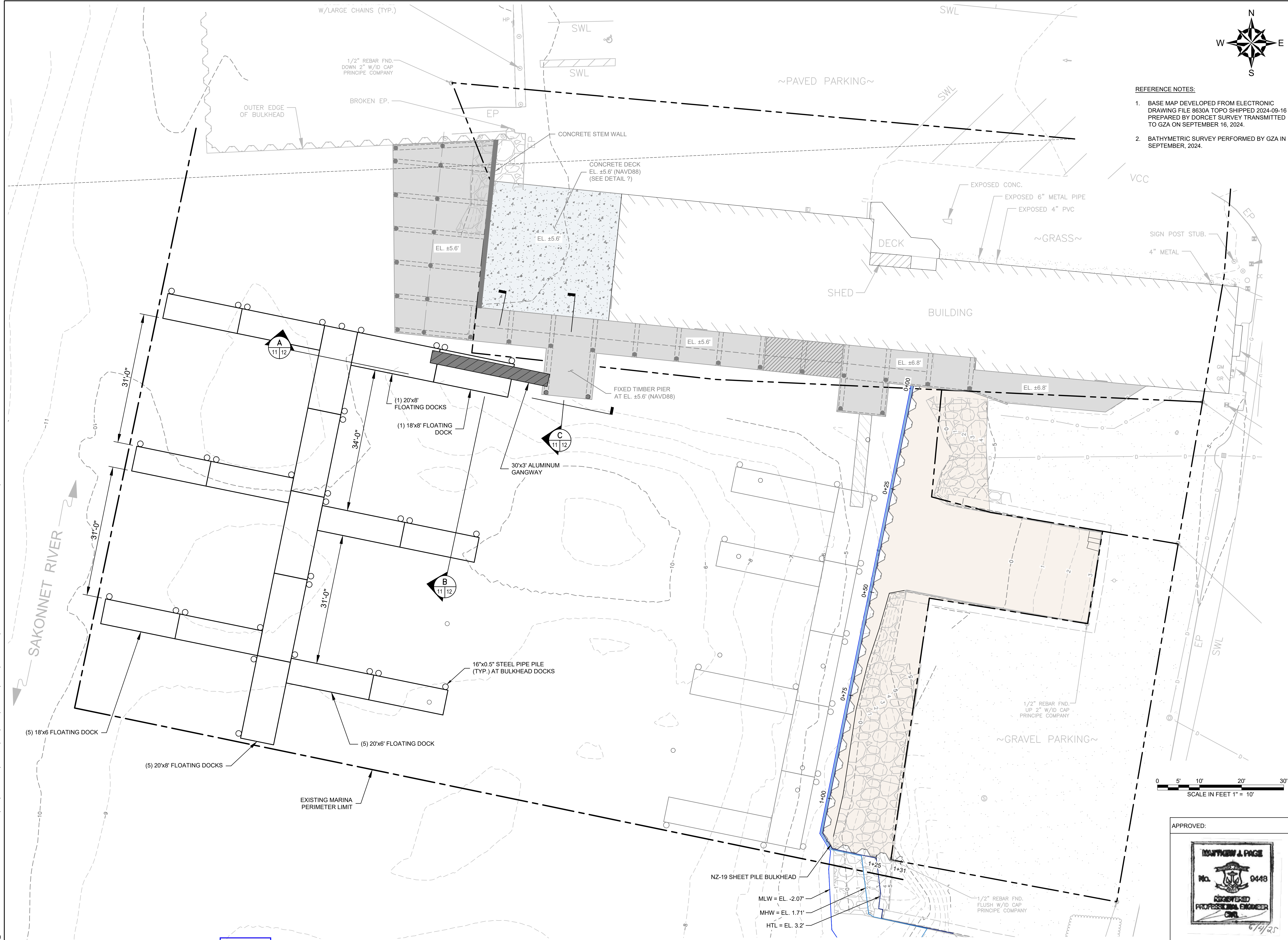
DRAWING

10

SHEET NO. 10 OF 12



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REFERENCE NOTES:

1. BASE MAP DEVELOPED FROM ELECTRONIC DRAWING FILE 8630A TOPO SHIPPED 2024-09-16 PREPARED BY DORCET SURVEY TRANSMITTED TO GZA ON SEPTEMBER 16, 2024.
2. BATHYMETRIC SURVEY PERFORMED BY GZA IN SEPTEMBER, 2024.

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REV	DESCRIPTION	DATE	BY

PROJECT NO: 35429.00

DATE: APRIL, 2025

PROJECT MGR: TWS

DESIGNED BY: JTF

DRAWN BY: GRB

CHECKED BY: TWS

REVIEWED BY: MJP

SCALE: AS NOTED

REVISION NO.

CHASE MARINA
169 RIVERSIDE DRIVE
TIVERTON, RHODE ISLAND

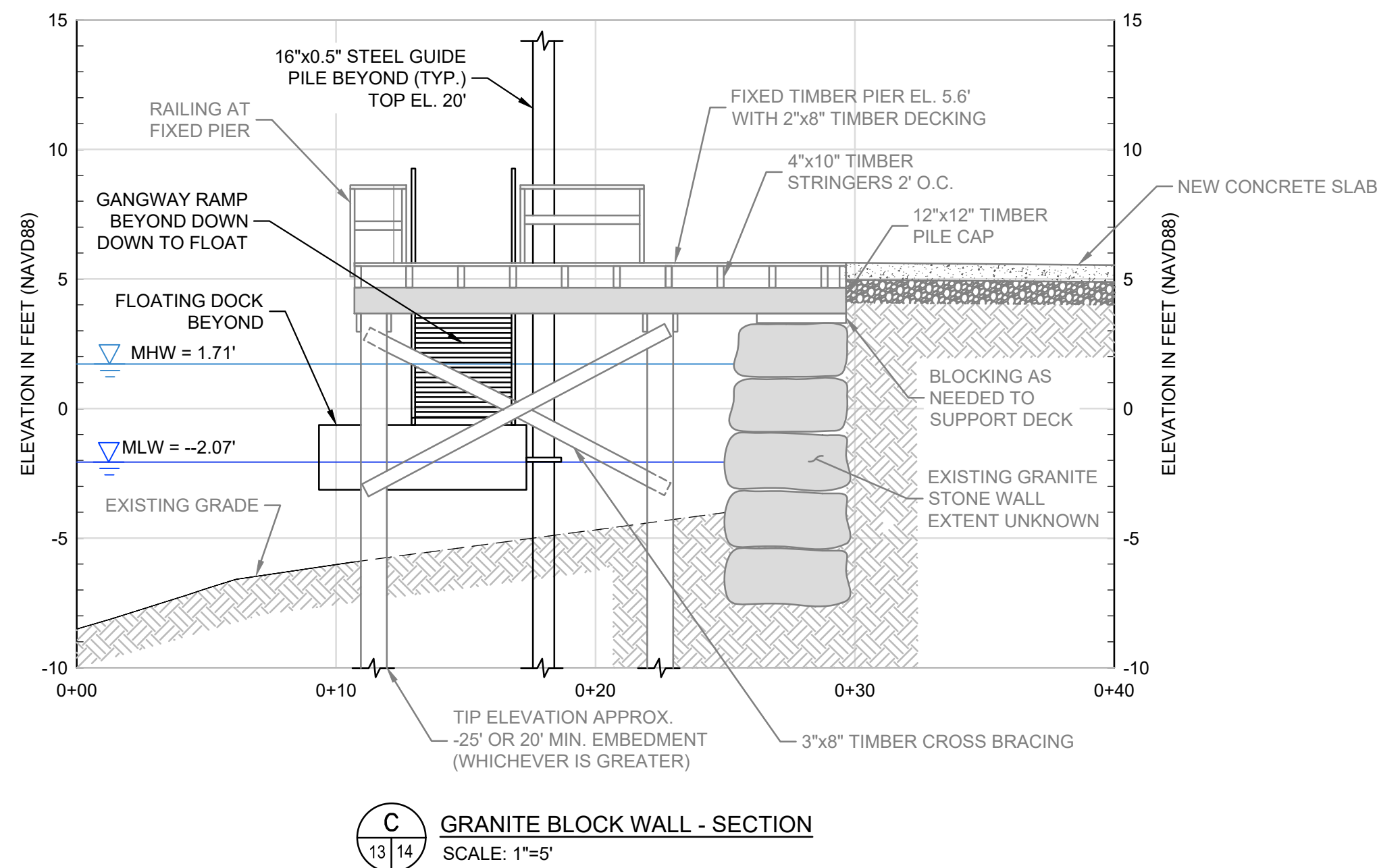
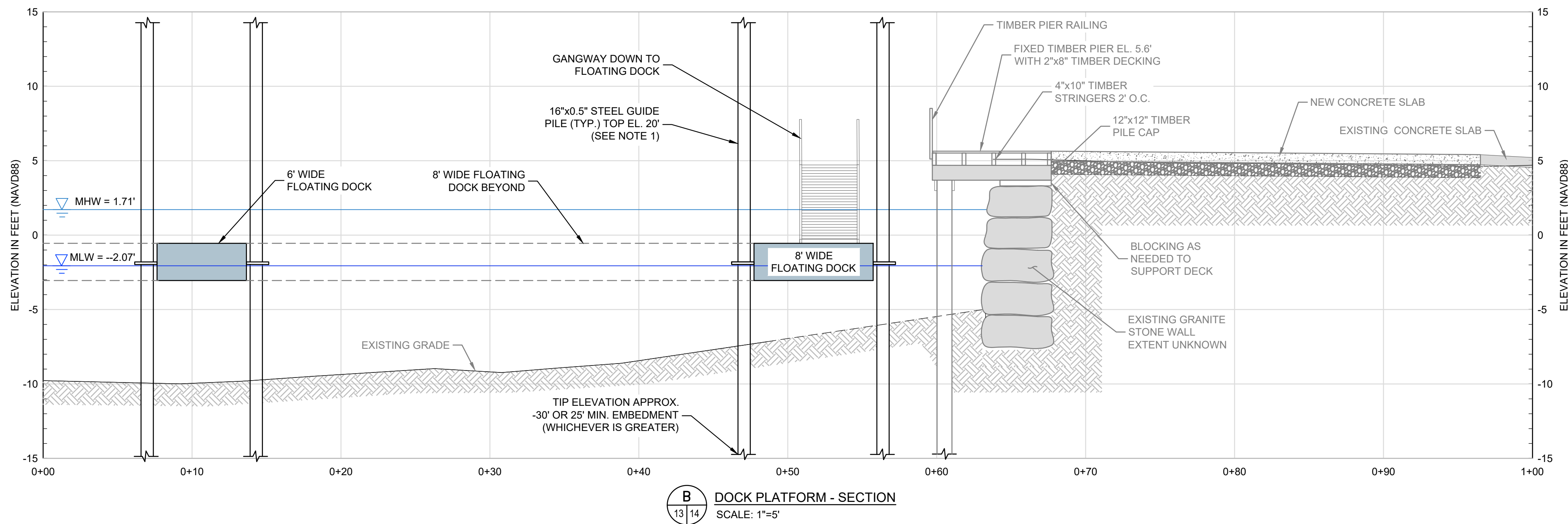
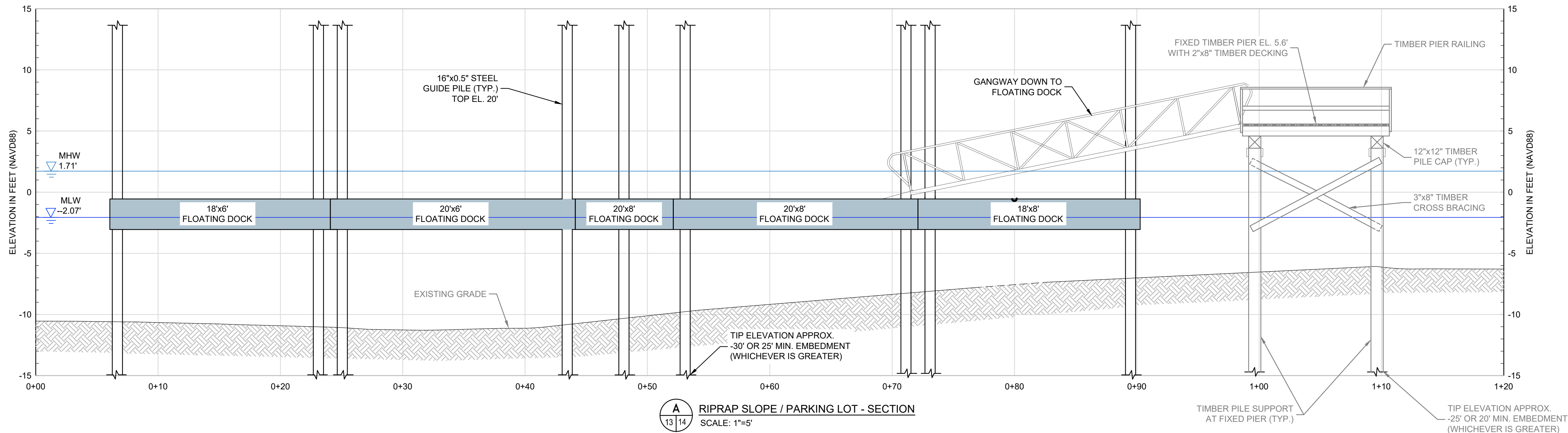
PROPOSED CONDITIONS PLAN
PHASE 2

DRAWING
11
SHEET NO. 11 OF 12

APPROVED:



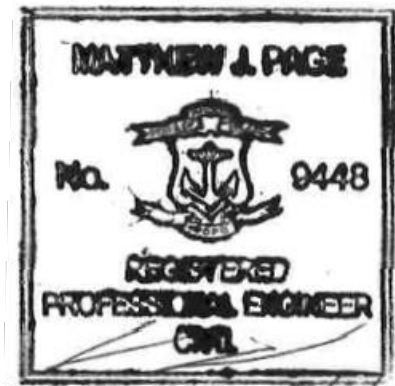
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NOTE:
1. TOP OF PILE CUTOFF ELEVATION = 20-FEET

0 3' 5' 10' 15'
SCALE IN FEET 1" = 5'

APPROVED:



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PROPOSED CONDITIONS SECTIONS
PHASE 2

DRAWING

12

SHEET NO. 12 OF 12